



NOTICE

of

COUNCIL ASSESSMENT PANEL MEETING

Pursuant to the provisions of Section 82 of the Planning, Development and Infrastructure Act 2016

TO BE HELD IN

**COUNCIL CHAMBERS
PLAYFORD CIVIC CENTRE
10 PLAYFORD BOULEVARD, ELIZABETH**

MEMBERS MAY PARTICIPATE BY ELECTRONIC MEANS

ON

THURSDAY, 20 APRIL 2023 AT 6:00PM

THIS MEETING WILL ALSO BE VIEWABLE AT
<https://www.youtube.com/user/CityOfPlayford>

A handwritten signature in blue ink, appearing to read "A Squires".

**ADAM SQUIRES
ASSESSMENT MANAGER**

Issue Date: Thursday, 13 April 2023

MEMBERSHIP

MR GEOFF PARSONS – PRESIDING MEMBER

Mr Aaron Curtis
Mr Paul Mickan

Ms Cherie Gill (Deputy)
Ms Misty Norris

Mr Nathan Grantham
Ms Jane Onuzans

**City of Playford
Council Assessment Panel Meeting**

**AGENDA
THURSDAY, 20 APRIL 2023 AT 6:00PM**

ACKNOWLEDGEMENT OF COUNTRY

We would like to acknowledge that this land we meet on today is the traditional land of the Kaurna people, and that we respect their spiritual relationship with their country. The City of Playford would also like to pay respects to Elders past, present and emerging.

1 ATTENDANCE RECORD

- 1.1 Present
- 1.2 Apologies
- 1.3 Not Present

2 CONFIRMATION OF MINUTES

RECOMMENDATION

The Minutes of the Council Assessment Panel Meeting held 19 January 2023 be confirmed as a true and accurate record of proceedings.

3 APPLICATIONS WITHDRAWN

4 DECLARATIONS OF INTEREST

5 APPLICATIONS FOR CONSIDERATION – PERSONS WISHING TO BE HEARD

- 5.1 Toursit Accommodation comprising two glamping tents, conversion of a portion of the existing shed to accomodate amenities (kitchen and bathroom facilities) and car parking (ID-21032783) (Attachments)6

Representors: Sandra Harbison
Applicant: Mrs Pasqualina La Serra

6 APPLICATIONS FOR CONSIDERATION – NO PERSONS TO BE HEARD

- 6.1 Construction of a building comprising two bulky goods tenancies with associated advertising and retaining wall (ID: 22039894) (Attachments).....39

Representors: Carol Bennett
Applicant: Leyton Property C-/ Future Urban Pty Ltd

7 APPLICATIONS FOR CONSIDERATION - CATEGORY 1

Nil

8 OUTSTANDING MATTERS – APPEALS AND DEFERRED ITEMS

Nil

9 OTHER BUSINESS**9.1 STAFF REPORTS**

Nil

10 CONFIDENTIAL MATTERS

Nil

11 POLICY DISCUSSION FORUM

Nil

12 CLOSURE

APPLICATIONS FOR CONSIDERATION

APPLICATIONS FOR CONSIDERATION – PERSONS WISHING TO BE HEARD

5.1 TOURSIT ACCOMMODATION COMPRISING TWO GLAMPING TENTS, CONVERSION OF A PORTION OF THE EXISTING SHED TO ACCOMMODATE AMENITIES (KITCHEN AND BATHROOM FACILITIES) AND CAR PARKING (ID-21032783)

Snapshot

Report author:	David Storey
Assessing Officer:	Danni Biar
Proposal:	Tourist Accommodation comprising two glamping tents, conversion of a portion of the existing shed to accommodate amenities (kitchen and bathroom facilities) and car parking
Development Number:	21032783
Date of Lodgement:	03/04/2022
Owner:	Mrs Pasqualina La Serra
Applicant:	Mrs Pasqualina La Serra
Location:	536 Medlow Road, Uleybury
Zone:	Hills Face Zone
Classification:	Performance Assessed
Public Notification Category:	Public Notification required
Representation Received:	Yes
Code Version:	2022.4 (3 April 2022)
Request for Additional Information Made?	Yes
Recommendation:	To Grant Development Plan Consent

Attachments:	1 1 . Application documents
	2 2 . Initial notification representations
	3 3 . Response to initial round representations
	4 4 . Second notification copy of representations
	5 5 . Response to second round notifications

1. The Subject Land

The subject land consists of a single allotment legally referred to as Allotment 537 of Folio 163505, Certificate of Title 5438 Folio 108. The land is more commonly referred to as 536 Medlow Road, Uleybury located on the south-east quadrant of Craigmores Road and Medlow Road,

The subject land is irregular in shape totaling 15,3774m². The site has a long primary frontage to Medlow Road of 753.1m and a secondary frontage to Craigmores Road of 222.2m.

The site currently contains a single storey detached dwelling located to the north east corner of the allotment with two sheds. The larger of the two sheds includes bathroom and toilet facilities which are to be upgraded by the applicant (see Subject Site map below).

The subject site contains stands of vegetation along the perimeter of the northern and western boundaries and around the existing dwelling.

The subject land is relatively flat with a gentle slope down from the eastern boundary to the central portion of the land. The land towards the western boundary has a more significant slope down towards the west.

Subject Site



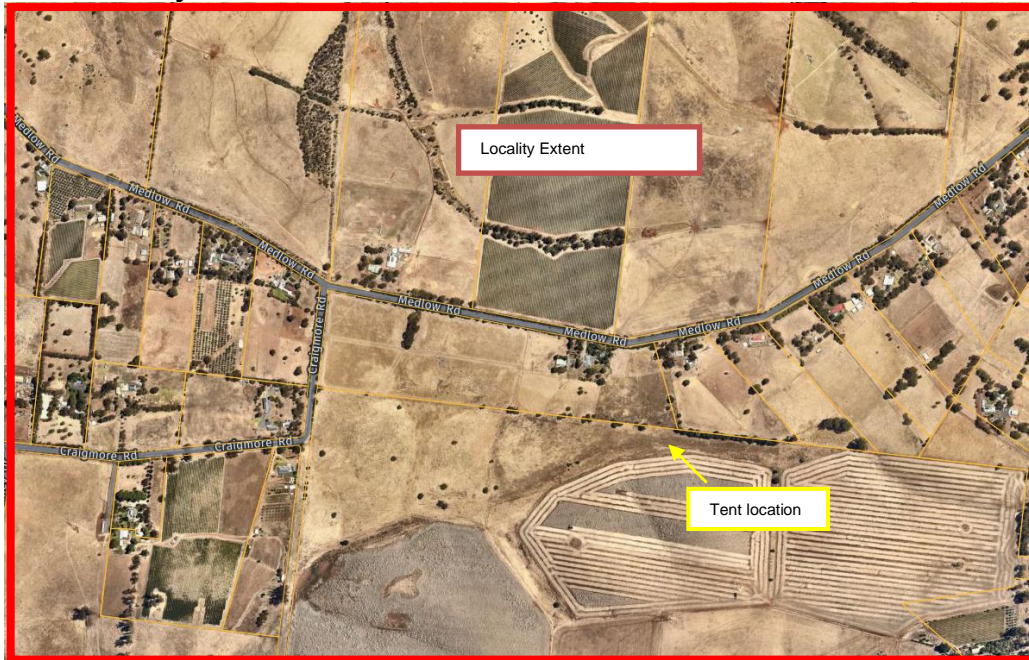
Source: Nearmap (2023)

2. The Locality

Based on the visibility of the allotment, the extent of the locality is considered to include the subject land, other rural living style allotments containing detached dwellings on large holdings along Medlow Road and further to the west along Craigmores Road and primary production activities including grazing and cropping further to the south and north.

The subject land and its locality comprises land that is undulating with stands of vegetation restricted to road reserves and the perimeters of property boundaries. The locality has an attractive natural landscape

2.1 Locality Plan



Source: Nearmaps March 2023

2.2 Zoning

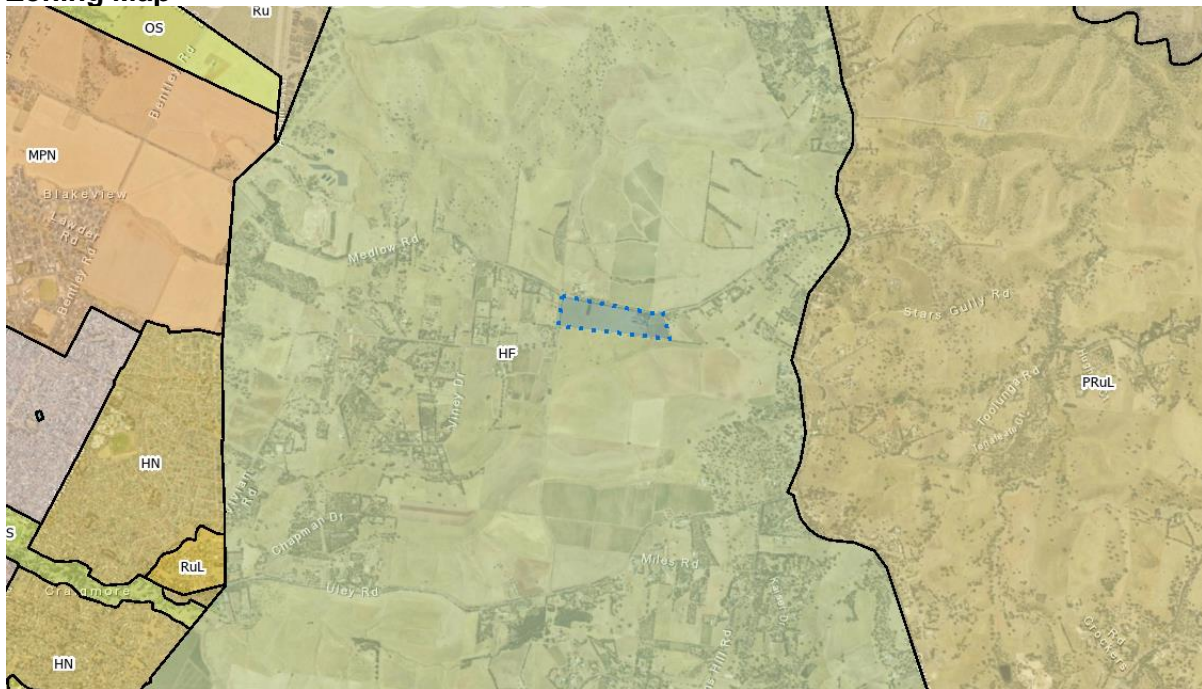
The subject land is located solely within the Hills Face Zone as identified within the Planning and Design Code.

The following Overlays relate to the subject land as per the Planning and Design Code:

Overlays-

- Defence Aviation Area
- Environment and Food Production Area
- Hazards (Bushfire - High Risk)
- Heritage Adjacency
- Native Vegetation
- Prescribed Wells Area
- Regulated and Significant Tree
- Water Resources

Zoning Map



Source: SAPPA

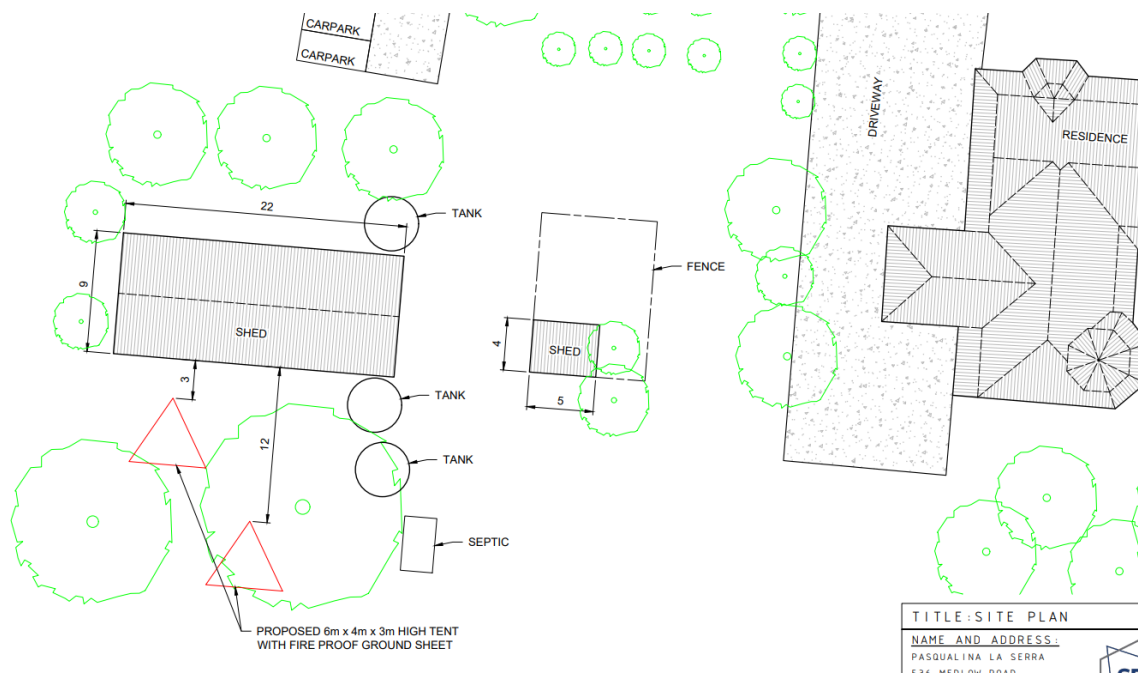
3. The Proposal

The proposal is for two (2) glamping tents to permanently sited on the property. The dimensions of the tents will be 6m long , 4m wide and 3m high (indicative tent design shown below). The proposal will accommodate 2 persons per tent with a maximum number of 4 visitors on site at any time.



Indicative Tent Design

The tent material is polyester cotton canvas with gauze windows and incorporate a zipped entrance. The tents will be placed on a fireproof ground sheet. The two tents will be located 3m and 12m respectively to the southern side of an existing shed



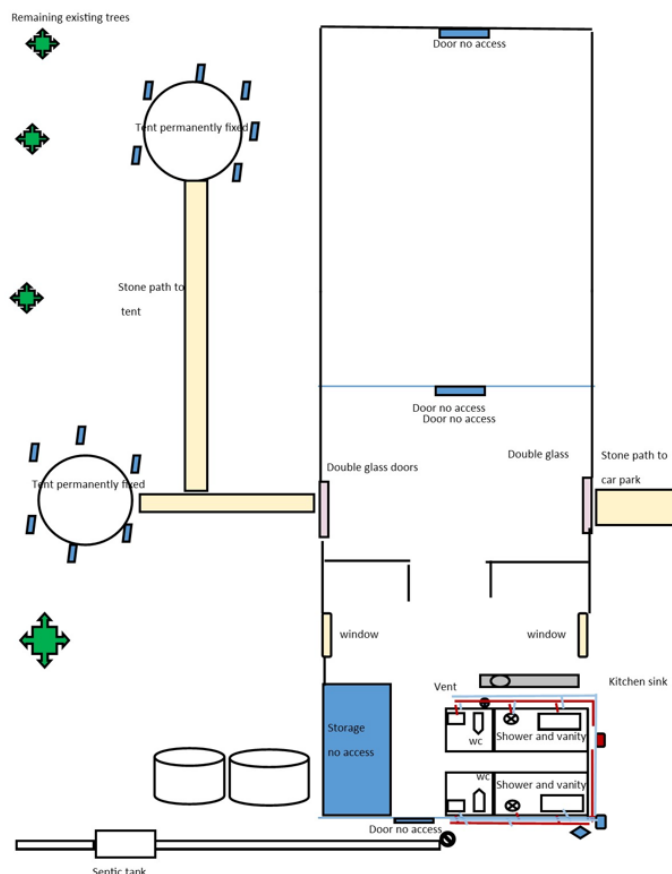
Tent Location Map

The proposal includes the partial conversion of an existing shed to provide storage and amenities (bathroom and toilet) for the use of visitors. Existing septic tank system and services (electricity and water) have been connected to the shed.

Four (4) car parking spaces are provided for visitors that are located between the existing sheds and the gate entrance and accessed via an existing driveway.

The proposal will not result in the removal or damage to any existing trees.

The tents will be located centrally on the allotment well away from any adjoining neighbours. The closest sensitive receptor is a detached dwelling located approximately 280m to the east being the adjoining property.



Tent and Amenities Location Map

4. Background

The application was lodged in 3 April 2022, with public notification undertaken 7 October – 27 October 2022. The initial round of notification provided two representations, one of which was in support and one which was not in support of the application.

Information was provided in support of the application to respond to this representation and to address Council's Request for Information. This additional detail introduced an additional element to the development, being the use of the existing shed as an amenities building. On consideration of the changes to the application and the additional element of the development, it was not considered that the element was sufficiently 'minor' to not be a notifiable form of development. A second round of notification was undertaken 21 February 2023 – 14 March which included the additional element, with 6 representations received. These representations are discussed in the below section.

5. Procedural Matters

5.1 Classification

The proposed development is considered all other code assessed development pursuant to Section 105 & 107 of the *Planning Development and Infrastructure Act 2016*, requiring an on-merit assessment against the relevant provisions of the Code.

Within the Hills Face Zone, tourist accommodation (comprising two (2) glamping tents, conversion of a portion of the existing shed to accommodate amenities including kitchen and bathroom facilities) and car parking are listed as a Performance Assessed forms of development.

5.2 Public Notification

All classes of performance assessed development require public notification unless pursuant to Section 107(6) of the *Planning Development and Infrastructure Act 2016*, the class of development is excluded from notification by Table 5 of the Procedural Matters Section of the relevant Zone of the *Planning and Design Code*.

In Table 5 of the Procedural Matters Section of the Hills Face Zone as the proposal for Tourist Accommodation is not listed in Column A.

As detailed above, the initial round of public notification was undertaken 7 October – 27 October 2022 with two representations received. One representation was in support and one was not in support.

The second round of public notification commenced on 21 February 2023 and concluded on 14 March 2023. Six (6) representations were received during the public notification period. Five representors do not support the proposed development and one person has elected to be heard by the panel. There is one representor who supports the proposed development.

The representations are listed below:

Representor	Issues Raised	Wishes to be Heard
Liane Lawrence	Does not with the policies of the Hills Face Zone Additional road and driveway required to access new facility Proposal does not preserve biodiversity and restore local indigenous vegetation and fauna habitats	No
Sandra Harbison	Noncomplying form of development. Considers the proposal to be classified as a Restricted Development Development will result in trespassing into adjoining rural properties and create fire risk Fires safety concerns with the adequacy and siting of refuge building and the operation of the sprinkler system.	Yes
Michael Baun	No maximum occupancy recorded Haven for criminal activity Pets will be smuggled in and cause death to wildlife Fences and personal	No

	property of nearby properties will be damaged	
David Carter	<p>Noise, pollution, stray animals attacking farm animals</p> <p>Roadway too narrow for proposed development</p> <p>Unmonitored entry / exit thoroughfare.</p> <p>Hygiene problems.</p>	No
Ryan (unknown)	<p>Traffic will increase on an already high traffic bad road</p> <p>Damage to fences on neighbouring properties due to people wanting to pat the animals.</p> <p>High safety fire risk.</p> <p>Unwanted noise in the area.</p> <p>Considers that work has started without approval.</p> <p>Proposal may result in caravans utilising facility.</p>	No
Nicola Pipicella	<p>Planning consent should be granted</p> <p>Proposal would be a peaceful retreat for people who would like a nice quiet getaway.</p> <p>Two glamping tents will not impact on anyone's livelihood in the area and will complement the already existing hospitality businesses nearby.</p>	No

The Applicant responded to the representation received and it is summarised as follows:

- An existing separate driveway has existed on the property (established 1998) as the subject land was previously used for horse agistment.
- The proposed development will not generate high levels of traffic (maximum of 4 persons on site at any time).

- The existing stables already had bathroom facilities and have been upgraded by the applicant.
- The tents will be located centrally on the allotment that is fenced and well away from any adjoining neighbours.
- Guests will not have access to neighbouring property fences when on site as they are contained to the fenced tourist site.
- There will only be a maximum of 4 people at one time
- The land use of the development is considered to be Tourist Accommodation and is not a Caravan Park

No Amendments to the plans were made in response to the representation.

5.3 Statutory Referrals

The proposed development required a referral to the Country Fire Service (CFS) as the development is located within the Hazards (Bushfire – High Risk) Overlay pursuant to Schedule 9(3)(2) of the *Planning, Design and Infrastructure Regulations 2017*.

A referral to the CFS was issued on 6 April 2022 and a response received 17 August 2022. Advice returned indicated the CFS has no objection to the proposal and has directed conditions be attached to any approval.

5.4 Internal Referrals

The proposed development has also been referred to Council's internal engineering and health teams.

Council's engineering team have considered stormwater and internal traffic movements on site. Stormwater design has generally appropriate discharge locations which would meet the requirements of Council standards. Traffic comments pertained to parking layout and waste vehicle movement which have been considered appropriate with labelling on parking spaces (e.g Staff).

Health team were consulted regarding waste and sewerage requirements for the allotment and have worked with the applicant to obtain necessary approvals for alterations to the existing system. This approval has been issued by Council's EHO team in November 2022.

6. Key Issues

The following matters are considered pertinent in reaching a recommendation for the proposal:

- Whether the proposal is an appropriate form of development in the Hills Face Zone
- Whether the proposal will impact on existing traffic movements in the locality
- Whether the proposal will generate high levels of noise or other interface issues with adjoining properties.

7. Planning Assessment - Zone

HILLS FACE ZONE

The Hills Face Zone seeks to maintain the western slopes of the South Mount Lofty Ranges as an important natural asset of Greater Adelaide by limiting development to low-intensity agricultural activities and public and private open space (DO1) and preserve and enhance the natural character of the zone.

The subject land is an open rural property that contains a single storey detached dwelling located to the eastern portion of the allotment. It is considered that the proposal for two permanent glamping tents with associated amenities and parking being a low scale development will maintain the natural character of the local area and supports DO1 of the Zone.

7.1 Land Use and Intensity

The proposal is for a tourist accommodation land use activity that includes two individual glamping tents with associated amenities with an existing shed and car parking.

The development is of low intensity and scale activity that will complement the rural and scenic qualities of the hills face landscape by accommodating a small number of tourists to this Hills region that includes wineries and other tourist attractions.

The development will have access to existing services and infrastructure with amenities for tourist (water, electricity, sewer) provided by the applicant resulting in no additional services at a cost to the community.

The proposed land use is considered appropriate in the Hill Face Zone and satisfies PO 1.1, PO 3.1 and is consistent with DO 1.

7.2 Built Form and Character

The proposed tents are limited in height and scale and represent an unobtrusive development being sited to the southern side of an existing shed and thus not easily viewed from Medlow Road.

The tents being 3 metres in height reflect a single storey building height that is limited in height when compared with the existing shed and dwelling which display heights of 4 metres and 6 metres respectively.

The tents will be located centrally on the allotment well away from any adjoining neighbours and will limit any visual intrusion when viewed from other local roads or from the Adelaide Plains. The closest sensitive receptor is a detached dwelling located approximately 280m to the east being the adjoining property.

The tents will be constructed of polyester canvas material in a 'cream' colour that is a low light and not reflective that can blend with the natural and rural landscape of the area.

It is considered that the proposal satisfies performance outcomes of PO 2.1, PO 2.2, PO 2.3 and PO 2.9 and the performance features set out in DTS/DPF 2.2.

7.3 Excavation and Filling

As the proposal is located to a flat portion of land on the allotment there will be minimal requirement for any excavation it satisfies the performance outcomes of PO 3.2.

7.4 Tourist Development

The proposal for two tents and the partial repurposing of an existing shed for amenities and storage is of low intensity and scale. Along with the tents being located behind a shed when viewed from Medlow Road, it is considered that the proposal meets with PO 7.1.

7.5 Driveways, Access Tracks and Car Parking

The proposal includes the formalisation of an existing driveway that will be approximately 35m in length from Medlow Road to the parking area. The existing driveway traverses a flat portion of land that will not result in any significant earthworks or removal of vegetation and therefore has no impact on the landscape character of the area. It is considered that the proposal supports PO 8.1 and the performance requirements of DTS/DPF 8.2.

Vehicle access will be provided via an existing 6.2m wide crossover from Medlow Road. The proposed access is considered appropriate and consistent to providing vehicular access for a small number of tourist accommodation visitors as would be expected with the proposed development.

7.6 Environment and Amenity

The proposal is of low scale and intensity with a small built footprint that will not result in any adverse impacts on the local landscape. The development does not involve the removal of native vegetation or impact on any known water sources. The development will be located on a flat portion of land and will be obscured behind an existing shed when viewed from Medlow Road. These attributes satisfy PO 10.1 and PO 10.2.

7.7 Native Vegetation

The proposed development will not result in the damage or loss to any known native vegetation and seeks to retain all existing trees on the subject site. These attributes satisfy PO 11.1, PO 11.2 and PO 11.3

8. PLANNING ASSESSMENT - OVERLAYS

8.1 Defence Aviation Area (All structures over 15 metres)

The proposal for two glamping tent structures will not impact on the Defence Aviation Area.

8.2 Environment and Food Production Area

The proposal does not result in the division of land and not adversely encroach on the rural locality.

8.3 Hazards (Bushfire – High Risk)

It is considered that the proposal for two glamping tents, vehicle parking and the provision of amenities to an existing shed have been designed and configured to limit the impact of bushfires.

The proposed development will not result in a significant increase of activity on the subject land that would impact on the ability to evacuate the subject land and the ability of firefighting vehicles to access the site.

There is suitable flat and open land around the glamping tent site that would enable firefighting vehicles to enter and exit the driveway in a forward direction and move around within the site.

The site of the proposed development has a clear and unobstructed pathway via the driveway to Medlow Road, which is a formed as an all-weather surface, 20 metres in length and on a generally flat portion of land.

The Country Fire Service (CFS) were issued with a referral in accordance with Schedule 9(3)(2) of the Planning, Development and Infrastructure (General) Regulations 2017 to provide expert assessment and direction on the potential impacts of bushfire on the development. The CFS raised no objection to the proposed development and directed conditions be attached to any approval.

In their response the CFS has recommended the following:

1. The applicants are to prepare and display a BUSHFIRE SURVIVAL PLAN (BSP) designed specifically for the purpose of any guests that may be in residence during a bushfire event, especially during the Fire Danger Season
 - should provide clear directions to persons that may be unfamiliar with the area/locality and unfamiliar with what protective actions they may need to take to protect their lives during a bushfire event, including when to take such protective actions
 - address the possibility that the owners may not be present at the time of the bushfire event; and
 - should not expect guests to be involved in fire-fighting operations.
2. The applicant should consider reducing operating hours and restrictions on days of heightened bushfire danger and/or bushfire events and consider including any alterations to bookings and services offered due to actual or predicted conditions during the Fire Danger Season in any booking/refund policy.

8.4 Heritage Adjacency

The site of the glamping tents is located outside of the Heritage Adjacency Overlay map and is considered not to impact on the setting of Local Heritage Places in the local area.

8.5 Native Vegetation

The applicant has declared in their submission that the proposed development does not result in the damage or loss to any known native vegetation on the subject land.

8.6 Prescribed Wells Area

The proposal for two glamping tent structures will not impact on any known wells.

8.7 Regulated and Significant Tree

There are no known Regulated or Significant Trees located on the subject land.

8.8 Water Resources

The proposed development is not located close to any dams or watercourses.

9. General Development Policies

Tourist Development

The proposed development provides for tourist accommodation that positively contributes to the local winery and tourist region of Uleybury and Yattalunga. The development represents a small tourist accommodation opportunity that has been appropriately located to minimize any impacts to the natural environment and the local character. It is considered that it satisfies the performance outcomes of the Tourist Development Module.

Transport, Access and Parking

The proposal supports the safe and convenient access of vehicles movements and minimises any adverse impact or interruption to Medlow Road and local road network.

Table 1 – General Off-Street Car Parking Requirements of the Planning and Design Code stipulates 1 car parking space per unit/guest room. Therefore, the proposal requires a minimum of 2 car parking spaces.

The proposed development includes 4 car park spaces and thus meets with the requirements Table 1 for the intended use.

10. Conclusion

The proposed development is considered to be consistent with the desired outcomes of the Hills Face Zone, relevant overlays and generally accords with the relevant general provisions.

The proposed land use is considered appropriate in the context of the subject site. The development comprising two glamping tents and associated amenities that are sited unobtrusively will provide tourist accommodation that is of a low intensity and scale in this region.

The assessment of the proposal required a referral to the CFS for an assessment of the potential bushfire issues. The CFS reviewed the application and provided relevant comments relating to the management and operation of the tourist accommodation.

The proposed development is not considered to generate any adverse impact to the local natural environment or local traffic network. The proposal presents a low impact activity that will not negatively impact on the scenic rural landscape and can be appropriately managed not to impact on adjoining neighbours and not present a potential bushfire risk.

As such, it is considered that the proposal satisfies the overall intent of the Planning and Design Code and is recommended for approval.

11. Recommendation

STAFF RECOMMENDATION

That pursuant to the authority delegated to the Council Assessment Panel by the Council, it is recommended that the Council Assessment Panel:

- a. DETERMINES that the proposed development is not seriously at variance with the policies in the Planning and Design Code; and
- b. GRANTS Planning Consent to the application by Pasqualina Le Serra for Tourist Accommodation comprising two glamping tents, conversion of a portion of the existing shed to accommodate amenities (kitchen and bathroom facilities) and car parking at 536 Medlow Road, Uleybury as detailed in Development Application ID 21032783 subject to conditions:

Conditions

Council Conditions:

1. The development must be undertaken, completed and maintained in accordance with the plan(s) and information detailed in this Application except where varied by any condition(s) listed below.

Reason: To ensure that the development is constructed and operated in accordance with the plans and details provided.

2. The tourist accommodation comprising of two glamping tents herein approved shall only be used for tourist accommodation, and any alternative land use will require a separate approval.
3. All waste and rubbish shall be stored in covered containers prior to removal and shall be screened from public view.
4. All external lighting of the site, including, but not limited to car parking area, tents and amenities building shall be designed and constructed to conform with Australian Standards and must be located, directed and shielded and of such limited intensity so as to not, in the opinion of the relevant authority, create unreasonable overspill onto any adjoining property or roadway which may create a nuisance to any neighbour or road user.
5. The nominated on-site car parks are available at all times for visitor car parking.
6. Car parking areas, driveways and vehicle manoeuvring areas shall be maintained at all times to the reasonable satisfaction of the relevant authority.
7. All storm-water drainage shall discharge so that it does not flow or discharge onto land of adjoining owners or, in the opinion of the relevant authority, detrimentally affect structures on this site, any adjoining land or public road.

Country Fire Service:

8. Vehicular access to serve the site shall be located in accordance with the Site Plan by On Architecture, Drawing Number DA02, date 20/01/2022.
9. All vehicles shall enter and exit Heaslip Road in a forward direction. All on-site vehicle manoeuvring areas shall remain clear of any impediments.
10. The illuminated signage shall be limited to a low level of illumination so as to minimise distraction to motorists ($\leq 150\text{cd/m}^2$).
11. Stormwater run-off shall be collected on-site and discharged without impacting the adjacent road network. Any alterations to the road drainage infrastructure required to facilitate this shall be at the applicant's cost.

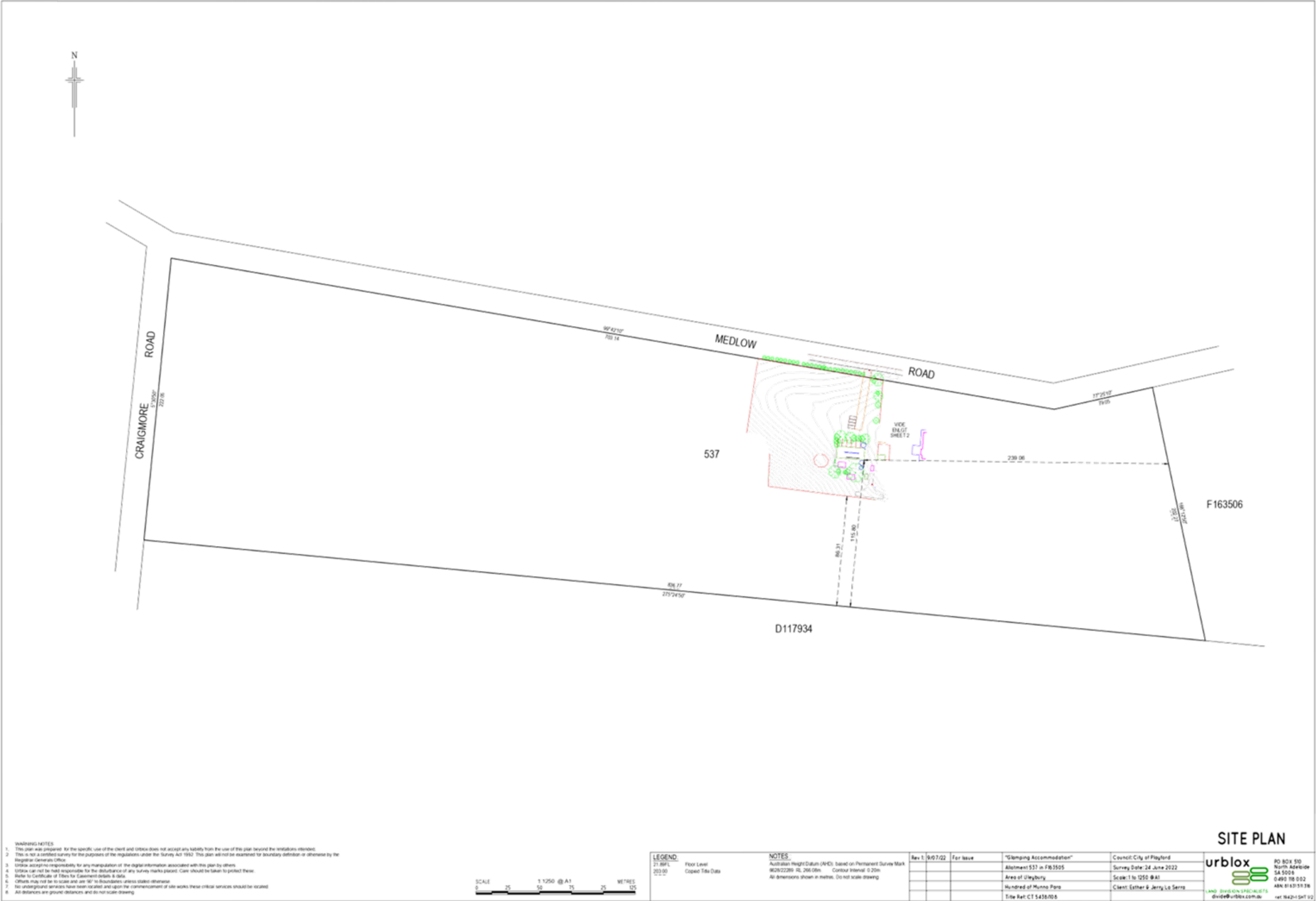
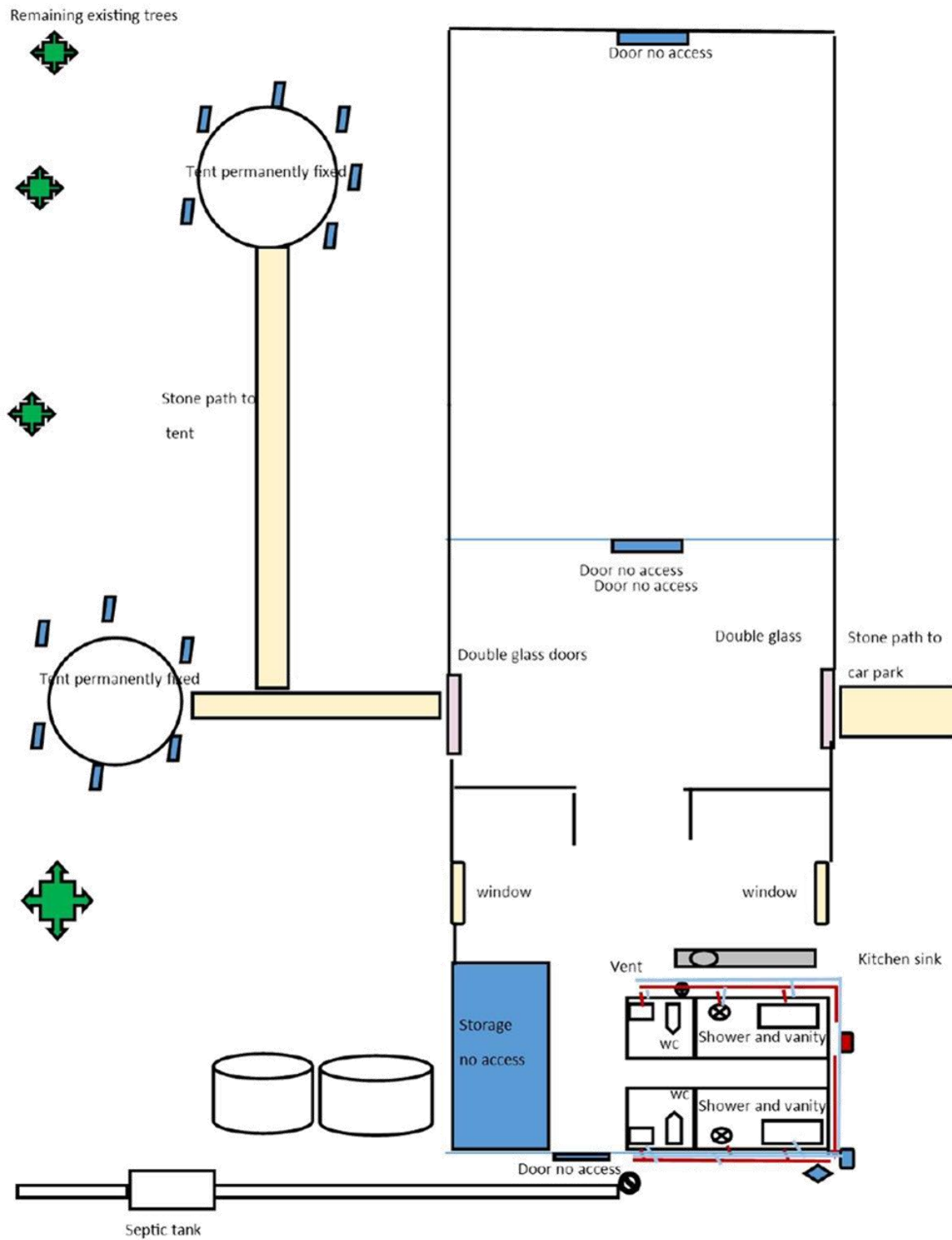




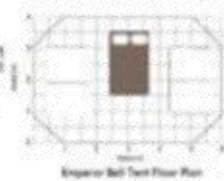
Diagram of shed /stable with ammenities







Product size: 500*400*300/80 cm (l*w*h) incl height
 Package size: 120*30*30cm
 Wall and floor connect with zipper
 Fabric: 300gsm polyester cotton canvas, PU coated,
 UV resistant, 1500 mm waterproof
 Door: 1*door in middle with zipper grommet, 180*130cm (W*H)
 Window: 8 zipper windows with grommet (grommet without zipper)
 Side windows: 32*10cm, 1 window on each side
 Front and back window: Arch window 40*30cm (W*H)
 2 in front & 2 in back
 Vents: 4 vents at foot top
 Floor: PVC
 Pole: Dia 16mm, aluminum
 Stripes: 28mm
 Rope: 1.5cm*11gsm, 5m*2gsm
 Gross weight: 150kg



Exterior Bell Tent Floor Plan



Things to know

Check-in: 2:00 pm

Checkout: 10:00 am

Self-check-in with key safe

No parties or events

Pets are not allowed

Smoking is not allowed

Maximum people per tent is 2

Bookings are available for midweek and weekend stays

Prices are per night

Cancellation Policy

Cancellations providing more than 21 days' notice prior to arrival date are refundable.

All Cancellations within 14 days of the scheduled arrival date will result in no refund. Reservations altered or cut short during occupancy, do not qualify for any refund

Details of Representations

Application Summary

Application ID	21032783
Proposal	Tourist Accommodation - Two Glamping Tents
Location	536 MEDLOW RD ULEYBURY SA 5114

Representations

Representor 1 - Nicola Pipicella

Name	Nicola Pipicella
Address	1054 Gawler-One Tree Hill Rd ULEYBURY SA, 5114 Australia
Submission Date	12/10/2022 12:27 PM
Submission Source	Online
Late Submission	No
Would you like to talk to your representation at the decision-making hearing for this development?	No
My position is	I support the development
Reasons I support people trying to make a living and creating a positive, happy, relaxing getaway for people who need it.	

Attached Documents

Representations

Representor 2 - Sandra Harbison

Name	Sandra Harbison
Address	454 KENTISH ROAD ULEYBURY SA, 5114 Australia
Submission Date	28/10/2022 03:42 PM
Submission Source	Email
Late Submission	No
Would you like to talk to your representation at the decision-making hearing for this development?	Yes
My position is	I oppose the development
Reasons	

Attached Documents

Representation_on_application_-_performance_assessed_development-4165004.docx

REPRESENTATION ON APPLICATION – PERFORMANCE ASSESSED DEVELOPMENT

Planning, Development and Infrastructure Act 2016

Applicant:	Pasqualina La Serra, Gilardo La Serra <i>[applicant name]</i>
Development Number:	21032783 <i>[development application number]</i>
Nature of Development:	Tourist Accommodation – Two Glamping Tents <i>[development description of performance assessed elements]</i>
Zone/Sub-zone/Overlay:	Hills Face Zone <i>[zone/sub-zone/overlay of subject land]</i>
Subject Land:	536 Medlow Rd Uleybury SA 5114 <i>[street number, street name, suburb, postcode]</i> <i>[lot number, plan number, certificate of title number, volume & folio]</i>
Contact Officer:	Karen Mitrovic <i>[relevant authority name]</i>
Phone Number:	82560350 <i>[authority phone]</i>
Close Date:	27/10/2022 <i>[closing date for submissions]</i>

My name*: Sandra & William Harbison	My phone number: 0428824219
My postal address*: 454 Kentish Road Uleybury	My email: harbison2@bigpond.com

* Indicates mandatory information

My position is:	<input type="checkbox"/> I support the development <input type="checkbox"/> I support the development with some concerns (detail below) <input checked="" type="checkbox"/> I oppose the development
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The specific reasons I believe that planning consent should be granted/refused are:

After careful consideration of this proposal, we believe that planning consent should be refused for the following reasons:

1. *Non complying development in Hills Face Zone*
2. *This is a rural area used largely for primary production. We breed valuable live stock – cattle, sheep, horses – and believe that there will be potential for increased risk to them and our operation from the following-*
 - a. *Trespass from people unacquainted with livestock; gates being left open.*
 - b. *Fire risk from campfires or smoking with inadequate awareness of the huge danger that this may pose to people, property and livestock.*
 - c. *We question the adequacy of water supply to extinguish a fire should such occur.*



Government of South Australia
Department for Trade
and Investment

[attach additional pages as needed]

Note: In order for this submission to be valid, it must:

- be in writing; and
- include the name and address of the person (or persons) who are making the representation; and
- set out the particular reasons why planning consent should be granted or refused; and
- comment only on the performance-based elements of the proposal, which does not include the:
 - [Click here to enter text.](#) *[list any accepted or deemed-to-satisfy elements of the development]*.

I:	<input checked="" type="checkbox"/> wish to be heard in support of my submission*
	<input type="checkbox"/> do not wish to be heard in support of my submission
By:	<input checked="" type="checkbox"/> appearing personally
	<input type="checkbox"/> being represented by the following person: Click here to enter text.

**You may be contacted if you indicate that you wish to be heard by the relevant authority in support of your submission*

Signature: Sandra Harbison

Date: 27.10.2022

Return Address: [Click here to enter text.](#) *[relevant authority postal address]* or

Email: harbison2@bigpond.com *[relevant authority email address]* or

Complete online submission: planninganddesigncode.plan.sa.gov.au/haveyoursay/

Response to Representation

I am responding to Sandra Harbison of Kentish Road.

After my husband, Jerry's phone conversation with you asking for a valid reason why you oppose our only two tent glamping experience on our private property. Your reasoning was unjust in that you were afraid that "people wander around and leave gates open" Somehow you are under the impression that people coming to stay on our secured property would go to your paddock and release your horses. Our property is across the road to your paddock so who's to say any person travelling on Medlow road could have access to your gates if you have failed to secure it. The neighbour across from you on Kentish Rd runs a horse riding and accommodation business and we are assured that none of those guests have approached your property and left gates open. There are many existing businesses on Medlow Rd and we purchased this property as an agistment property approx. 24 years ago. At one time we had 35 horses on our property and never had any trouble with the public or any of the neighbours. We are concerned that you have opposed us because of a hear-say of separate intended proposal by your next door neighbour which is a separate case to ours.

Furthermore our glamping accommodation is for guests to experience sleeping in a luxury tent in natural surroundings. The guests would need to visit local wineries and restaurants as we do not provide any other activities on the property. Only relaxation and a sunset.

Details of Representations

Application Summary

Application ID	21032783
Proposal	Tourist Accommodation - Two Glamping Tents
Location	536 MEDLOW RD ULEYBURY SA 5114

Representations

Representor 1 - Nicola Pipicella

Name	Nicola Pipicella
Address	1054 Gawler-One Tree Hill Rd ULEYBURY SA, 5114 Australia
Submission Date	12/10/2022 12:27 PM
Submission Source	Online
Late Submission	No
Would you like to talk to your representation at the decision-making hearing for this development?	No
My position is	I support the development
Reasons I support people trying to make a living and creating a positive, happy, relaxing getaway for people who need it.	

Attached Documents

Representations**Representor 2** - Sandra Harbison

Name	Sandra Harbison
Address	454 KENTISH ROAD ULEYBURY SA, 5114 Australia
Submission Date	28/10/2022 03:42 PM
Submission Source	Email
Late Submission	No
Would you like to talk to your representation at the decision-making hearing for this development?	Yes
My position is	I oppose the development
Reasons	

Attached Documents

Representation_on_application_-_performance_assessed_development-4165004.docx

Response to Ryan Unknown.

Medlow Rd would not be considered a high traffic road. The impact that our Airbnb business would give to traffic would be 2 extra cars on any given day.

We have an existing separate driveway which has been there since well before we bought the property (1998) as it was a horse agistment. While we continued the horse agistment we had many horses through the years and up to 20 at any one time. There was never issues with the traffic or damage to fences or neighboring properties. The existing stables already had bathroom facilities we just upgraded them.

People will use our glamping site as a private getaway to relax and chill. The tents will be in a fenced off 5 acre paddock with no neighbours on any side. There will only be a max of 4 people at one time.

Response to David Carter

There will not be any noise that will disturb anyone with only two people in each tent at any one time.

There will be no pollution as the guest can only drive their car up the driveway to the designated car park.

We will not allow guests to bring in any animals or pets as this is glamping accommodation with high quality furnishings and we would like them to remain pristine.

Existing driveway is in a 5 acre paddock. We also have security cameras in place on the property and our gates are always locked.

The existing stables have always had bathroom facilities and all of this has been inspected by officers from the council.

I would be more concerned with the hygiene of the dump 1 km down the road.

A birthday party at ones house would have more traffic and noise than a couple coming for a quiet getaway on our property.

Response to Michael Baun

Maximum occupancy has been stated as 4 people. Two in each tent.

Guests will come to our glamping Airbnb for peace and relaxation.

There will be no access for guests to go near neighbours properties as they will be confined to a five acre paddock which there are no neighbours.

Anyone using the public road has access to neighbours, fences and properties.

Response to Liane Lawrence

Our glamping Airbnb has been designed specifically to not have any negative impact on any indigenous vegetation or fauna. We bought the property as a horse agistment in 1998 . Through the years many horse were in the paddocks what vegetation remains has not been touched by humans and will always remain there. Our two tents will complement the natural enviroment not destroy it. There is an existing driveway so no new road will be put in. Just another layer of rubble on the old one

Response to Sandra Harbison

I would like to make it clear that we will be running a Glamping Airbnb with only two tents. It is not a caravan park or a tourist park . An Airbnb for max 4 people at any one time. This property was an existing business when we purchased it 25 years ago.

The local area immediately around us has two thriving wineries and there are many airbnbs set up. There are many other businesses up and running on Medlow Rd and surrounds. Our two tent Airbnb will not have any negative impact on anyone in the area.

Guest staying in our tents will be in a secluded area where there is no access to neighbouring properties. There will be no campfires or smoking permitted.

CFS officers have inspected all our fire safety equipment and water sources.

APPLICATIONS FOR CONSIDERATION

**APPLICATIONS FOR
CONSIDERATION – NO PERSONS
TO BE HEARD**

6.1 CONSTRUCTION OF A BUILDING COMPRISING TWO BULKY GOODS TENANCIES WITH ASSOCIATED ADVERTISING AND RETAINING WALL (ID: 22039894)

Snapshot

Report Author:	David Storey – Consultant Planner
Assessing Officer:	Danni Biar
Proposal:	Construction of a building comprising two bulky goods tenancies with associated advertising and retaining wall
Development Number:	22039894
Date of Lodgement:	13 December 2022
Owner:	Leyton Property
Applicant:	Leyton Property C-/ Future Urban
Location:	Lot 10 Mingari Avenue, Munno Para
Zone:	Employment
Classification:	Performance Assessed
Public Notification Category:	Yes
Representation Received:	Yes
Request for Additional Information Made?	Yes
Recommendation:	To Grant Planning Consent
Attachments:	<div> <div>1 ↓.</div> <div>2 ↓.</div> <div>3 ↓.</div> <div>4 ↓.</div> <div>5 ↓.</div> <div>6 ↓.</div> <div>7 ↓.</div> </div> <div> <div>Planning Report</div> <div>Application Documents</div> <div>Traffic Report and Response to RFI</div> <div>Stormwater Management and Civil Plans</div> <div>Acoustic Report</div> <div>Representation</div> <div>Applicant's Response to Representation</div> </div>

1. The Subject Land

The subject land known as Lot 10 Mingari Street, Munno Para which comprises a single corner allotment that is bound by Curtis Road to the south, Mingari Street to the west and Myall Avenue to the north.

The land is currently vacant and is part of a wider development which includes a retail fuel outlet, shops in the form of bulky goods, restaurant and fast food outlet, carparking, advertising over stages (DA 21024444).

In addition, a land division to divide the land into 4 allotments has also been undertaken (DA 21035564). The sites within the wider development site are to be connected through a series of rights of way which were approved in the land division.

The development site (see locality plan below) is 4,016 square metres in area with a 79.83 metre frontage to Curtis Road and 50 metre frontage to Mingari Street.

The topography of the land is relatively flat as is the locality. The site contains stands of established trees located along the perimeter of the wider grouped allotments on the southern boundary of Curtis Road.

There are no Land Management Agreements or encumbrances registered on the subject land.

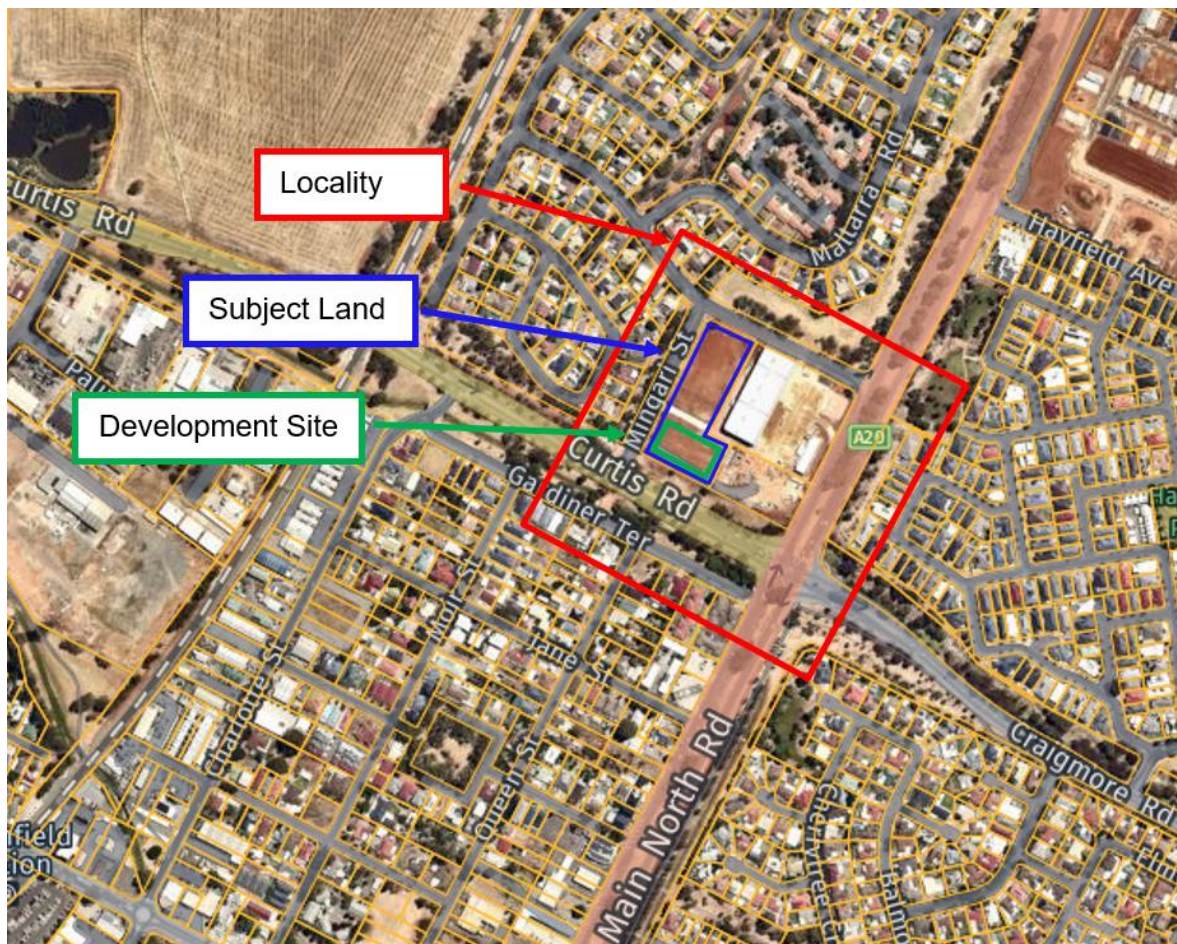
2. The Locality

The locality comprises a combination of low density established residential areas to the northwest and southwest, and more recent residential housing to the east and south east.

The subject land is surrounded by landscaped reserves that act as significant landscape buffers to the residential dwellings including well-established trees along the road reserves of Curtis Road and Main North Road and through the medium strip separating the north and south bound traffic on Main North Road.

On the eastern side of Main North Road is a well landscaped dog park and landscaped reserve and the new housing development of Blakeview.

2.1 Locality Plan



2.2 Zoning

The subject land is located within the Employment Zone as identified in the Planning and Design Code.

The land is entirely within:

3. Background

The subject land was previously known as the Munno Para Bowls Club until its amalgamation with Elizabeth Bowls Club. The Clubs are now known as Playford Bowling Club Inc. sharing the facilities at Goodman Road, Elizabeth since the opening on 6 June 2018.

Given Council are the current owners of the land, Council staff identified a perceived conflict of interest in assessing the application, as Council was the previous owner of the land and has a financial interest in the land. A letter was previously sent to the Minister for Planning to request that the State Planning Commission be appointed as the relevant authority for the application for the original application, DA 21024444. Council was advised that, in that instance, it was not warranted for the State Planning Commission to act as the relevant authority and that the matter should be decided by the Council Assessment Panel.

The same approach of Council Assessment Panel determining the application has been adopted for this proposal, in line with the previous direction provided by the Minister of Planning.

4. The Proposal

The Applicant seeks consent for the following:

- Two Bulky Goods tenancies comprising of:
 - 1849 sqm total footprint
 - Tenancy 1 – 1500 sqm
 - Tenancy 2 – 363 sqm
- Associated retaining wall
- Advertising
 - installation of one freestanding pylon sign with internally illuminated panels
 - installation of six advertising wall signs on the building, one of which is internally illuminated

The main building (bulky goods outlet) is single level and has a height of 6.9 metres above the finished floor level. The advertising display sits atop the building to reach a total height 10 metres. The building will be setback 8.86 metres from Mingari Street, 19.8 metres from Curtis Road, 2.74 metres from the eastern side boundary and sited on the boundary to the right of way easement at the rear.

Each tenancy is designed with a corporate sign above their respective front entrance, one of which is proposed to be internally illuminated (RSEA). Five additional non-illuminated signs are proposed on the building including two of which rise above the roof and address the south and east.

An 8-metre-high freestanding pylon advertising sign is proposed on the corner of Mingari Street and Curtis Road and will display the corporate logos for the two future tenancies. It will be internally illuminated.

A copy of the architectural drawings depicting the above are contained within Attachment 1.

5. Procedural Matters

5.1 Classification

The proposed development comprises of a shop (bulky goods outlet), retaining wall and advertising.

The above elements are not classified as an Accepted, Deemed-to-Satisfy or Restricted development within the relevant Tables of the Zone. The proposed development is therefore a Code Assessed - Performance Assessed development pursuant to Sections 105(b) and 107 of the Act, requiring an on-merit assessment against the relevant provisions of the Code.

5.2 Public Notification

Generally, all classes of performance assessed development require public notification unless, pursuant to Section 107(6) of the Act, the class of development is excluded from notification by the Code in Table 5 – Procedural Matters (PM) - Notification of the relevant Zone.

Public notification was required as the following elements did not meet the requirements of Table 5:

- The proposed development exceeds the maximum building height specified in Zone DPF 3.5 of 9 metres.

The notification process commenced on 31 January 2023 and closed on 20 February 2023 and Council received the following representation:

Representor	Summary of Issues Raised	Wish to be heard
Carol Bennett, 3 Myall Avenue, Munno Para SA	<ul style="list-style-type: none">• Development will result in additional traffic movements in the locality.• Will contribute to further congestion along Curtis Road.	No

The Applicant's Planning Consultant has responded to the representation.

The representation received is contained within Attachment 6 while the Applicant's Planning Consultant's response is contained within Attachment 7.

5.3 Statutory Referrals

Pursuant to Schedule 9 of the Regulations, the Commissioner of Highways received a referral for this development application. Their response is as follows:

Agency	Referral Response
Department of Infrastructure and Transport	Support the proposal with conditions (contained within the Recommendation).

5.4 Internal Referrals

Internal referrals were undertaken to Council's engineers to review traffic management, car parking, access and stormwater. Internal comments were included in the request for information and have been resolved in the amended plans save for technical details

relating to the stormwater plan, which is detailed within the recommendations of the report. The traffic, access and car parking matters are discussed in detail below.

6. Key Issues

The following matters are considered pertinent in reaching a recommendation for the proposal:

- Whether the proposal is an appropriate form of development in the Employment Zone
- Whether the proposal is consistent with the general policies of the Planning and Design Code that relate to Bulky Goods Outlet, Retaining and Advertising
- Whether the proposal will create an adverse impact and conflict between other land uses within the locality
- Whether the development will create an adverse traffic impact on the existing road network in the locality.

7. Planning Assessment

7.1 Desired Outcome and Land Use

The subject land is located within the Employment Zone (Zone). The Desired Outcomes (DO) for the Zone encourages a 'diverse range of light industrial and business activities that complement the role of other zones accommodating significant industrial, shopping and business activities...and...distinctive building, landscape and streetscape design to achieve high visual and environmental amenity particularly along arterial roads.

Generally, the Zone supports a variety of commercial and light industrial development that achieve high visual and environmental amenity particularly along arterial roads zone boundaries and public open space. Whilst the Desired Outcomes facilitate a focus on the above-mentioned land uses, retail and commercial land uses are also permitted where appropriate.

The proposal, consisting of a bulky goods outlet, retaining and associated advertisements provides what is considered to be appropriate land use when design and traffic aspects are appropriately considered and addressed. Zone Designated Performance Feature (DPF) 1.1(a) and (k) supports the elements proposed in this application. Further, Zone Performance Outcome (PO) 1.2 encourages shops in the zone and DPF/DTS 1.2(b) identifies bulky goods outlet as an appropriate development.

The Zone seeks to ensure that any proposed bulky goods outlets are conveniently located to local communities. The position of the proposed development located near to the intersection of Main North Road being a State maintained arterial road and Curtis Road is well suited to this location and supports DPF/DTS 1.4 that requires that any bulky goods outlet is located with a frontage to a State Maintained Road.

The proposed land uses are further supported on the basis that the location and design of the development does not prejudice the development of adjoining land in a manner envisaged with the relevant zoning. It achieves this by presenting an attractive architectural design that is well sited, with ample car parking and provides suitable access for vehicles to enter and exit from Mingari Street and dedicated pedestrian pathways within the subject site.

It is considered that the proposed development does not adversely impact on Retail Activity Centre Subzone and the Concept Plan 11 – Munno Para as it presents a

relatively small hub of retail activity that compliments surrounding businesses in the locality and the broader development of the site. Further it is located well away from designated Suburban Activity Centres identified for the Munno Para locality.

Based on the above, it is considered that the proposed land uses are not only supported in the Zone but further do not detract from the viability of nearby Suburban Activity Centres, meeting the requirements of the relevant Desired Outcomes and Performance Outcomes.

7.2 Built form and Design

Design

The Employment Zone encourages '*distinctive building, landscape and streetscape design to achieve high visual and environmental amenity particularly along arterial roads, zone boundaries and public open spaces*', as per Zone DO 2.0 and PO 2.1.

It is considered that the proposed plans suitably address the intent of the Desired Outcome in relation to design as the articulation of built elements displayed to the elevations provide sufficient visual interest and finishes through the use of a mix of materials in finishes that present to the frontages of public roads, specifically the frontage to Mingari Street.

The proposed materials consist of precast wall panels in a combination of colours including 'Black', 'Basalt' and a 'Bright Yellow' that present a typical bulky goods identification display that seeks to achieve high visibility.

Following a review of the plans and documentation, Council staff requested the applicant provide an amended plan to show additional articulation to the northern elevation (requested 20 January 2023). The applicant agreed to the request and provided the amended plans on 16 March 2023 that provides additional architectural line or channel treatments through the cladding walls.

The proposal will include a retaining wall to the northern elevation with 1000mm high balustrade to ensure a consistent floor level to the rear loading area of the building.

Setbacks

With regards to setbacks and the presentation of the built form to street frontages there are no existing or approved developments which address Mingari Street as the primary street and therefore no existing pattern of setbacks.

The corresponding designated performance feature to PO 3.1 (DPF 1.1), states that the primary street setback should be no less than 3m where no building exists on an adjoining site with the same primary street frontage. Since there are no existing buildings on adjoining sites, the 8.9 metre primary street setback provided is considered to be appropriate, especially noting the space from the nearby residential uses and the more recently approved buildings to the north.

The secondary street is separated from the subject site by approximately 27 metres, with a large number of established trees in place which work to reduce visual prominence of the building and improve the overall appearance when viewed from the secondary street.

The proposed building has been sited to allow access for loading and deliveries to the rear of the building as well as a 2.8 metre side setback to allow access for maintenance and emergency purposes.

Height

Two advertising displays are attached to the building exceeding the overall roof height of the building, reaching a height of 10 metres above the finished floor level.

Although exceeding DPF 3.5 pertaining to building height (max building height - 2 building levels up to a height of 9m), in the context of the locality, the proposed height is not considered to be of a detriment especially due to the distance from residential properties and the extensive Council verge from the main street.

Based on the above, it is considered that the development provides distinctive buildings of appropriate scale for the land use and location. The built form comprises of a variety of building materials with visual interest and a scale that is appropriate within the Zone.

7.3 Advertisements

Advertising is proposed at the entrance for each of the tenancies as well as three additional signs on the building, two of which rise above the roof line of the building.

A single free standing pylon sign at the southwest corner is also proposed, reaching a total height of 8 metres. It will involve internally illuminated corporate advertising and located at the south-western corner of the subject site.

The tenancy advertisements:

- will not flash or be animated;
- will convey the name of the prospective operator and relate, therefore, to the intended uses of the site; and
- feature corporate colours.

With these attributes it is considered most unlikely to distract motorists from their primary task or detract from the external appearance of the proposed buildings in this context.

The proposed pylon sign, although exceeding Zone DPF 6.1 in relation to overall height, is not considered to be visually dominant within the locality (per Zone PO 6.1), as:

- it is setback approximately 26 metres from Curtis Road due to the large verge; and
- the remainder of the subject land includes various pylon signs, reaching a total height of 10 metres.

The location and illuminated nature of the signage has been assessed by DIT and appropriate conditions would be included in relation to the luminance levels.

On balance, the proposed signage is considered to be appropriate in context to the land use and is not considered to unreasonably impact or proliferate the signage in the locality.

7.4 Interface Between Land Uses

The subject site is located approximately 35 metres from residential properties within the nearby Master Planned Neighbourhood Zone to the north-west and is separated by Mingari Street, the verge, and a reserve with established mature trees.

DO 1 of the Interface between Land Uses Module advises: *DO 1 Development is located and designed to mitigate adverse effects on or from neighbouring and proximate land uses....* Due to the separation distance of 35 metres and land features between the proposed development and adjoining residential properties it is considered there to be no adverse interface issues.

To address the impact issues of the proposed development the applicant engaged with an Acoustic consultant and a Traffic Consultant to undertake an assessment of noise and traffic management issues.

Noise

An acoustic report prepared by Echo Acoustic Consulting (Attachment 5) advises that the proposed development achieves the *Environment Protection (Noise) Policy 2007* without any specific acoustic treatments to the built form.

The Report found that no unreasonable adverse impacts to neighbouring land uses are anticipated due to separation distance from the nearest residential developments and siting of the entrances and carpark of the bulky goods outlets away from the nearest sensitive receivers.

7.5 Traffic Impact, Access and Car Parking

Access and Layout

An assessment of the access and traffic management issues of the proposed development is found in the traffic impact assessment report prepared by MFY consultants (dated 25 November 2022) which concludes the access points and movements through the site are considered to be safe and convenient.

MFY Pty Ltd advised in their report that the proposed development will be developed within an integrated site but will independently cater for the anticipated peak parking demand. The land use will be complimentary to the approved development as it relates to traffic and parking criteria.

Connectivity with the adjacent parking areas will increase opportunities for access to the site with the layout design facilitating pedestrian, cyclist and vehicle connectivity between uses on the site, thus providing for shared trips and reduced circulation on the external road network and the proposed car park will satisfy the design requirements of AS/NZS2890.1:2004.

Traffic Impact

Council had a number of queries in relation to traffic impacts resulting from the proposed development, which were clarified within the applicants Response to Further Information by MFY Pty Ltd. In addition, the applicant responded to the traffic impact concerns raised during the public notification process.

From reviewing the traffic impact assessment report, the site has been designed such that all movements can enter and exit in a forward direction to ensure the safety of vehicle users and minimise interference with the traffic flow of Curtis Road and the nearby State maintained – Main North Road (Major Urban Transport Routes Overlay PO 1.1).

The traffic generation associated with the overall development is considered to be within the capacity for the various access points and surrounding roads, in line with Traffic Generating Development PO 1.2 and the design allows service and delivery movements by commercial vehicles to be undertaken separately from areas associated with domestic vehicles (Transport, Access and Parking module PO 1.3).

Parking

The Code designates a vehicle parking rate of 2.5 spaces per 100 square metres for land uses classified as 'shop...in the form of a bulky goods outlet...' as identified in *Table*

1 - General Off-Street Car Parking Requirements. Based on this rate, there is a theoretical parking demand of 47 spaces.

The proposal provides a total of 48 spaces, thus surpassing the minimum carparking requirements sought within the Zone.

The parking areas have been designed to satisfy the policies of the Transport, Access and Parking module as vehicle movements between the different parking areas is done from within the subject site and does not require the use of external public roads to do so (PO 6.1).

In addition, a loading area is provided to the side of the site, with delivery and service vehicles able to safely utilise without the need for on-street collection (PO 6.6).

Based on the above, Council staff are satisfied that the proposal provides adequate on-site car parking when assessed against the Code. The proposed access arrangements are supported by DIT and Council's traffic engineer. The proposal will have an acceptable impact upon the adjoining road and accordingly, it is considered that the proposal satisfies the relevant traffic, access and car parking provisions of the Planning and Design Code.

7.6 Landscaping

A landscaping plan (shown in Appendix 1 of the Planning Report) has been prepared for this project which comprises landscaping strips to the primary and secondary street frontages. A wide range of plant species have been selected including small trees, medium trees and ground covers. The existing established trees which surround the site are to be retained, further contributing to the amenity of the site and locality.

It is considered that there is an appropriate level of existing landscaping to assist in softening the hardstand areas and to enhance the visual appearance of development and overall amenity of the site and locality, satisfying Zone Performance Outcome 5.1 and 5.

It is confirmed there are no Regulated or Significant Trees on the subject site.

7.7 Stormwater

PT Design were engaged to ensure stormwater is able to be appropriately managed as a result of the proposed development.

The calculations and civil plans confirm that:

- the development will be designed to ensure that the post-development discharge flows do not exceed the pre development discharge flows;
- the proposed floor level is greater than 300mm above street level (PO 1.1 - Hazards (Flooding – General) Overlay);
- stormwater is to be directed to an underground detention tank which will ensure minimisation of post-development stormwater flows, reducing flooding risks; and
- pollution control devices are proposed to improve the quality of surface runoff prior to discharging to the Council drainage system (Design module PO 31.1, 31.2 and 32.1).

The stormwater management plan and calculations were referred internally to Council's Civil Engineer for comments. In Council's Request for Further Information, it was requested the applicant provide additional detail in relation to the proposed Stormwater Management noting that the overall concept is supported.

This information was supplied by the applicant as part of their response to Council and is currently pending review with Council's engineering team. As the overall concept is

supported pending technical review, Administration consider that this information is able to be addressed by way of a reserved matter, so as to allow for the application to proceed. This reserved matter is contained within the recommendation of this report

7.8 Council Trees

The Applicant clarified in the Response to the Request for Further Information that all the Council trees along the Curtis Road frontage will need to be removed to facilitate the development. This is to allow for the construction of the retaining wall to support the cut and fill required on the development site for the car park and bulky goods building.

None on the trees proposed to be removed are regulated trees. Councils Tree Services have reviewed the proposal.

8. Conclusion

The development is considered to align with the overarching principles of the Employment Zone to provide a use that supports the local community and passing traffic. The proposed development delivers a land use that is considered to be acceptable for the Employment Zone. The design, bulk and scale of the proposed development is compatible with the Employment Zone Desired and Performance Outcomes and has been designed to minimise impact on the other land uses surrounding.

The proposal is not anticipated to result in any unreasonable noise impacts upon the living amenity of nearby residents as outlined in the Acoustic Report and with the amended acoustic treatments.

Council's traffic engineer is satisfied with the on-site car parking provision, which satisfies the quantitative requirement of the Planning and Design Code. Vehicular access and egress is considered to be safe and convenient as confirmed by the DIT referral and by Council's traffic engineer.

The concerns of the representor have been addressed by the in the response provided by the applicant. The primary concerns in relation to traffic impacts have been adequately addressed and supported with relevant reports and documentation prepared by suitably qualified professionals.

The development exhibits suitable merit when assessed on balance against the relevant Desired Outcomes, Performance Outcomes and applicable Designated Performance Features to such a degree that it warrants Planning Consent subject to the conditions listed below in the Recommendation.

9. Recommendation

STAFF RECOMMENDATION

That pursuant to the authority delegated to the Council Assessment Panel by the Council, it is recommended that the Council Assessment Panel:

- a) DETERMINES that the proposed development is not seriously at variance with the policies in the Planning and Design Code; and
- b) GRANTS Planning Consent to the application by Leyton Property C-/ Future Urban for a Bulky Goods Outlet, Retaining and Associated Advertising at Lot 10 Mingari Road, Munno Para as detailed in Development Application ID 22039894 subject to reserved matter and conditions:

Reserved matter

The following matter has been reserved pursuant to section 102(3) of the Planning, Development & Infrastructure Act 2016. Prior to the issue of Development Approval and to the reasonable satisfaction of the Assessment Manager/Council, the Applicant shall provide:

- The provision of a Stormwater Management Plan that resolves the outstanding technical matters from Council's Request for Further Information Letter dated 20 January 2023, to the reasonable satisfaction of Council.

Council Conditions:

1. The development must be undertaken, completed and maintained in accordance with the plan(s) and information detailed in this Application except where varied by any condition(s) listed below.

Reason: To ensure that the development is constructed and operated in accordance with the plans and details provided.

2. Deliveries and service hours herein approved are as follows:

9am to 7pm on Sundays or Public Holidays

7am to 7pm on any other day

The private collection of waste from the store occurs between:

7am and 7pm Monday to Saturday and not on public holidays or Sundays.

3. All loading and unloading of goods and merchandise shall be carried out upon the subject land and no loading of any goods or merchandise shall be permitted to be carried out in the street in conjunction with the consent herein granted.
4. Landscaping shown on the plans herein approved shall be established to the reasonable satisfaction Council prior to the operation of the development and shall be maintained and nurtured at all times with any diseased or dying plants being replaced.
5. All waste and rubbish shall be stored in covered containers prior to removal and shall be screened from public view.
6. All external lighting of the site, including, but not limited to car parking areas, advertising signs and all buildings shall be designed and constructed to conform with Australian Standards and must be located, directed and shielded and of such limited intensity so as to not, in the opinion of the relevant authority, create unreasonable overspill onto any adjoining property or roadway which may create a nuisance to any neighbour or road user.
7. All driveways, parking and manoeuvring areas must be formed, sealed with concrete, bitumen or paving, and be properly drained. They must be maintained in good condition thereafter.
8. All stormwater resulting from the subject development shall be managed in an orderly manner and in accordance with the approved plans and documentation so that it does not flow or discharge onto land of adjoining owners or, in the opinion of Council, detrimentally affect structures on this site or any adjoining land.

Department of Infrastructure and Transport Conditions:

9. Access to Curtis Road and Mingari Street shall be constructed as shown on Brown Falconer, Proposed Site Plan – RSEA Safety, Job No. 2020082, Drawing No. 3375 DA03, Revision 6, dated 17 November 2022 and operate in accordance with MFY Traffic Report, File MLM/22-0117, dated 25 November 2022.
10. The Curtis Road access point shall be suitably flared to accommodate the left turn in movement of a Heavy Rigid Vehicle (HRV) as per AS2890.2:2018.
11. Appropriate signage and line marking shall be installed to reinforce the desired traffic flow at the access points.
12. All vehicles shall enter and exit the site in a forward direction.
13. All land required from CT 6264/204 and CT 5554/48 for the construction of the channelised left turn and associated infrastructure (eg. footpath, pedestrian facilities, and associated road reserve) shall be set aside from the subject allotments for road purposes at no cost to the department. The new infrastructure shall match into the adjacent/proposed facilities to the satisfaction of DIT and Council.
14. Stormwater run-off shall be collected on-site and discharged without impacting the adjacent road network. Any alterations to the road drainage infrastructure required to facilitate this shall be at the applicant's cost.



PLANNING REPORT BULKY GOODS OUTLET

Lot 10 Mingari Street, Munno Para

Prepared for:
Leyton Property

Date:
25.11.2022



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Proprietary Information Statement

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APPENDICES

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1. INTRODUCTION

This report has been prepared to accompany a development application by Leyton Property ('the Proponent') to construct a building comprising two bulky goods tenancies with associated advertising signage and retaining on the site at Lot 10 Mingari Street, Munno Para ('the site').

In preparing this report, we have:

- inspected the site and its immediate surroundings;
- identified and subsequently reviewed what we consider to be the most pertinent provisions of the Planning and Design Code ('the Code');
- had regard to the Planning, Development and Infrastructure Act 2016 ('the Act') and to the Planning, Development and Infrastructure (General) Regulations 2017 ('the Regulations');
- also had regard to the certificate of title in **Appendix 1**;
- examined the architectural drawings in **Appendix 2**;
- reviewed the supporting documents, including:
 - » stormwater and civil prepared by PT Design in **Appendix 3**;
 - » traffic and parking assessment prepared by MFY in **Appendix 4**; and
 - » acoustic report prepared by Echo Acoustic in **Appendix 5**.

This report contains our description of the site, its surroundings and the proposal, and our assessment of the proposal against what we consider to be the most relevant provisions of the Code.



2. PROPOSED DEVELOPMENT

The Proponent seeks to obtain planning consent to:

- construct a building comprising two bulky goods tenancies;
- associated retaining;
- install one freestanding pylon sign with internally illuminated panels; and
- install six advertising signs on the building, one of which is internally illuminated.

The proposal is further summarised below.

2.1 Design and Built Form

The proposed building has a total footprint of 1,849 square metres. Tenancy 1 (RSEA) comprises of 1,500 square metres and tenancy 2 comprises of 363 square metres.

2.2 Building height

The main building is single level and will reach a height of 6.9 metres above the finished floor level. The advertising display then sits atop the building to reach a total height 10 metres.

2.3 Siting

The building will be setback 8.86 metres from Mingari Street, 19.8 metres from Curtis Road, 2.74 metres from the eastern side boundary and sited on the boundary to the right of way easement at the rear.

2.4 Traffic Management

2.4.1 Access

The proposal will utilise a new 6.6 wide crossover from Mingari Road at the rear of the proposed building, predominantly for deliveries. This crossover will also provide access to the car parking area which will then connect into the remainder of the subject land.

2.4.2 Parking

The development will be serviced by a total of 48 spaces, including 1 space reserved for disabled access.

2.5 Stormwater Management

Stormwater and civil plans were prepared by PT Design (**Appendix 3**).

The following has been confirmed in relation to stormwater management for the site:

- stormwater is to be directed to an underground detention tank which will ensure minimisation of post-development stormwater flows, reducing flooding risks; and
- pollution control devices are proposed to improve the quality of surface runoff prior to discharging to the Council drainage system.



2.6 Landscaping

A landscaping plan and planting schedule has been provided within the architectural set displaying space for landscaping and a variation in plant types along the southern and western boundaries of the site. Multiple trees immediately surrounding the site are planned to be maintained.

2.7 Advertisement

Each tenancy is designed with a corporate sign above their respective front entrance, one of which is proposed to be internally illuminated (RSEA).

Five additional non-illuminated signs are proposed on the building including two of which rise above the roof and address the south and east.

An 8 metre high freestanding pylon advertising sign is proposed on the corner of Mingari Street and Curtis Road and will display the corporate logos for the two future tenancies. It will be internally illuminated.



3. SPATIAL ATTRIBUTES

3.1.1 Subject Land

The subject land comprises of one allotment, legally described on Certificate of Title 6271/130, or otherwise known as Lot 10 Mingari Road, Munno Para.

The land is currently vacant and is part of a wider development which includes a retail fuel outlet, shops, carparking and a carwash, all of which have received planning consent (21024444).

The sites within the wider development site are to be connected though a series of rights of ways.

In addition, a land division to divide the land into 3 allotments has also been undertaken (DA 21035564).

The subject site is within the wider development site is shown in Figure 3.1 below.

Figure 3.1 *Subject Land*



3.1.2 Development Site

The development site is 4,016 square metres in area with a 79.83 metre frontage to Curtis Road and 50 metre frontage to Mingari Street.

No regulated or significant trees are proposed to be removed as a result of the proposed development and the land is not heritage listed.

A right of way along the non-street frontage boundaries is incorporated to enable access between the proposed development and other previously approved developments.

The development site is shown overleaf in Figure 3.2.



Figure 3.2 Development site



3.1.3 Zoning

The development site is situated within the Employment Zone and the Retail Activity Centre Subzone as shown in Figure 3.3 below.

Figure 3.3 Subject Site Zoning





The site is also within the following overlays and Technical and Numeric Variations (TNVs):

- Defence Aviation Area (All structures over 90 metres);
- Future Road Widening;
- Hazards (Flooding General);
- Major Urban Transport Routes;
- Noise and Air Emissions;
- Prescribed Wells Area;
- Regulated and Significant Tree;
- Traffic Generating Development;
- Concept Plan (Concept Plan 11 – Munno Para); and
- Concept Plan (Concept Plan 81 – Edinburgh Defence Airfield Lighting Constraints).

3.1.4 Locality

In our opinion, the locality is defined predominantly by sites which are able to directly view the development site.

Upon undertaking an inspection of the development site and its surroundings, the following was observed:

- The development site is and its surroundings are vast and cleared of any structures.
- The subject land makes up the entirety of the Employment Zone.
- The locality is dominated by the high traffic Curtis Road and Arterial State Maintained Main North Road, with Main North Road being under the care and control of the Department for Infrastructure and Transport.
- The locality has a relatively low amenity value.
- The nearby residential area primarily comprises of low-density single storey detached dwellings, albeit separated by a road and large Council verge/reserve from the development site.

The site in relation to its immediate surroundings, is captured in Figure 3.4 overleaf.



Figure 3.4 *Locality plan*





4. PROCEDURAL MATTERS

At the time of preparing this report, the relevant version of the Planning and Design Code was gazetted and subsequently consolidated on 24 November 2022 (V2022.22). Due to amendments, the version of the Code used to prepare this report may not be the relevant version at the time of lodgement of the application. To the extent of any inconsistency, the version of the Code at the time of lodgement will be relevant for the processing and assessment of the application.

4.1 Verification

For the purposes of regulation 31(1)(a), (b) and (c) of the Regulations, the following applies:

Table 4.1 *Verification snapshot*

Verification matter	Comment
Nature of Development	Construction of a bulky goods outlet comprising two tenancies with associated retaining and advertising, including a freestanding pylon sign.
Elements	Shop (Bulky goods outlet) Advertisement Retaining
Category of Development	Performance Assessed
Relevant Authority	Council Assessment Panel at the City of Playford

4.1.2 Nature of development

It is noted that a bulky goods outlet fits within the definition of a 'shop', as defined in *Part 7 – Land Use Definition* of the Code:

“Shop” means:

(a) premises used primarily for the sale by retail, rental or display of goods, foodstuffs, merchandise or materials; or

(b) a personal or domestic services establishment

including: a bulky goods outlet...”

4.1.3 Category of Development

The proposed development is excluded from being a restricted form of development in table 4 of the Employment Zone due to being a bulky goods outlet.

The Zone lists both a shop and advertisement within table 3. The development application therefore results in the performance assessed pathway with the Code specifying the required policies for assessment purposes.

4.2 Referrals

The site is subjected to the overlays that may require a referral, pursuant to Section 122(1) of the Act, in accordance with regulation 41(1) to a body prescribed in Schedule 9 of the Regulations.



We submit the following comments in relation to the relevant referral triggers:

Table 4.2 *Referral triggers*

Overlay	Referral (Y/N)	Comment
Future Road Widening Overlay	No	Development is greater than 6 metres away from Main North Road
Major Urban Transport Routes	No	Impact to the State maintained Main North Road is considered to be minor due to the primary access point being from Mingari Street.
Traffic Generating Development Overlay	No	Retail floor area is less than 2000 square metres.

4.3 Public Notification

Pursuant to section 107(6) of the Act, the Planning and Design Code may exclude specified classes of development from the requirement to undergo public consultation. Accordingly, Table 5 of the Zones provides the following:

Table 4.3 *Table 5 – Procedural Matters (excerpt)*

EMPLOYMENT ZONE	
Class of Development (Column A)	Exceptions (Column B)
(2) Any development involving any of the following (or of any combination of any of the following): a. advertisement	Except development that exceeds the maximum building height specified in Employment Zone DTS/DPF 3.5 or does not satisfy any of the following: 1. Employment Zone DTS/DPF 3.6 2. Employment Zone DTS/DPF 3.7
(7) Shop	Except: 1. where the site of the shop is adjacent land to a site (or land) used for residential purposes in a neighbourhood-type zone or 2. shop that exceeds the maximum building height specified in Employment Zone DTS/DPF 3.5 or 3. shop that does not satisfy Employment Zone DTS/DPF 1.2.

The proposed development requires public notification has it exceeds the maximum building height specified in Zone DPF 3.5 of 9 metres.



5. ASSESSMENT AGAINST PLANNING AND DESIGN CODE

Table 3 of the Employment Zone lists the applicable policies for a shop (bulky goods outlet), retaining and advertising.

The applicable policies include Desired Outcomes (DOs) which “*automatically apply in relation to a performance assessed development*” and Performance Outcomes (POs). It is also worth noting that some POs have a standard outcome that is considered to satisfy the corresponding PO, referred to as Designated Performance Features (DPFs). The Rules of Interpretation within Part 1 of the Code state the following in relation to DPFs (underlining our emphasis):

“A DPF provides a guide to a relevant authority as to what is generally considered to satisfy the corresponding performance outcome but does not need to necessarily be satisfied to meet the performance outcome and does not derogate from the discretion to determine that the outcome is met in another way, or from the need to assess development on its merits against all relevant policies.”

It is noted the ERD Court has recently provided guidance with respect to the interpretation of the Planning and Design Code, more particularly the manner in which DPFs are to be viewed in the context of a planning assessment. In *Garden College v City of Salisbury [2022] SAERDC 10*, the full Court held;

“That said, it must not be overlooked that the way in which the DTS/DPF criteria serve a procedural function is through the intermediary of a procedural table specifying classes of development excluded from public notification and exceptions to such exclusions that incorporate such criteria by reference. It does not follow where the satisfaction of DTS/DPF criteria excludes performance assessed development from public notification, a relevant authority would be prevented from deciding not to grant planning consent on the elements of a development requiring a merits assessment against the Code. Indeed, s107(8) and the Code Rules of Interpretation make it plain that satisfaction of DTS or DPF criteria does not derogate from the relevant authority’s discretion to determine the outcome on a merits assessment against all relevant provisions of the Code, including any relevant corresponding POs and DOs.”

As a result of the above, the assessment below focusses on the applicable DOs and POs and may only refer to the DPF in instances where it assists in the exercise of discretion.

5.1 Land Use

The Employment Zone envisages:

PO 1.1 *A range of employment-generating light industrial, service trade, motor repair and other compatible businesses servicing the local community that do not produce emissions that would detrimentally affect local amenity.*

PO 1.4 *Bulky good outlets and standalone shops are located to provide convenient access.*

Bulky good outlets are clearly envisaged within the Zone, offering an employment-generating land use whilst also considered to compliment surrounding businesses in the locality and the broader development for this site. Having access from Mingari Street and connection to the other establishments within the wider development, the proposed shops work to create a hub of services for the local community. The proposal enables convenient access to users, satisfying PO 1.1 and PO 1.4 above.

5.2 Built Form and Design

5.2.1 Siting

Relating to setbacks, the Employment Zone advises:



PO 3.1 *Buildings are set back from the primary street boundary to contribute to the existing/emerging pattern of street setbacks in the streetscape.*

PO 3.2 *Buildings are set back from a secondary street boundary to accommodate the provision of landscaping between buildings and the street to enhance the appearance of land and buildings when viewed from the street.*

PO 3.4 *Buildings are sited to accommodate vehicle access to the rear of a site for deliveries, maintenance and emergency purposes.*

There are no existing or approved developments which address Mingari Street as the primary street. The corresponding designated performance feature to PO 1.1 (DPF 1.1), states that the primary street setback should be no less than 3m where no building exists on an adjoining site with the same primary street frontage. Since there are no existing buildings on adjoining sites, the 8.9 metre primary street setback provided is considered to be appropriate, especially noting the space from the nearby residential uses and the more recently approved buildings to the north.

The secondary street is separated from the subject site by approximately 27 metres, with a large number of established trees in place which work to reduce visual prominence of the building and improve the overall appearance when viewed from the secondary street.

The proposed building has been sited to allow access for loading and deliveries to the rear of the building as well as a 2.8 metre side setback to allow access for maintenance and emergency purposes.

The building is considered to be appropriately sited in accordance with the above PO's.

5.2.2 Building Height

Two advertising displays are attached to the building exceeding the overall roof height of the building, reaching a maximum height of 10 metres above the finished floor level. Although exceeding DPF 3.5 pertaining to building height, in the context of the locality, the proposed height is not considered to be of a detriment especially due to the distance from residential properties and the extensive Council verge from the main street.

5.3 Interface between Land Uses

DO 1 of the Interface between Land Uses module advises:

DO 1 *Development is located and designed to mitigate adverse effects on or from neighbouring and proximate land uses*

The subject site is located approximately 35 metres from residential properties within the nearby Master Planned Neighbourhood Zone to the north-west and is separated by Mingari Street, the verge, and a reserve with established mature trees.

5.3.1 Noise

An acoustic report prepared by Echo Acoustic advises that the proposed development achieves the *Environment Protection (Noise) Policy 2007* without any specific acoustic treatments to the built form.

Therefore, no unreasonable adverse impacts to neighbouring land uses are anticipated due to separation distance from the nearest residential developments and siting of the entrances and carpark of the bulky goods outlets away from the nearest sensitive receivers.



5.4 Traffic Management

5.4.1 Access

A comprehensive analysis of access and traffic management of the proposed development is found in the traffic report prepared by MFY which concludes the access points and movements through the site are considered to be safe and convenient.

In summary, the application meets the requirements of the Code in that:

- the access points and internal circulation within the site and between both tenancies has been designed to accommodate the turning movements of all vehicles anticipated within the site;
- the site has been designed such that all movements can enter and exit in a forward direction to ensure the safety of vehicle users and minimise interference with the traffic flow of the nearby State maintained road (Major Urban Transport Routes Overlay PO 1.1);
- the traffic generation associated with the overall development is considered to be within the capacity for the various access points and surrounding roads, in line with Traffic Generating Development PO 1.2; and
- the design allows service and delivery movements by commercial vehicles to be undertaken separately from areas associated with domestic vehicles (Transport, Access and Parking module PO 1.3).

5.4.2 Parking

The Code designates a vehicle parking rate of 2.5 spaces per 100 square metres for land uses classified as 'shop (in the form of a bulky goods outlet)'. Based on this rate, there is a theoretical parking demand of 47 spaces. The proposal provides a total of 48 spaces, thus surpassing the theoretical minimum carparking requirements sought within the Zone.

The parking areas have been designed to satisfy the policies of the Transport, Access and Parking module as:

- moving between the different parking areas is done from within the subject site and does not require the use of external public roads to do so (PO 6.1);
- a loading area is provided to the side of the site, with delivery and service vehicles able to safely utilise without the need for on-street collection (PO 6.6); and
- they comply with the requirements of the Australian and New Zealand Standards.

5.5 Stormwater Management

The Proponent engaged PT Design to ensure stormwater is able to be appropriately managed as a result of the proposed development.

The calculations and civil plans confirm that:

- the development will be designed to ensure that the post-development discharge flows do not exceed the pre development discharge flows;
- the proposed floor level is greater than 300mm above street level (PO 1.1 - Hazards (Flooding – General) Overlay);
- stormwater is to be directed to an underground detention tank which will ensure minimisation of post-development stormwater flows, reducing flooding risks; and



- pollution control devices are proposed to improve the quality of surface runoff prior to discharging to the Council drainage system (Design module PO 31.1, 31.2 and 32.1).

5.6 Landscaping

The Employment Zone advises the following in relation to landscaping:

PO 5.1 *Landscaping is provided to enhance the visual appearance of development when viewed from public roads and thoroughfares.*

PO 5.2 *Development incorporates areas for landscaping to enhance the overall amenity of the site and locality.*

A landscaping plan has been prepared for this project, including landscaping strips to the primary and secondary street frontages. A wide range of plant species have been selected including small trees, medium trees and ground covers. The existing established trees which surround the site are to be retained, further contributing to the amenity of the site and locality.

5.7 Advertisements

Advertising is proposed at the entrance for each of the tenancies as well as three additional signs on the building, two of which rise above the roof line of the building.

A single free standing pylon sign at the south west corner is also proposed, reaching a total height of 8 metres. It will involve internally illuminated corporate advertising and located at the south-western corner of the subject site.

The corporate advertisements:

- will not flash or be animated;
- will convey the name of the prospective operator and relate, therefore, to the intended uses of the site; and
- feature corporate colours.

It is therefore, most unlikely to distract motorists from their primary task or detract from the external appearance of the proposed buildings in this context.

The proposed pylon sign, although exceeding Zone DPF 6.1 in relation to overall height, is not considered to be visually dominant within the locality (per Zone PO 6.1), as:

- it is setback approximately 26 metres from Curtis Road due to the large verge; and
- the remainder of the subject land includes various pylon signs, reaching a total height of 10 metres.



6. CONCLUSION

We have concluded from our assessment of the proposal that it is deserving of consent.

In support of our conclusion, we wish to highlight that:

- the land use is envisaged within the zones;
- building height and scale is appropriate within the local context, Zone and Subzone;
- the siting of the building is considered appropriate, allowing for convenient access, providing landscaping to both street frontages and limited impacts to nearby residential land uses;
- adequate parking for staff and visitors is available on site;
- all expected vehicles will be able to enter and exit the site in a forward direction;
- vehicles will be able to access other services within the broader development without relying on the need to access the surrounding public roads;
- proposed and retained landscaping will create a positive improvement for the proposed building;
- stormwater runoff will be captured and disposed of in an environmentally sound manner; and
- simple corporate advertising is proposed and will not detract motorists from driving, but will be clearly defined for persons seeking to utilise the shops.



PRELIMINARY		
WORK IN PROGRESS ISSUE		
ISSUED 3.35.23 PM		
No.	Amendment	Date
1	PRELIMINARY DA PACKAGE	26.07.22
2	PRELIMINARY DA PACKAGE	29.08.22
3	PRELIMINARY DA PACKAGE	01.09.22
4	PRELIMINARY DA PACKAGE	17.11.22
5	MINUTE FOR CAJUDGEMENT	16.02.23

PRINTING NOTE
THIS DRAWING IS TO BE PRINTED IN COLOUR
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MUNNO PARA DEVELOPMENT - RSEA SAFETY
CNR MINGARI STREET & CURTIS ROAD, MUNNO PARA
DEVELOPMENT APPLICATION DRAWINGS - WORK IN PROGRESS ISSUE

ARCHITECTURAL DRAWING SCHEDULE

SHEET LIST - DA			
Sheet Number	Sheet Name	Revision	Revision Date
DA01	COVER SHEET	01	16.03.23
DA02	EXISTING CONDITION AND SITE CONTEXT	01	17.11.22
DA03	SITE & LANDSCAPE PLAN	01	16.03.23
DA04	RSEA SAFETY - FLOOR PLANS & ROOF PLAN	01	17.11.22
DA05	RSEA SAFETY - ELEVATIONS	01	16.03.23
DA06	SPRINKLES	01	16.03.23

Leyton
Property

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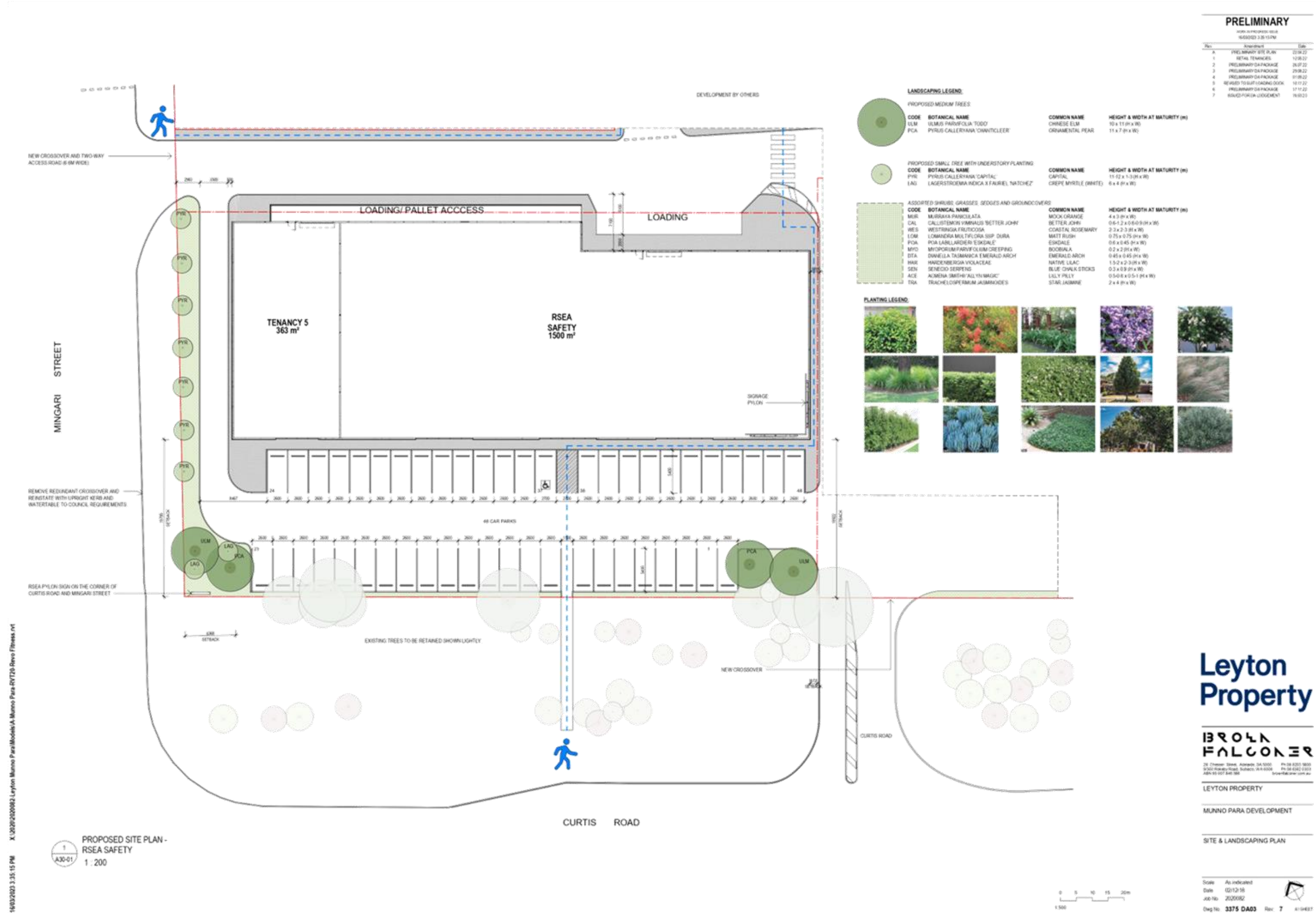
LEYTON PROPERTY

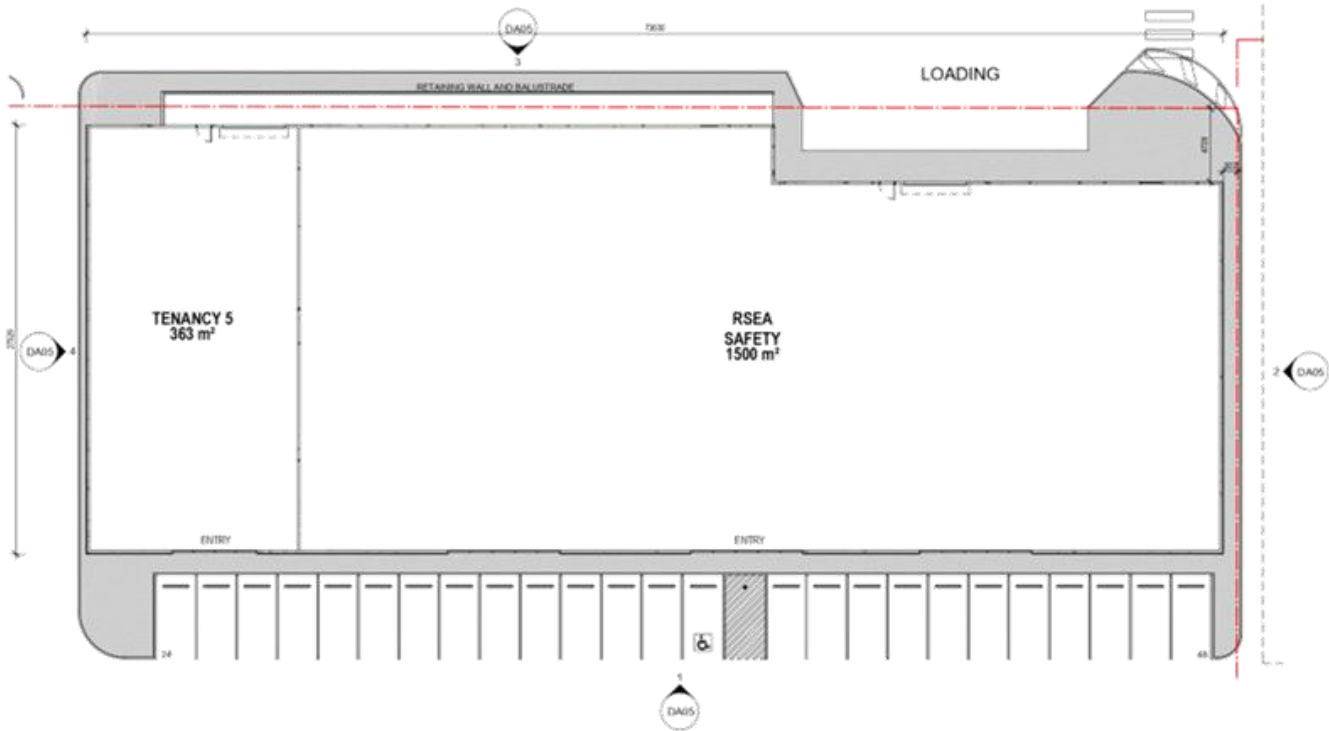
MUNNO PARA DEVELOPMENT

COVER SHEET

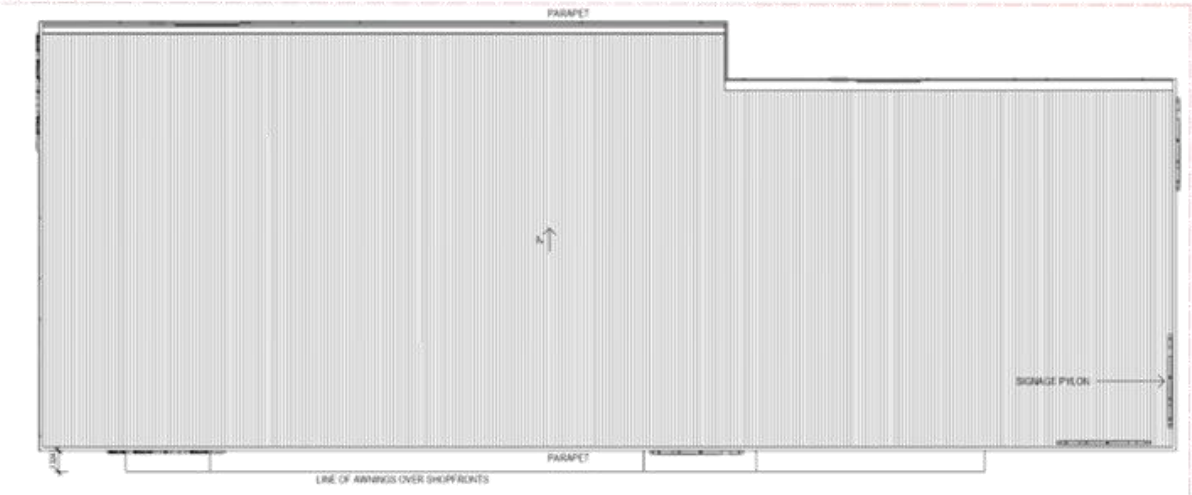
Scale
Date 03.15.18
Job No. 2020082
Orig No. 3375 DA01 Rev: 1 4/19/2017







1
11-01
FLOOR PLAN - RSEA
SAFETY
1 : 200



2
A30-01
ROOF PLAN - RSEA
SAFETY
1 : 200

PRELIMINARY		
WORK IN PROGRESS - 08/12/21		
No.	Amendment	Date
1	PRELIMINARY DA PACKAGE	26/07/21
2	PRELIMINARY DA PACKAGE	29/08/21
3	PRELIMINARY DA PACKAGE	01/09/21
4	PRELIMINARY DA PACKAGE	17/11/21

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Property

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MUNNO PARA DEVELOPMENT

RSEA SAFETY - FLOOR PLAN &
ROOF PLAN

Scale: 1 : 200
Date: 08/12/21
Job No: 2020082
Dwg No: 3375 DA04 Rev: 4 4/19/21





PRELIMINARY		
WORK IN PROGRESS - 08/18		
ISSUED 3.34.55 PM		
No.	Amendment	Date
1	PRELIMINARY DA PACKAGE	26.07.22
2	PRELIMINARY DA PACKAGE	29.08.22
3	PRELIMINARY DA PACKAGE	01.09.22
4	PRELIMINARY DA PACKAGE	17.11.22
5	MINUTE FOR CAJUDGEMENT	16.02.23



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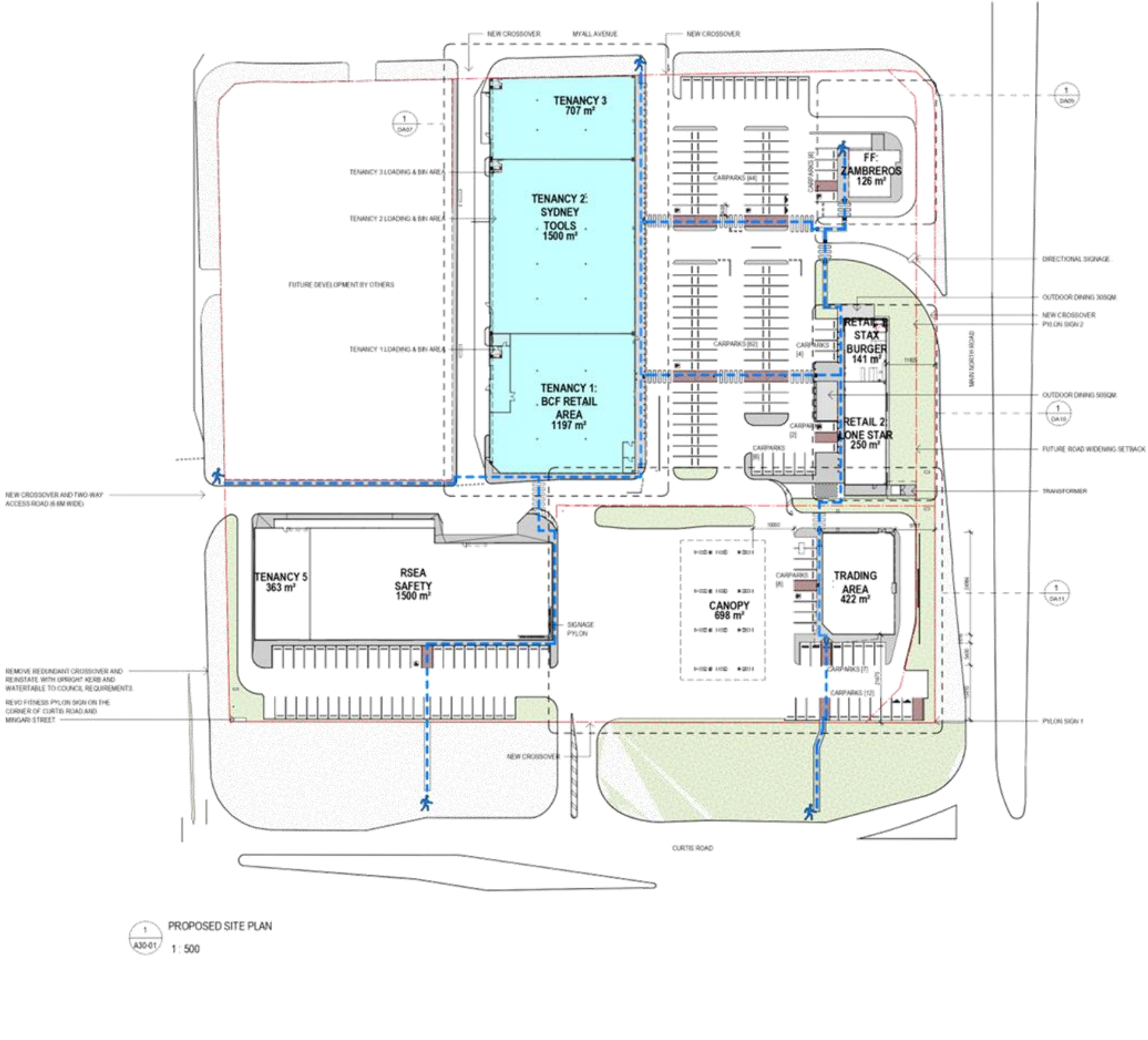
LEYTON PROPERTY

MUNNO PARA DEVELOPMENT

3D IMAGES

Scale
Date 02/12/18
Job No 2020082
Dwg No 3375 DA06 Rev: 5 4/19/2021





PRELIMINARY		
NOVA ARCHITECTURE		
No.	Amendment	Date
1	FOR APPROVAL	10/08/21
2	REVISED TO ROAD WIDENING SETBACKS	16/08/21
3	NO FOOTPRINT ADJUSTED	17/08/21
4	DEVELOPMENT APPLICATION	18/08/21
5	DEVELOPMENT FOR PLANNING	30/08/21
6	PLANNING DARP RESPONSE	08/11/21
7	PLANNING DARP RESPONSE	11/11/21
8	REVISOR FOR PLANNING	24/11/21
9	TRAFFIC AMENDMENTS TO ORCA REPORT, 14/12	15/12/21
10	RETAIL CLIA REVISED AND OUTDOOR DINING AREAS ADDED	20/01/22
11	REVISED RETAIL	19/02/22
12	REVISED RETAIL SIGNAGE	24/03/22
13	REVISED RETAIL SIGNAGE	01/04/22
14	REVISED FOR PLANNING	21/02/23

Leyton Property

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LEYTON PROPERTY

MUNNO PARA DEVELOPMENT

SITE PLAN

Scale: As indicated
Date: 02/12/16
Job No: 2020082
Dwg No: 3375 DA05 Rev: 12 A1 SHEET

MLM/22-0117

25 November 2022

Mr. Marc Duncan
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MFY Pty Ltd
ABN 79 102 630 759

Dear Marc,

PROPOSED BULKY GOODS DEVELOPMENT, CURTIS ROAD, MUNNO PARA

We refer to the proposal to develop a bulky goods development on the corner of Mingari Street and Curtis Road. As requested, we have reviewed the traffic and parking implications associated with the proposal. The assessment has been based on Brown Falconer's Site Plan (Drawing Number: 3375 DA03), Revision 6 dated 17 November 2022.

1 SUBJECT SITE

The subject site fronts Mingari Street and Curtis Road. It is located in the south-western corner of the land bound by Mingari Street to the west, Curtis Road to the south, Main North Road to the east and Myall Avenue to the north. Figure 1 identifies the subject site.



Figure 1: Subject site

An existing approval for development on the land includes bulky goods tenancies, a car wash, a retail fuel outlet and a quick service restaurant. Access to the approved development will be provided via Main North Road, Curtis Road and Myall Avenue.

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A variation to the existing approval will be sought to ensure that the access for the subject proposal and the approved development access are consistent.

1.1 ROAD NETWORK

Curtis Road and Mingari Street are in the care and control of City of Playford.

Curtis Road is a major collector road. It has an annual average daily traffic total in the order of 16,200 vehicles. The road has a posted speed limit of 60 km/h.

Adjacent the site, the road has a two-lane divided carriageway which widens at intersections to accommodate turning lanes. The road includes bicycle lanes in both directions. Approximately 100 m east of the site, Curtis Road forms a signalised intersection with Main North Road and Craigmores Road.

Mingari Street connects Myall Avenue and Curtis Road. The urban default speed limit of 50 km/h is applicable to the road.

Mingari Street and Curtis Road form an intersection adjacent to the site. The intersection is treated with channelised left and right turn lanes on Curtis Road.

2 PROPOSAL

The proposed bulky goods development will include two tenancies with a combined floor area of 1,863 m².

The proposal includes a car park with 48 parking spaces. The car park will be designed to meet the requirements in Australian/New Zealand Standard, *Parking Facilities Part 1: Off-street car parking (AS/NZS 2890.1:2004)* in that:

- spaces will be 2.6 m wide;
- spaces will be 5.4 m long; and
- the adjacent aisle will have a minimum width of 7.0 m.

2.1 ACCESS

The site will be integrated with the adjacent development and therefore will be able to utilise the existing access points on Main North Road, Curtis Road and Myall Avenue.

In addition, a new crossover on Mingari Street is proposed as part of this development. The proposed access will be designed in accordance with AS/NZS 2890.1:2004. It will permit all movements to and from the site and will cater for simultaneous movements as shown in Figure 2.

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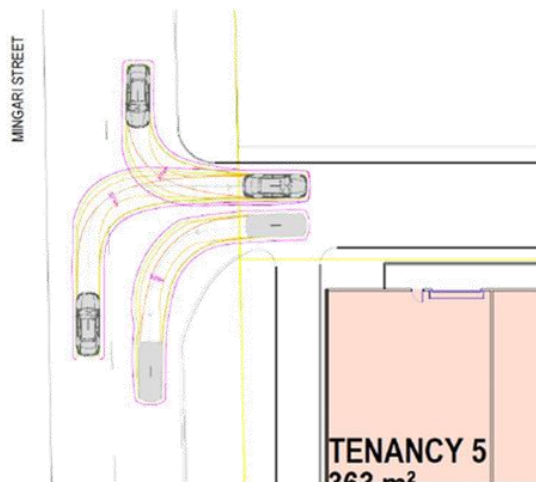


Figure 2: Simultaneous vehicle movements at the proposed Mingari Street access

The proposed car park will be accessed via the driveway connecting to Curtis Road. All movements will be permitted at this access. The design of the car park access will provide for simultaneous turning movements to ensure that the vehicles entering from Curtis Road are not impeded when accessing the car park, as illustrated in Figure 3.

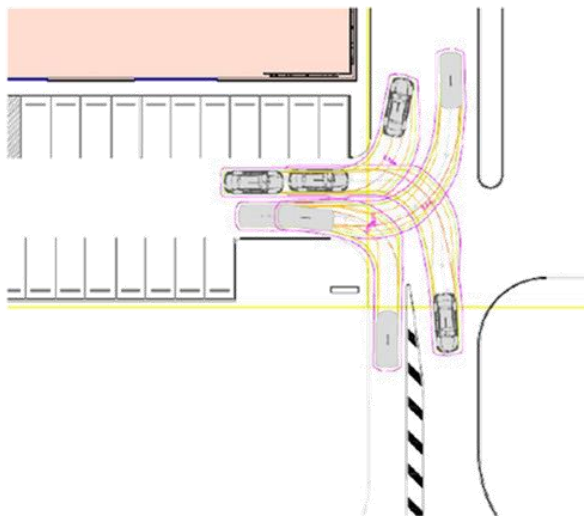


Figure 3: Simultaneous movements at the access

An addition egress will also be provided from the car park to facilitate movements to Mingari Street. The intersection of the aisles will be located to provide for the turning movements of a B99 vehicle to Mingari Street, as illustrated in Figure 4

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25 November 2022
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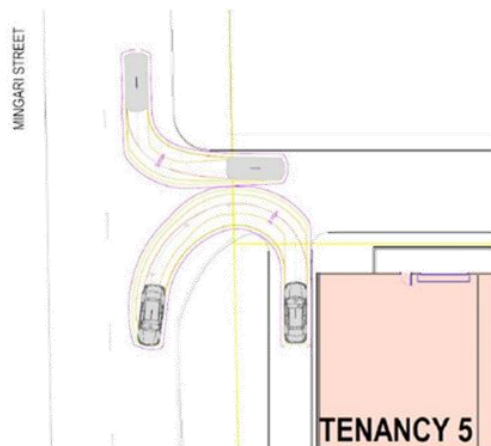


Figure 4: Egress movements from car park to Mingari Street

Adequate sight lines for drivers will also be provided at the car park egress, as illustrated in Figure 5.

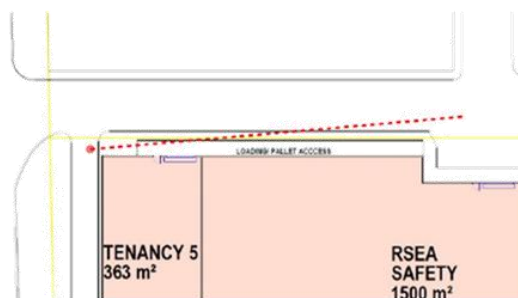


Figure 5: Sight line for drivers at the connections to the northern driveway

2.2 DELIVERY VEHICLES

An indented loading bay will be provided adjacent the northern side of the proposed building. It will be accessed via the northern aisle. The design of the loading bay will comply with Australian Standard, *Parking Facilities Part 2: Off-street commercial vehicle facilities (AS 2890.2:2018)*. A ramp will be constructed to provide for goods to be delivered to the western tenancy, given the height difference between the FFL and the aisle at this location.

The largest vehicle which will access to loading bay will be a heavy rigid vehicle (HRV). These vehicles will enter the site via Curtis Road and exit the site via Mingari Street and will enter and exit the loading area in a forward direction, as illustrated in Figure 6.

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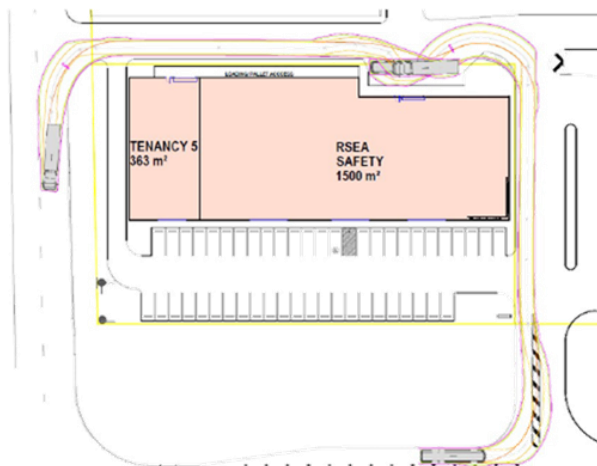


Figure 6: An HRV entering and exiting the loading bay

Refuse will also be collected from the loading bay. Each tenancy will be responsible for storing bins independently on-site and presenting them for collection within the loading area. The refuse vehicle will enter and exit the loading area in a forward direction.

2.3 PEDESTRIAN CONNECTIVITY

Footpaths will be constructed along the northern, eastern and southern boundaries of the building. These paths will provide for safe pedestrian access within the proposed development site, as well as providing connectivity to other facilities to the east and to the adjacent road network.

3 PARKING ASSESSMENT

3.1 VEHICLE PARKING DEMAND

The Planning and Design Code identifies a parking provision rate of 2.5 spaces per 100 m² for a bulky goods facility. This equates to a requirement for 47 parking spaces. The proposal provides 48 parking spaces and therefore will accommodate the forecast parking demand.

3.2 BICYCLE PARKING DEMAND

The Planning and Design Code does not identify a specific bicycle parking rate for a bulky goods development. Accordingly, reference has been made to Austroads "Guide to Traffic Management – Part 11: Parking Management Techniques" which identifies the following rate for a retail showroom:

- one space per 750 m² of sales area for employees; plus
- one space per 1000 m² of sales area for shoppers.

Based on the above rate, there will be a requirement for approximately four bicycle parking spaces which could be provided with two bicycle rails. Such rails can readily be accommodated on the site.

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4 TRAFFIC ASSESSMENT

The Guide to Traffic Generating Developments Technical Direction (TDT 2013/04A) identifies an average traffic generation rate of 1.31 trips per 100 m² for a bulky goods facility in the pm peak hour.

Based on this rate, the proposal is forecast to generate approximately 25 trips in the pm peak hour, of which 10 will enter the site and 15 will exit the site. Such an increase in traffic volumes is low and will have no appreciable impact on the road network.

5 SUMMARY

The proposed bulky goods development will be developed within an integrated site but will independently cater for the anticipated peak parking demand. The land use will be complimentary to the approved development as it relates to traffic and parking criteria.

The design will facilitate pedestrian, cyclist and vehicle connectivity between uses on the site, thus providing for shared trips and reduced circulation on the external road network. The proposed car park will satisfy the design requirements of AS/NZS2890.1:2004.

Access for the site will be safe and convenient and will ensure all vehicles can enter and exit in a forward direction. Connectivity with the adjacent parking areas will increase opportunities for access to the site.

Traffic generated by the development during the commuter peak hour will be minimal and will not change the nature and function of the road network. Further, the integrated nature of the design will ensure that traffic is distributed on the broader road network, thus minimising the impact on any one road.

Yours sincerely,
MFY PTY LTD

A handwritten signature in black ink, appearing to read 'Melissa Mellen'.

MELISSA MELLEN
Director



MLM/22-0271

9 March 2023

Mr. Marc Duncan
Future Urban Group
GPO Box 2403
ADELAIDE SA 5001



Traffic • Parking • Transport

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FULLARTON SA 5063

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MFY Pty Ltd
ABN 79 102 630 759

Dear Marc,

**APPLICATION ID 22039894 – REQUEST FOR INFORMATION
LOT 10 MINGARI STREET, MUNNO PARA**

I am in receipt of a request for information from Council relating to the proposal for bulky goods tenancies on the above site. As requested, I have reviewed the request as it relates to traffic and parking matters and provide the following information to assist Council in its assessment of the proposal. For clarity I have repeated the query raised by Council prior to my response.

- *Updated site plan (or similar) demonstrating pedestrian connectivity with appropriate path widths to the north and east developments (internal on private land/within right of way) and external to the site including to Curtis Road and Mingari Street.*

Brown Falconer Drawing No. 3375 DA05 Rev 12 illustrates pedestrian connectivity on the broader integrated development site on the corner of Curtis Road and Mingari Street. A pedestrian connection is shown on the footpath adjacent the subject site.

Access for pedestrians using the bulky goods tenancies will be available to and from this footpath. Accordingly customers who wish to walk between facilities on the site will be able to directly access this footpath from the proposed car park.

It is not proposed to provide a direct footpath access through the bulky goods tenancies to the proposed warehouses as such a connection would be through the loading facilities. There will be minimal demand for pedestrians to walk between the warehouses and the bulky goods tenancies on the broader site as there would not be significant shared use opportunities between these land uses. Should a pedestrian require access between the proposed warehouses and the bulky goods tenancies this will be available via the footpath within the Mingari Street road reserve. This connection will still provide for safe and convenient access for pedestrians.

- *MFY has utilised Bulky Goods Facility traffic generation rate from the Guide to Traffic Generating Developments Technical Direction (TDT 2013/04A) as 1.31 trips per 100m2 in the Report however peak hour trip distribution differs from previous application and extract below. Please provide justification for changes.*

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9 March 2023
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Bulky goods retail stores

Six surveys were conducted in 2009. Two of the surveys were conducted within the Sydney urban area (one electrical goods and one furniture) and four within regional New South Wales (two electrical goods and two furniture). Summary vehicle trip rates are as follows:

Weekday daily vehicle trips = 17 (including 1 heavy) vehicles per 100 m² of gross floor area
 Weekday peak hour vehicle trips = 2.7 vehicles per 100 m² of gross floor area. (note that the morning site peak hour during weekdays does not generally coincide with the network peak hour.)
 Weekend day daily vehicle trips = 19 vehicles per 100 m² of gross floor area (minimal heavy vehicles)
 Weekend day peak hour vehicle trips = 3.9 vehicles per 100 m² of gross floor area.

The above rate documented in the *Guide to Traffic Generating Developments Technical Direction (TDT 2013/04A)* is based on data collected at numerous sites. The results are documented in the *Trip Generation and parking Generation Surveys Bulky Goods/Hardware Stores Analysis Report* prepared by Hyder. This document details the results of the surveys for different traffic periods. An extract of Table 3.4 which is the traffic results summary for the Bulky Goods data is illustrated in Figure 1.

Vehicle-based Trips						
- Site Peak Hour	61	232	26	118	57	35
Trips/ 100m ² GFA	1.42	1.56	4.33	1.96	4.75	2.06
- Network AM Peak	Network AM peak is outside of opening hours					
Trips/ 100m ² GFA						
- Network PM Peak	35	180	Outside of	70	27	19
Trips/ 100m ² GFA	0.81	1.21	opening hrs	1.16	2.25	1.12
Daily Total LV Trips	437	1743	133	898	319	170
Trips/ 100m ² GFA	10.16	11.74	22.17	14.89	26.58	10.00
Daily Total HV Trips	9	0	18	12	28	4
Trips/ 100m ² GFA	0.21	0.00	3.00	0.20	2.33	0.24
Daily Total Vehicle Trips	446	1,743	151	910	347	174
Trips/ 100m ² GFA	10.37	11.74	25.17	15.09	28.92	11.60

Figure 1: Extract of Table 3.4 Trip Generation and parking Generation Surveys Bulky Goods/Hardware Stores Analysis Report which documents traffic generation rates identified during surveys

It can be seen in the above table that the traffic generation rates during the afternoon commuter peak are significantly lower than the peak generated by the site. The rate adopted in the MFY assessment reflects an average of the rates identified for the pm peak which is consistent with the recommended rates in the RMS guide (that is the rates recommended in the Guide are the average of the survey results for the development peak). It is relevant to apply the forecast volume during the pm commuter peak rather than the development peak if the two periods will not coincide.

- *Traffic control signage and line marking internal to the site required. Please review and provide an updated site plan.*

MFY Drawing No. 22-0271_03_SH01 is a signage and linemarking plan for the proposed development.

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9 March 2023
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In summary, the additional plans and information provided have addressed the queries raised by Council in relation to the subject proposal.

Yours sincerely,

MFY PTY LTD

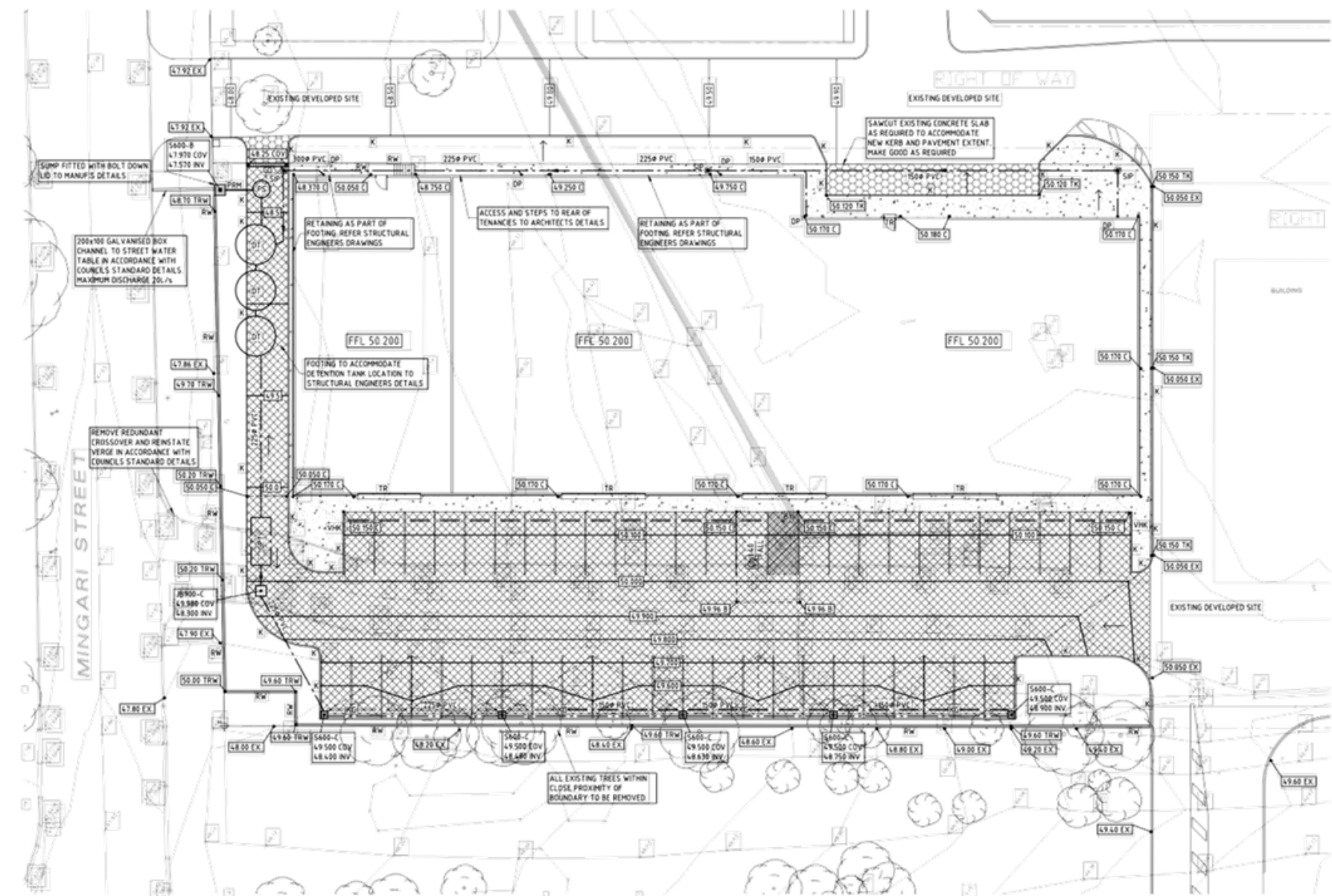
A handwritten signature in black ink, appearing to read 'Melissa Mellen', written over a light blue horizontal line.

MELISSA MELLEN

Director



2010 NATIONAL WINNER
2010 TELSTRA SOUTH AUSTRALIAN
BUSINESS WOMAN OF THE YEAR



LEGEND

- SEWER GRADE PVC STORMWATER PIPE
- 1000 AG PIPE Laid AT FALL
- 5400-X 450 SQUARE GRATED SUMP GRATE CLASS AS NOTED
- 5900-X 900 SQUARE GRATED SUMP GRATE CLASS AS NOTED
- 8900-X 900 SQUARE JUNCTION BOX COVER CLASS AS NOTED
- DP DOWNPIPE
- SP SITE INSPECTION POINT
- GO ACO 5200K POWERDRAIN GRATE (OR APPROVED EQUIV.) WITH CLASS C GRATE. INSTALLED STRICTLY IN ACCORDANCE WITH MANUF'S REQUIREMENTS INCLUDING CONTROL JOINTS WHERE APPLICABLE.
- GPT ECOSOL STORM PIT (CLASS 2) - 2000 L/HW INSTALLED STRICTLY IN ACCORDANCE WITH THE MANUF'S REQUIREMENTS
- DT 3 x 22 TH. BI-INDUSTRIES DETENTION TANK STRICTLY IN ACCORDANCE WITH MANUF'S DETAILS. PROVIDE INSPECTION OPENINGS, BREATHING VALVES, ETC. STRICTLY IN ACCORDANCE WITH MANUF'S DETAILS
- PRM PUMP RISING MAIN TO PUMP MANUF'S DETAILS
- PS GLOBAL WATER DRAINAGE (OR EQUIVALENT) PRE-PACKAGED PUMPING STATION TO MANUF'S DETAILS 17.30L/H PUMP RATE (EQUAL PUMP REFER NOTES) (NO STORMWATER HOLDING CAPACITY IS REQUIRED)
- DESIGN LEVEL: P - PAVING, C - CONCRETE, B - BITUMEN, G - GROUND
- CON - COVER, INV - INVERT, WT - WATER TABLE, EX - EXISTING (E.S.A.)
- CONTOUR LINE
- DIRECTION OF SURFACE FALL
- GRADE LINE
- RW RETAINING WALL TO FUTURE DETAILS
- B1 BOLLARD BY OTHERS
- K KERB
- G GUTTER
- TR THRESHOLD RAMP
- 35mm THICK HOT MIX BITUMEN (NOT SUITABLE FOR HEAVY VEHICLE LOADS) ON 100 THICK FINE CRUSHED ROCK (IPM2/200G / PM4/400G) ON 150 THICK COMPACTED QUARRY RUBBLE (IPM2/200G)
- 100 THICK CONCRETE SLAB WITH SL82 MESH TOP (IN3 CONCRETE) PROVIDE TOOLED CONTROL JOINTS AT 3.0m MAX. C/C ON COMPACTED BASE
- 150 THICK CONCRETE SLAB WITH SL92 MESH TOP (IN3 CONCRETE) ON 150 THICK COMPACTED QUARRY RUBBLE (IPM2/200G)

RETAINING WALL NOTES:

RETAINING WALL HEIGHTS AND/OR LEVELS SHOWN ON THE DRAWING ARE INDICATIVE ONLY. ALL FINISHED RETAINING WALL LEVELS MUST BE CONFIRMED ON SITE BY THE CONTRACTOR. IF RETAINING WALL HEIGHTS ENCOUNTERED ON SITE EXCEED THOSE HEIGHTS SPECIFIED IN THE DETAILS, PLEASE CONTACT THIS OFFICE IMMEDIATELY FOR FURTHER INSTRUCTION.

NOTE:

DRAWINGS SHALL BE READ IN CONJUNCTION WITH ALL OTHER CONSULTANT'S DRAWINGS AS A PACKAGE. REFER TO ARCHITECT'S DRAWINGS FOR ALL SETOUT DIMENSIONS. ALL LEVELS SHALL BE CONFIRMED ON SITE PRIOR TO CONSTRUCTION. SHOULD ANY DISCREPANCY OCCUR THE CONTRACTOR SHALL CONTACT THIS OFFICE IMMEDIATELY FOR FURTHER INSTRUCTION.

CONTRACTORS NOTES:

COVER LEVELS GIVEN FOR PITS ARE NOMINAL ONLY. COVER LEVELS SHALL MATCH FINISHED PAVING LEVELS. WHERE EXISTING SERVICE COVERS ARE FOUND WITHIN THE SCOPE OF THE NEW WORKS, THE CONTRACTOR MUST ALLOW TO ADJUST THE COVERS TO SUIT THE PROPOSED FINISHED SURFACE LEVEL. THE CONTRACTOR IS RESPONSIBLE FOR CHECKING LOCATION OF ALL UNDERGROUND SERVICES PRIOR TO COMMENCING ANY EXCAVATION WORK. ANY DAMAGE CAUSED TO ANY SERVICES SHALL BE REPORTED IMMEDIATELY TO THE SUPERINTENDENT & SHALL BE REPAIRED BY THE APPROPRIATE AUTHORITIES. ALL COSTS ASSOCIATED WITH REPAIRS SHALL BE AT THE CONTRACTOR'S EXPENSE. PHONE 'DIAL BEFORE YOU DIG' (1100) FOR ASSISTANCE. WHERE PROPRIETARY ITEMS ARE SPECIFIED, ALTERNATE EQUIVALENT PRODUCTS MAY BE ADOPTED WITH THE PRIOR WRITTEN APPROVAL OF THIS OFFICE.

PUMP NOTES:

PUMP SHALL BE DUAL PUMP. THE PUMP CONTROLS SHALL BE SET UP TO ENABLE ALTERNATE PUMP OPERATION AT EACH START. IN THE EVENT THAT A PUMP FAILS TO OPERATE WHEN THE WATER LEVEL IN THE WELL REACHES THE PUMP START, THE OTHER PUMP SHALL BE ACTIVATED AND A VISIBLE ALARM INITIATED. IN THE EVENT THAT BOTH PUMPS FAIL TO OPERATE, AN AUDIBLE ALARM SHALL BE INITIATED.

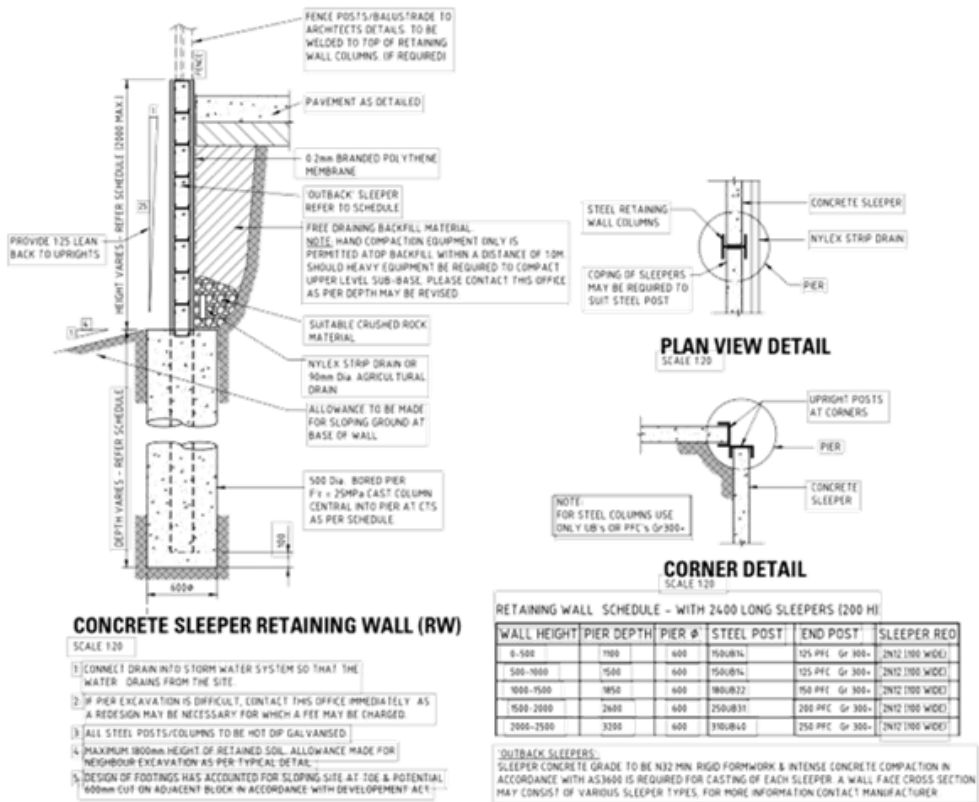
01/02/2023	ISSUED FOR PLANNING APPROVAL	E
18/11/22	SITE UPDATE	D
27/01/23	ISSUED FOR PLANNING APPROVAL	C
21/01/23	PRELIMINARY ISSUE	B
07/01/23	PRELIMINARY ISSUE	A
Date	Revision	Issue



PT Design Pty Ltd 141-143 Houslet Street Adelaide SA 5000
T: 08 9412 4300 E: info@ptdesign.net.au

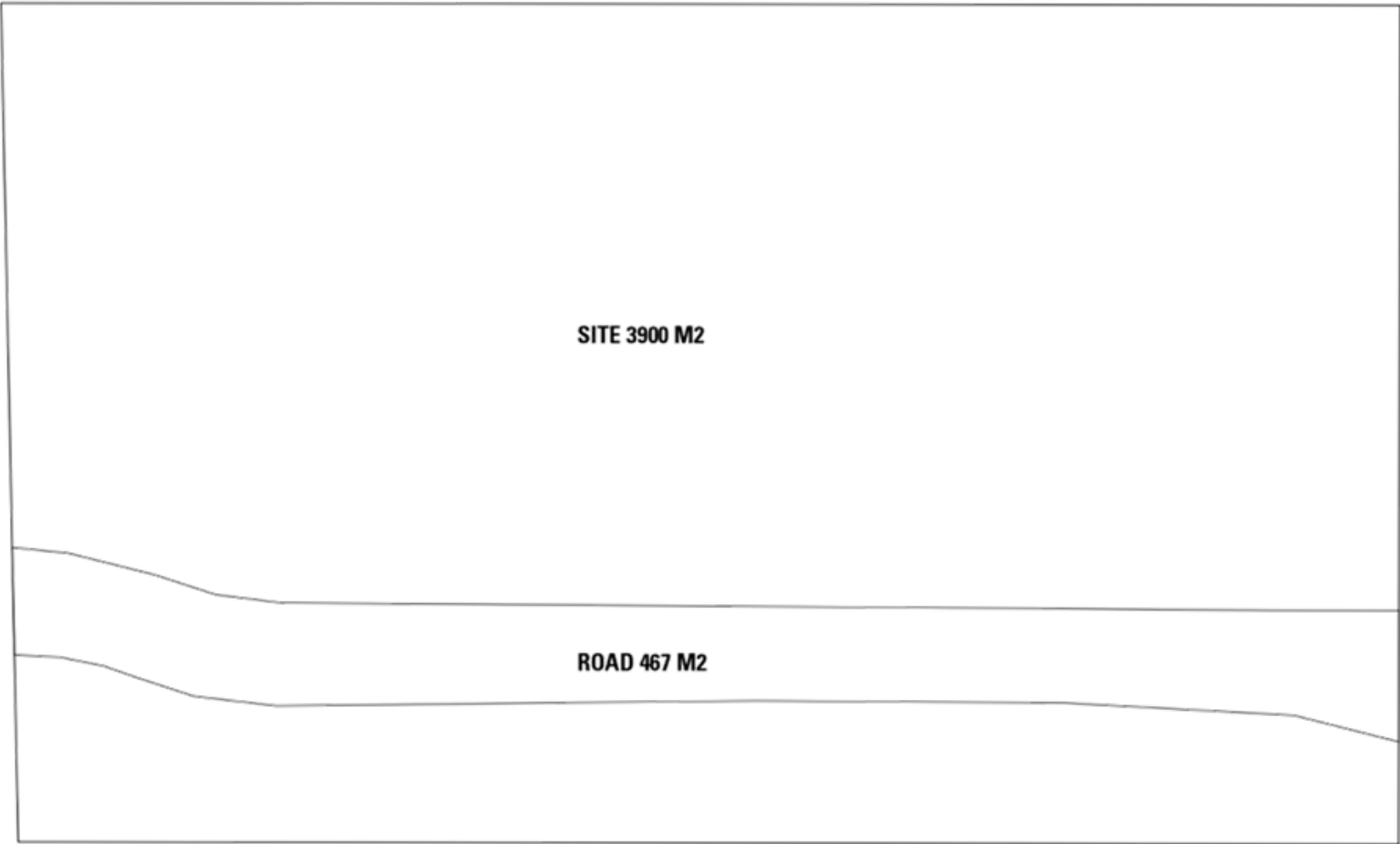
Designed	JB	Drawn	SR
Approved		Date	OCT 22
Civil		Sheet	1 OF 2

Project	PROPOSED DEVELOPMENT CNR MAIN NORTH ROAD & CURTIS ROAD MUNNO PARA
Client	LEYTON PROPERTY
Drawing Title	SITE LEVELS AND DRAINAGE LAYOUT
Drawing Number	23148-C01
Scale	1:200
Issue	E

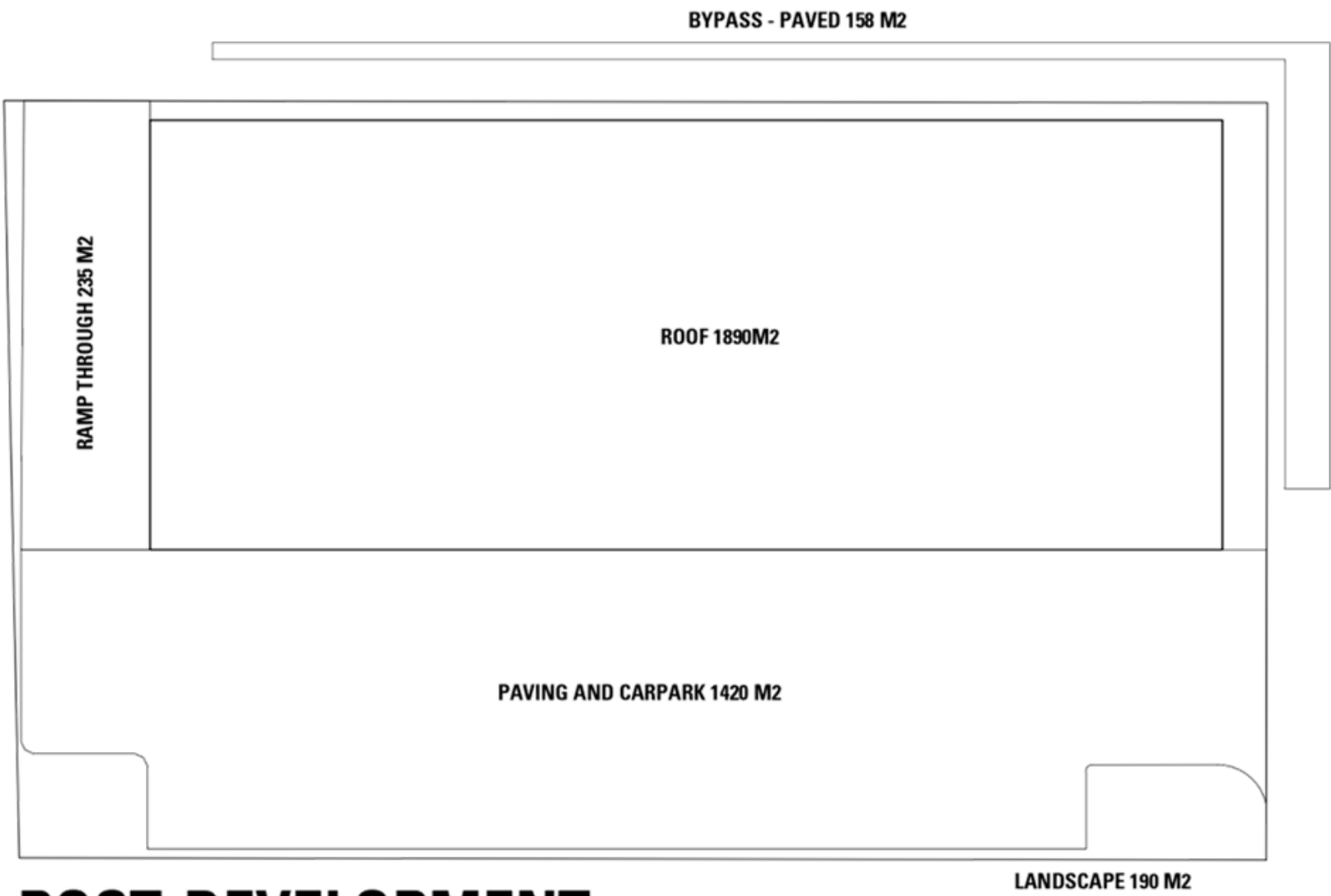


PRELIMINARY

07/02/2023		ISSUED FOR PLANNING APPROVAL		A	
Date	Revision			Issue	
PT Design structural • civil • engineers					
PT Design Pty Ltd 141-143 Houslet Street Adelaide SA 5000 T (08) 9412 4300 E [info@ptdesign.net.au]					
Designed	-	Drawn	SR		
Approved		Date	FEB 23		
Civil		Sheet	2 OF 2		
Project PROPOSED DEVELOPMENT CNR MAIN NORTH ROAD & CURTIS ROAD MUNNO PARA					
Client LEYTON PROPERTY				N +	
Drawing Title DETAILS SHEET				Scale VARIES	
Drawing Number 23148-C02				Issue A	



PRE-DEVELOPMENT



POST-DEVELOPMENT



structural • civil • engineers

ABN [35 008 116 916]

W [ptdesign.com.au]

T [08 8412 4300]

PT DESIGN Pty Ltd 141-149 Ifould Street Adelaide SA 5000

DETENTION CALCULATIONS

Curtis Road (Cnr Main North Road), Munno Para - Stage 2

Prepared by:

PT Design ABN 35 008 116 916
141-149 Ifould Street, ADELAIDE SA 5000
Tel: (08) 8412 4300

Project No: 23148
Revision: -00-
Date of Issue: 07/02/2023



Project:	Curtis Road (Cnr Main North Road), Munno Para	Project #	23148
	- Stage 2	Date	07.02.2023
Design By:		Page	1

1% AEP DETENTION VOLUME

PRE-DEVELOPMENT FLOW - 20% AEP

Duration	5 mins
Exceedance per Year (EY)	78.30 mm/hr

Catchment Area	C	Area (m ²)	
Roof	0.90	0	0.00
Impervious	0.85	467	8.63
Pervious	0.20	3433	14.93
Total			23.57 L/sec

POST-DEVELOPMENT FLOW - 1% AEP (capped at 20L/sec)

Duration	mins (critical TBC)
Annual Exceedance Probability (AEP)	1% AEP mm/hr

BYPASSING DETENTION

Catchment Area	C	Area (m ²)	
Roof	0.90	0	0.00
Impervious	0.85	157	6.26
Pervious	0.20	0	0.00
Total			6.26 L/sec

Max allowable discharge rate from pump system	17.30 L/sec
---	-------------

Catchment Area	C	Area (m ²)	
Roof	0.90	1890	0.47
Impervious	0.85	1655	0.39
Pervious	0.20	190	0.01
Total			0.87 1% AEP

Site Area (m²)
3892

Pumped System



Project: Curtis Road (Cnr Main North Road), Munno Para	Project # 23148
- Stage 2	Date 07.02.2023
Design By:	Page 2

CRITICAL STORAGE VOLUME

Q in	Q out
<u>0.87</u> 1% AEP	<u>17.30</u> L/sec

Duration (mins)	1% AEP (mm/hr)	Q in (L/sec)	Pump initiated t (mins)	V total (L ³)	
5	169	147.7	0.6	34529	
10	124	108.4	0.8	50269	
20	84.9	74.2	1.2	64282	
30	66.4	58.0	1.5	69652	CRITICAL
60	42.4	37.0	2.3	68324	
90	32.2	28.1	3.1	56508	
120	26.5	23.2	3.7	40835	
180	20	17.5	5.0	1827	
360	12.2	10.7	8.1	-140231	
720	7.37	6.4	13.4	-460502	

PEAK STORAGE REQUIRED 69652 L³

PUMP DISCHARGE RATE 17.30 L/sec

Allow for 3 x Ri-Industries 22.73kL Underground Concrete Tanks.

Minor capacity difference to be made up in pits and pipes.



RSEA Lot 479 Mingari Street, Munno Para

Environmental Noise Assessment

24 November 2022
Reference ID: 126-3



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Abbreviations

DO	Desired Outcome of the Code
DTS	Deemed to Satisfy criteria of the Code
EPA	South Australian Environment Protection Authority
PO	Performance Outcome of the Code
WHO	World Health Organization

Glossary

A-weighting	A mathematical adjustment to the measured noise levels to represent the human response to sound. An <i>A-weighted noise level</i> is presented as dB(A)
Ambient noise level	The noise level associated with the environment in the absence of the activity under investigation
Characteristic	A characteristic determined in accordance with the <i>Environment Protection (Noise) Policy 2007</i> (the Policy) to be fundamental to the nature and impact of the noise. For example, a noise source is deemed to exhibit a characteristic if it produces distinctive tonal, impulsive, low frequency or modulating features
Code	<i>Planning and Design Code</i> Version 2022.21 dated 10 November 2022, PlanSA
Day	A period defined by the <i>Environment Protection (Noise) Policy 2007</i> as between 7am and 10pm
EP Act	<i>Environment Protection Act 1993</i>
Equivalent noise level	The A-weighted noise level which is equivalent to a noise level which varies over time. The descriptor is L_{Aeq} and it is the A-weighted <i>source noise level (continuous)</i> referenced in the Policy. The L_{Aeq} is also referenced as an average noise level in this assessment for simplicity
dB	The logarithmic unit of measurement to define the magnitude of a fluctuating air pressure wave. Used as the unit for <i>sound</i> or <i>noise level</i>
Indicative Noise Level	The noise level assigned by the Policy at a location to represent an impact on the acoustic amenity at that location. No further action is required to be taken under the <i>Environment Protection Act 1993</i> for noise levels which are lower than the Indicative Noise Level
Night	A period defined by the <i>Environment Protection (Noise) Policy 2007</i> as between 10pm and 7am
Noise	An interchangeable term with sound but which is most often described as <i>unwanted sound</i>

Noise Sensitive Premises or Receiver	Premises that could be "noise-affected". For the purposes of this assessment, the noise sensitive premises (or <i>receivers</i> referenced by the Code) are residential dwellings. Commercial properties are not considered sensitive to the sources of noise considered in this assessment.
Policy	The <i>Environment Protection (Noise) Policy 2007</i>
Sound	An activity or operation which generates a fluctuating air pressure wave. The ear drum can perceive both the frequency (pitch) and the magnitude (loudness) of the fluctuations to convert those waves to sound
Sound pressure level	The magnitude of sound (or noise) at a position. The sound pressure level can vary according to location relative to the noise source, and operational, meteorological and topographical influences
Sound power level	The amount of sound energy an activity produces for a given operation. The sound power level is a constant value for a given activity. The sound power level is analogous to the power rating on a light globe (which remains constant), whereas the lighting level in a space (sound pressure level in this analogy) will be influenced by the distance from the globe, shielding and different locations within the space.
WHO Guidelines	<i>Guidelines For Community Noise</i> Birgitta Berglund Thomas Lindvall Dietrich H Schwela London, United Kingdom, April 1999, World Health Organization

Executive Summary

A new RSEA store is proposed at Lot 479 Mingari Street, Munno Para (the facility).

The facility is well located from a noise perspective, being a relatively low intensity retail land use on a major road within an *Employment Zone*. Furthermore, the operating hours for the facility vary between 7:30am and 6:00pm depending on the day of the week.

The noise sources at the facility include passenger vehicles in the carpark, deliveries of safety products, the collection of waste bins and the operation of mechanical services (air conditioning systems).

The assessment predicts noise levels at the closest dwellings based on established inputs from similar facilities and manufacturer's noise data for the mechanical services at a comparable store.

The predicted noise levels are compared against standards in the *Environment Protection (Noise) Policy 2007* (the Policy) to ensure the facility does not adversely impact on the amenity of any dwelling in the locality in accordance with the relevant provisions of the *Planning and Design Code* (the Code).

This assessment determines that the facility can achieve the *Environment Protection (Noise) Policy 2007* without any specific acoustic treatments to the built form and where the private collection of waste from the store occurs between 7am and 7pm Monday to Saturday and not on public holidays or Sundays.

Introduction

A new RSEA safety product business to business retail store is proposed at Lot 479 Mingari Street, Munno Para (the facility).

The location of the facility, the closest dwellings, and the *Planning and Design Code* zones are shown in Figure 1 below.

Figure 1 The facility and surrounding locality



The facility is well located from a noise perspective, being a relatively low intensity retail land use on a major road located within an *Employment Zone*. Furthermore, the operating hours for the facility vary between 7:30am and 6:00pm depending on the day of the week.

The noise sources at the facility include passenger vehicles in the carpark, deliveries, the collection of waste bins and the operation of mechanical services (air conditioning systems). The assessment predicts noise levels at the closest dwellings based on established inputs from similar facilities and manufacturer's noise data for the mechanical services at a comparable store.

Assessment Criteria

The Planning and Design Code

The facility is in an *Employment Zone* of the *Planning and Design Code Version 2022.21* dated 10 November 2022 (the Code). The dwellings located to the east are in a *Master Planned Neighbourhood Zone* and the dwellings to the south are in a *General Neighbourhood Zone*. The following provisions within the Code are considered relevant to the environmental noise assessment.

Employment Zone (Part 2 - Zones and Sub Zones)

Performance Outcome PO 1.1

A range of employment-generating light industrial, service trade, motor repair and other compatible businesses servicing the local community *that do not produce emissions that would detrimentally affect local amenity*.

Interface between Land Uses (Part 4 – General Development Policies)

Desired Outcome DO 1

Development is located and designed to *mitigate adverse effects* on or from neighbouring and proximate land uses.

Performance Outcome PO 1.2

Development adjacent to a site containing a sensitive receiver (or lawfully approved sensitive receiver) or zone primarily intended to accommodate sensitive receivers *is designed to minimise adverse impacts*.

Performance Outcome PO 2.1

Non-residential development *does not unreasonably impact the amenity of sensitive receivers* (or lawfully approved sensitive receivers) or an adjacent zone primarily for sensitive receivers through its hours of operation having regard to:

- a) the nature of the development
- b) measures to mitigate off-site impacts
- c) the extent to which the development is desired in the zone
- d) measures that might be taken in an adjacent zone primarily for sensitive receivers that mitigate adverse impacts without unreasonably compromising the intended use of that land.

Performance Outcome PO 4.1

Development that emits noise (other than music) *does not unreasonably impact the amenity of sensitive receivers* (or lawfully approved sensitive receivers).

Deemed to Satisfy Criteria DTS 4.1

Noise that might affect sensitive receivers achieves the relevant *Environment Protection (Noise) Policy* criteria.

Performance Outcome PO 4.2

Areas for the on-site manoeuvring of service and delivery vehicles, plant and equipment, outdoor work spaces (and the like) are designed and sited to *not unreasonably impact the amenity of adjacent sensitive receivers (or lawfully approved sensitive receivers) and zones primarily intended to accommodate sensitive receivers* due to noise and vibration by adopting techniques including:

- a) locating openings of buildings and associated services away from the interface with the adjacent sensitive receivers and zones primarily intended to accommodate sensitive receivers
- b) when sited outdoors, locating such areas as far as practicable from adjacent sensitive receivers and zones primarily intended to accommodate sensitive receivers
- c) housing plant and equipment within an enclosed structure or acoustic enclosure
- d) providing a suitable acoustic barrier between the plant and / or equipment and the adjacent sensitive receiver boundary or zone.

The Policy

Interface between Land Uses DTS 4.1 references the *Environment Protection (Noise) Policy 2007* (the Policy).

The Policy was developed under the *Environment Protection Act 1993* (the EP Act). The EP Act incorporates a requirement to ensure the acoustic amenity of a locality is not unreasonably interfered with. The Policy provides a quantitative approach to satisfy this requirement underpinned by the World Health Organization's *Guidelines for Community Noise* (WHO Guidelines) as it relates to community annoyance and sleep disturbance.

Compliance with the Policy will satisfy *Interface between Land Uses DTS 4.1*; and is considered to also satisfy the subjective requirements of the Desired and Performance Outcomes in the Code (being *Employment Zone PO 1.1*, and *Interface between Land Uses DO 1*, *PO 1.2*, *PO 2.1*, *PO 4.1*, and *PO 4.2*).

The Policy establishes noise levels that apply at noise sensitive premises for both the day (7.00am to 10.00pm) and night (10.00pm to 7.00am the following day). The noise levels vary according to the land use zoning in which the facility and the dwellings are located.

This assessment has applied the *equivalent noise level* criteria that applies to a new development, being the *Indicative Noise Level* minus 5 dB(A).

In this situation, the relevant noise level that applies at dwellings for the operation of a new facility in the *Employment Zone* with operating hours varying between 7:30am and 6:00pm depending on the day of the week is an *equivalent noise level* of 51 dB(A).

The *equivalent noise level* of 51 dB(A) is to be considered over the Policy's default assessment period of 15 minutes at the dwellings in the *Master Planned Neighbourhood Zone* and the *General Neighbourhood Zone*.

When predicting noise levels for comparison to the Policy, the predicted *equivalent noise levels* are to be adjusted (increased) where the activities exhibit "annoying" characteristics (dominant tonal, impulsive, low frequency content or modulation characteristics) in comparison to the surrounding ambient environment.

For waste collection, the Policy effectively restricts private collection (as distinct to public collection occurring at the same time as other surrounding dwellings) to between 7am and 7pm Monday to Saturday and not on public holidays or Sundays.

Assessment

Noise Data Inputs

The noise level from the facility has been based on noise level measurement data from similar facilities including carpark activity, truck movements, and unloading activity. In addition, manufacturer's noise data for the proposed air-conditioning selection at a comparable store has been utilised for the noise from mechanical services.

Operational Assumptions

The following inputs have been utilised for the assessment over the default 15-minute period of the Policy and are the basis for the predicted noise levels:

- one heavy rigid vehicle (HRV) truck movement in and out of the loading area in any 15-minute period, with a sound power level of 96 dB(A) per arrival/departure (vehicle manoeuvring, opening, and closing doors)
- continuous unloading of a HRV truck with the engine off whilst using a manual pallet jack, with a sound power level of 87 dB(A) when moving pallets
- 20 passenger vehicle movements in the car park with a sound power level of 81 dB(A) per arrival or departure (manoeuvring into the parking space, opening and closing doors and conversing)
- three roof mounted packaged air-conditioning units each with a sound power level of 79 dB(A) (based on a Temperzone "OPA705RKTBG-P" selection).

Existing Environment

Measurements were conducted in the vicinity of the facility on 21 October 2022 using a calibrated *Rion NL-42 sound level meter* to establish the noise level in the existing ambient environment at a location which represents the closest dwellings. The measurements indicated that the noise was controlled by vehicle movements on Curtis Road and Mingari Street. The noise modulated with traffic flow (both collectively and for individual vehicles) as it passed through the intersection of Main North Road and Curtis Road.

Predicted Noise Levels

The noise from the facility has been predicted using the noise calculation method provided by the *International Standard ISO1613-2:1996 "Acoustics - Attenuation of sound during propagation outdoors - Part 2 General method of calculation"*, and the operational assumptions summarised above. The results of the noise predictions are summarised in Table 1 for the closest dwellings (circled in Figure 1).

When predicting noise levels for comparison with the Policy, the noise levels are to be adjusted (increased) where the activities exhibit "annoying" characteristics (dominant tonal, impulsive, low frequency content or modulation characteristics) in comparison to the surrounding ambient environment.

In this circumstance an adjustment is not applicable to the predicted noise levels for "annoying" characteristics (dominant tonal, impulsive, low frequency content or modulation characteristics) on the basis that the predicted levels are not dominant in either overall level or character when compared to the noise from vehicles in the ambient environment.

Table 1 Predicted Noise Levels dB(A)

Dwelling	Predicted cumulative noise level (dB(A))		Compliance
	Day		
	L _{Aeq}		
Criterion	51		
1	39		Yes
2	45		Yes
3	46		Yes
4	36		Yes

Based on the above, the facility can easily achieve the Policy requirements without any specific noise control measures to the built form or to the operations, other than that associated with private waste collection, should that occur at the facility.

Noise Control Measures

Waste Collection

It is recommended the hours of private waste collection from the facility (as distinct to waste collection which occurs at the same time as other dwellings in the area) occur between 7am and 7pm Monday to Saturday and not on public holidays or Sundays.

Conclusion

A new RSEA store is proposed at Lot 479 Mingari Street, Munno Para (the facility).

The facility is well located from a noise perspective, being a relatively low intensity retail land use, on a major road located within an *Employment Zone*. Furthermore, the operating hours for the facility vary between 7:30am and 6:00pm depending on the day of the week.

The assessment predicts noise levels at the closest dwellings based on manufacturer's noise data and measurements conducted of similar processes. The predicted noise levels are compared against standards in the *Environment Protection (Noise) Policy 2007* to ensure the facility does not adversely impact on the amenity of the locality in accordance with the relevant provisions of the *Planning and Design Code*.

This assessment determines that the facility can achieve the *Environment Protection (Noise) Policy 2007* without any specific acoustic treatments to the built form and where the private collection of waste from the store occurs between 7am and 7pm Monday to Saturday and not on public holidays or Sundays.

Based on the above, this assessment concludes the proposed business to business retail facility will not adversely impact on the amenity of any dwelling in the locality, and will meet the relevant *Environment Protection (Noise) Policy 2007* and *Planning and Design Code* provisions.

References

Brown Falconer "Munno Para Development" (Job "2020082") drawings "3375 DA01" to "3375 DA06 dated 17 November 2022

Environment Protection (Noise) Policy 2007, SA EPA

Guidelines For the Use of The Environment Protection (Noise) Policy 2007, SA EPA June 2009

Guidelines For Community Noise Birgitta Berglund Thomas Lindvall Dietrich H Schwela London, United Kingdom, April 1999, World Health Organization

Lucid "Roof Plan Air Conditioning and Ventilation Arrangement" drawing "LCE22561-M101" dated June 2022 (for a comparable store)

MFY HRV Traffic Analysis Drawing MFY_2201117_04_SK02 B indicating the HRV swept path and movement through the site

Planning and Design Code Version 2022.21 dated 10 November 2022, PlanSA

Temperzone "Air Cooled Packaged Units Technical Data - OPA 465, OPA 550, OPA 705, OPA 855, OPA 960 (ECO)"

Document History

Distribution:

Issue date	24 November 2022
Issued to	Future Urban
Description	Environmental Noise Assessment

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Details of Representations

Application Summary

Application ID	22039894
Proposal	Construction of a building comprising two bulky goods tenancies with associated advertising and retaining
Location	LOT 10 MINGARI ST MUNNO PARA SA 5115

Representations

Representor 1 - Carol Bennett

Name	Carol Bennett
Address	3, Myall Ave MUNNO PARA SA, 5115 Australia
Submission Date	21/01/2023 05:02 PM
Submission Source	Online
Late Submission	No
Would you like to talk to your representation at the decision-making hearing for this development?	No
My position is	I support the development with some concerns

Reasons

I live 2 houses from Mingari Street and when i bought my home here it was a quiet place and Curtis Road didn't come all the way to Main North Rd, i realise things change but i didn't want to live next to a lot of businesses if that was so i would have purchased a home somewhere else. My biggest problem with this development is the traffic movements, as it is now, the traffic on Curtis Rd can be a real problem and having a driveway on Mingari Street will only make things more difficult for us and many others who use this as a way to leave Munno Para, line ups on Mingari street are a common thing, waiting to get onto Curtis Rd, which of course will also be affected by another driveway into this development. Anyway, just wanting to let you know that this will affect our life and many others. The little road into the Bowling club was not a problem as the amount of traffic that went in there was minimal. I do hope the noise will be kept down, but of course that is not something you can control. Thanks for listening, nobody will listen but i have had my say.

Attached Documents



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February 16, 2023

Danni Biar
City of Playford
Via the PlanSA Portal

Dear Danni,

RE: RESPONSE TO REPRESENTATIONS (DA 22039894)

I refer to the proposed development application for the construction of a building comprising two bulky goods tenancies with associated advertising and retaining at lot 10 Mingari Street, Munno Para.

Public notification has been undertaken and completed with one representation received supporting the development but with concerns regarding traffic increase and noise. They do not wish to be heard.

Based on the opinion of an independent and well-regarded professional traffic engineering firm, the proposed increase in traffic volumes is low and will have no appreciable impact on the road network.

An acoustic report was prepared by Echo Acoustic, advising that the proposal has no unreasonable adverse impacts to neighbouring land uses due to the separation distance and siting of the entrances and carpark.

All of the matters raised in the representations have been adequately addressed in the application documents.

Please do not hesitate to contact me if you need anything further.

Yours sincerely,

A handwritten signature in black ink, appearing to read "M. Duncan", written over a horizontal line.

Marc Duncan
Director