



NOTICE

of

COUNCIL ASSESSMENT PANEL MEETING

Pursuant to the provisions of Section 82 of the Planning, Development and Infrastructure Act 2016

TO BE HELD IN

**COUNCIL CHAMBERS
PLAYFORD CIVIC CENTRE
10 PLAYFORD BOULEVARD, ELIZABETH**

MEMBERS MAY PARTICIPATE BY ELECTRONIC MEANS

ON

THURSDAY, 16 NOVEMBER 2023 AT 6:00PM

THIS MEETING WILL ALSO BE VIEWABLE AT
<https://www.youtube.com/user/CityOfPlayford>

**ADAM SQUIRES
ASSESSMENT MANAGER**

Issue Date: Thursday, 9 November 2023

MEMBERSHIP

MR GEOFF PARSONS – PRESIDING MEMBER

Mr Aaron Curtis
Mr Paul Mickan

Ms Cherie Gill (*Deputy*)
Ms Misty Norris

Mr Nathan Grantham
Ms Jane Onuzans (*Deputy*)

**City of Playford
Council Assessment Panel Meeting**

AGENDA
THURSDAY, 16 NOVEMBER 2023 AT 6:00PM

ACKNOWLEDGEMENT OF COUNTRY

We would like to acknowledge that this land we meet on today is the traditional land of the Kaurna people, and that we respect their spiritual relationship with their country. The City of Playford would also like to pay respects to Elders past, present and emerging.

1 ATTENDANCE RECORD

- 1.1 Present
- 1.2 Apologies
- 1.3 Not Present

2 CONFIRMATION OF MINUTES

RECOMMENDATION

The Minutes of the Council Assessment Panel Meeting held 19 October 2023 be confirmed as a true and accurate record of proceedings.

3 APPLICATIONS WITHDRAWN

4 DECLARATIONS OF INTEREST

5 APPLICATIONS FOR CONSIDERATION – PERSONS WISHING TO BE HEARD

- 5.1 Child care facility with ancillary acoustic boundary fencing, retaining wall and associated advertisement (ID 23024476) (Attachments)6

Representors: Frank Grillo
Applicant: Development Holdings Pty Ltd

6 APPLICATIONS FOR CONSIDERATION – NO PERSONS TO BE HEARD

Nil

7 APPLICATIONS FOR CONSIDERATION - CATEGORY 1

Nil

8 OUTSTANDING MATTERS – APPEALS AND DEFERRED ITEMS

Nil

9 OTHER BUSINESS**9.1 STAFF REPORTS****Matters to be considered by the Committee and referred to Council***Matters which cannot be delegated to a Committee or Staff*

9.1.1 Council Assessment Panel - Terms of Reference (Attachments)115

10 CONFIDENTIAL MATTERS

Nil

11 POLICY DISCUSSION FORUM

Nil

12 CLOSURE

APPLICATIONS FOR CONSIDERATION

APPLICATIONS FOR CONSIDERATION – PERSONS WISHING TO BE HEARD

5.1 CHILD CARE FACILITY WITH ANCILLARY ACOUSTIC BOUNDARY FENCING, RETAINING WALL AND ASSOCIATED ADVERTISEMENT (ID 23024476)

Snapshot

Author:	Danni Biar
Proposal:	Child care facility with ancillary acoustic boundary fencing, retaining wall and associated advertisement
Development Number:	23024476
Date of Lodgement:	11 September 2023
Owner:	Vincenzo Cavallaro and Rosa Cavallaro
Applicant:	Development Holdings Pty Ltd
Location:	37 Main North Road, Smithfield
Zone:	General Neighbourhood
Classification:	Performance Assessed
Public Notification:	Yes
Representation Received:	Yes
Request for Additional Information Made?	Yes

Recommendation:	To Grant Planning Consent
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Attachments:	1 . Planning Report 2 . Architectural Plans 3 . Environmental Noise Assessment Report 4 . Traffic and Car Parking Report 5 . Stormwater Management Plan 6 . Representation 7 . Response To Representation 8 . Referral Response Commissioner Of Highways
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1. The Subject Land

The subject land comprises one allotment, legally described on Certificate of Title Volume 5819 Folio 112 or otherwise known as 37 Main North Road Smithfield. The features of the subject land include:

- A frontage of 31.85 metres to Main North Road, which is a State Maintained Road
- A depth of 63.760 metres
- A site area measuring some 2,026 m²
- The subject land is regular and rectangular in shape and gently slopes towards the rear of the property but generally flat
- The subject land currently accommodates a single storey masonry dwelling and domestic structures. A low level masonry fence with gates extends across a portion of the front property boundary with sheet metal fencing for the remainder of the front property boundary
- Trees, which are neither regulated or significant, are dispersed across the rear yard of the existing dwelling.

A bus stop and stobie pole are situated generally in line with the northern property boundary. Associated with the development proposed below, the bus stop will be relocated further south to enable a new access point off Main North Road for the proposed child care centre.

2. The Locality

The adjacent locality is characterized by residential development fronting Main North Road and adjacent residential streets. Apart from a two storey dwelling on the site directly to the south of the subject land, dwellings are predominantly single storey in nature. The wider locality includes a petrol filling station situated further north, and other commercial development comprising another petrol filling station, offices and service industry further south along Main North Road.

The locality is dominated by the high traffic corridor of Main North Road, being a State Maintained Road under the care and control of the Department of Infrastructure and Transport. The well landscaped road reserve of Main North Road provides a visual barrier between the western side of Main North Road and the development to the east of Main North Road.

2.1 Locality Plan



Source: Nearmaps, September 2023

2.2 Zoning

By virtue of its location, the land is entirely within:

- The General Neighborhood Zone.
- Covered by the following Overlays:
 - Defence Aviation Area (All structures over 90 metres)
 - Future Road Widening

- Major Urban Transport Routes
- Prescribed Wells Area
- Regulated and Significant Tree
- Stormwater Management
- Traffic Generating Development
- Urban Tree Canopy
- Has the following Technical Numeric Variations (TNVs):
- Concept Plan 81 (Defence Airfield Lighting Constraints)

3. The Proposal

The Applicant seeks to demolish all existing structures on the site and construct a childcare centre with associated carparking, signage and masonry fence.

The proposed single storey building will accommodate up to 84 children, which are younger than primary school age, from 6:30am to 6:30pm Monday to Friday. The building will have a total area of 690m² with 5 activity rooms equating to an area of 286m². The building also includes a cot room, reception area, kitchen, office, laundry, staff room, toilets, and store areas.

Outside play areas of approximately 591m² will be located to the north, west and south of the building, with the remainder of the site utilised for car parking, waste storage and landscaping.

Twenty one car parking spaces, including one disabled car parking space will be located at the front of the allotment. Parking layout demonstrates two designated staff parking spaces. Egress and ingress will be provided by a 6.2 metres crossover onto Main North Road, which is a state-maintained road.

An advertising display to identify the child care facility is proposed on the front property boundary consisting of a masonry wall with a height of 1.35 metres in height and a width of 2.82 metres. An additional sign will be installed on the front facade of the building with a dimension of 0.81 metres in height and 1.76 metres in length. Both of the advertisements will be non-illuminated.

Site works are required to provide a provide a level outdoor play space and building to meet the relevant standards for a child care facility. The land falls to the rear and as a consequence will have retaining walls up to 1.1 metres at the north west corner, 0.9 metres at the south west corner and the western (rear) boundary proposes a 1.1 metres high retaining wall. To manage the noise impacts on the adjacent dwellings, a portion of the southern (side) boundary will include 2.4 metres and 2.7 metres high fencing. It is noted that this will not be installed onto the retaining walls on the boundary. The 2.7 metres high section will be limited to a length of 7.3 metres and includes an acoustic canopy connecting to the proposed building. This results in a combined fence & retaining wall height of:

- 2.7-2.9 metres on the western (rear) boundary
- 2.1-2.7 metres on the northern (side) boundary
- 2.1-3.0 metres on the southern (side) boundary

A landscape plan has been prepared by Das Studio that proposes the planting of a number of native species in the form of tall trees, low level shrubs and groundcovers adjacent the building and car park to soften their appearance.

4. Procedural Matters

4.1 Classification

The proposed development comprises of a child care facility, fencing, retaining walls and advertising.

The above elements are not classified as an Accepted, Deemed-to-Satisfy or Restricted development within the relevant Tables of the Zone. The proposed development is therefore a Code Assessed - Performance Assessed development pursuant to Sections 105(b) and 107 of the Planning, Development and Infrastructure Act 2016 (the Act), requiring an on-merit assessment against the relevant provisions of the Code.

4.2 Public Notification

Generally, all classes of performance assessed development require public notification unless, pursuant to Section 107(6) of the Act, the class of development is excluded from notification by the Code in Table 5 – Procedural Matters (PM) - Notification of the relevant Zone.

Public notification was required as the child care facility did not meet any of requirements of Table 5 to be excluded from notification.

The notification process commenced on 21 September 2023 and closed on 12 October 2023 and Council received one representation as detailed below:

Representor	Summary of Issues Raised	Wish to be heard
Mr. Frank Grillo 35 Main North Road Smithfield	<ul style="list-style-type: none">• Land use• Traffic• Noise• Property value	Yes

The Applicant's Planning Consultant has responded to the representation addressing the following concerns:

- General Neighbourhood Zone contemplates a child care facility
- Development has been designed to manage the interface with surrounding residential uses to maintain the residential character and amenity
- Based on Sonus Environmental Noise Assessment Report noise generated by children playing outdoors will not exceed 50dB(A) at the closest dwellings
- Noise levels generated from the proposal is well below the maximum noise level measured under the existing condition on the site, which measures at 67 dB(A) in correlation with the typical vehicle passbys along Main North Road
- CIRQA have confirmed that the additional vehicle movements will result in a *"minimal impact on the adjacent road network"*
- The perceived impact on 'property value' is not relevant to planning assessment

4.3 Statutory Referrals

The proposed development includes alterations to an access onto a State Maintained Road within the Urban Transport Route Overlay pursuant to Schedule 9(3)(7) of the Planning, Development, and Infrastructure Regulations 2017.

A referral to the Commissioner of Highways was required and a response was received 26 October. The Commissioner has no objection to the proposed development, subject to the inclusion of a number of conditions of approval, which are included within the staff recommendation.

4.4 Internal Referrals

Internal referrals were undertaken to Council staff to review traffic management, car parking, stormwater and the future use comprising of food preparation. Council's Health Team raised no concerns with the future facility.

Council's Engineering Team have considered stormwater and internal traffic movements on site. Stormwater design has appropriate discharge rates and locations which meet the requirements of Council standards. Further, the CIRQA report has demonstrated that the proposed car parking area can accommodate safe vehicle movements and an acceptable number of car parking spaces.

5. Key Issues

The following matters are considered pertinent in reaching a recommendation for the proposal:

- Whether the proposal is an appropriate form of development in the General Neighbourhood Zone
- Whether the proposal is consistent with the general policies of the Planning and Design Code that relate to a Child Care Facility
- Whether the proposal will create an adverse impact and conflict between other land uses within the locality
- Whether the development will create an adverse traffic impact on the existing road network in the locality

6. Planning Assessment

6.1 Land Use and Intensity

The General Neighbourhood Zone anticipates non-residential development outside of activity centres that support local needs of the community.

Childcare centres within residential areas serve the local population to which they are located as by their nature, users of such facilities will use those centres that are most convenient to their needs. In addition, the siting of the childcare centre on an arterial road, results in a highly accessible location within a neighbourhood setting without loss or harm to the nature of the Zone or impacting the residential road network through any increase in vehicle movements.

The proposed number of children to be accommodated is considered to be of an appropriate scale outside of an activity centre zone and complements the Zone in

accordance with Performance Outcome (PO) 1.1. Furthermore, Designated Performance Feature (DPF) 1.1 of the Zone identifies that a "Child Care Facility" is an envisaged type of development within the Zone.

Further, the facility servicing only 84 children is considered to complement the existing residential character and amenity of the neighbourhood, in accordance with PO 1.3.

Based on the above it is considered that the child care facility is an appropriate land use, is of a scale that will maintain the existing residential character and amenity and is well located and accessible on an arterial road.

6.2 Built Form, Height and Character

The building is proposed with a range of external materials including textured rendered hebel, white axon vertical cladding, timber decowood cladding with a grey Colorbond roof. The materiality proposed is considered to provide a modern building that will complement the immediate residential character and amenity of the neighbourhood, in accordance with PO 1.3.

The proposed building is considered to contribute to the low-rise suburban character as the single storey building with a maximum wall height of 4 metres and total height of 5.826 metres, is within the 2 building levels as sought by the 'low-rise' definition and achieving the equivalent DPF 4.1 seeking no greater than two levels, 7 metres wall height and 9 metres in total height.

The bin storage area is screened and located behind extensive landscaping south of the building and next to the car park, which satisfies PO 1.5. Further, the mechanical plant will not be visible from the public realm, being located behind the Laundry/Dry Court Area.

The proposed development provides a site coverage area of 853.7sqm which is 43% coverage of the site. This satisfies DPF 3.1 which requires that development does not result in site coverage exceeding 60%.

The dwelling to the north has a setback of 9 metres and the dwelling to the south has a setback of 7 metres from the primary frontage. Accordingly, the proposed setback of 19.01 metres satisfies DPF 5.1 as siting of the proposed building is considered to appropriately contribute to the existing pattern of development in the streetscape and is not considered to have a detrimental impact on the streetscape. The side setback provided is much greater than what is sought by Zone DPF 8.1, thus considered to provide sufficient separation between the building walls.

The rear setback proposed to be 8.96 metres also achieves the DPF requirements and is considered to provide adequate separation between neighbours to allow access to natural light and ventilation, reasonably achieving PO 9.1 of the Zone.

The dwellings adjoining the site are generally single storey in nature, with the exception of the property to the south being double storey. DO1 of the Zone accommodates low rise development that supports employment and community services that positively contribute to the local neighbourhood. It is considered that a single storey child care facility is consistent with the desired outcome of the Zone.

6.3 Traffic, Access and Car Parking

CIRQA have undertaken a traffic and parking assessment report for the proposal. The report concludes that there is sufficient onsite car parking to meet the anticipated demand, access points and movements through the site are considered to be safe and convenient. The proposed development satisfies the policies within the Transport, Access and Parking module, as follows:

- The access point and car parking area has been designed to facilitate safe and convenient vehicle movements in accordance with PO 3.3.
- The site has been designed such that all movements can enter and exit in a forward direction to ensure the safety of vehicle users and minimise interference with the traffic flow of the nearby State maintained road (Major Urban Transport Routes Overlay PO 1.1)
- The traffic generation associated with the overall development is considered to be within the capacity for the various access points and surrounding roads, in line with Traffic Generating Development PO 1.2
- Pedestrian linkages between parking areas, the property boundary and the development are provided and are safe and convenient as per PO 6.4
- The allocated loading area has been accommodated for the largest service vehicle within the boundary of the site, satisfying PO 1.3 and 6.6

The Commissioner of Highways was referred the application as the development includes alterations to an existing access onto a State Maintained Road within the Urban Transport Route Overlay. Council received the referral response with the relevant conditions relating to the new crossover location onto Main North Road and the relocation of the existing bus stop. Accordingly, these conditions have been included within the staff recommendation.

Table 1 - General Off-Street Car Parking Requirements sets out the parking rates for classes of development. Child Care Centre development is to provide 0.25 spaces per child. Based on the childcare centres' capacity of 84 children, the site has a theoretical demand of 21 spaces. A total of 21 spaces are provided and accordingly satisfies PO 5.1 of the Transport, Access and Parking module. If all land is required in accordance with the Future Road Widening, Council's Senior Traffic Engineer is comfortable with the removal of 2 car parking spaces as CIRQA have demonstrated a theoretical demand in the order of 11-20 spaces for the site, with more recent surveys indicating a lower demand than this.

Council's engineering staff have reviewed the traffic related matters of the proposed development and confirmed that they are satisfied that the proposal provides adequate onsite car parking when assessed against the Code.

The proposal will have an acceptable impact upon the adjoining road network and accordingly, it is considered that the proposal satisfies the relevant traffic, access and car parking provisions of the Planning and Design Code.

6.4 Interface between Land Uses

The subject land is located within the General Neighbourhood Zone and adjacent sensitive noise receivers. The proposed hours of operation for the child care facility will be from 6:30am to 6:30pm Monday to Friday only. This generally accords with the operating hours of other non-residential development as specified in the performance features of PO 2.1 (7am to 9pm Monday to Friday).

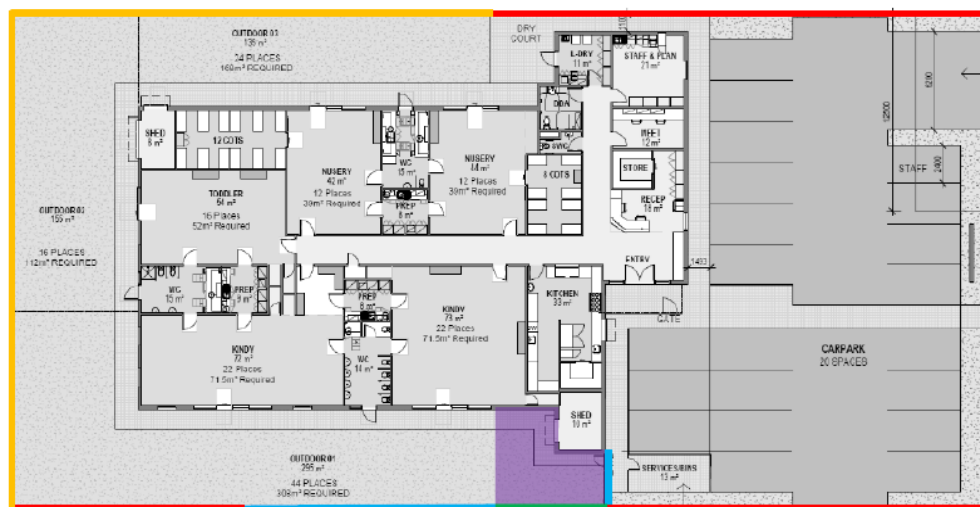
An environmental noise assessment prepared by Sonus has been undertaken of the proposed development considering noise generated from the outdoor play and car parking areas, the operation of the air conditioning units and the waste collection.

Sonus have recommended acoustic treatments to ensure the highest predicted level of noise generated by outdoor play activities does not exceed 50 dB(A) at any existing sensitive receiver, as are detailed below.

Sonus noise measurements confirmed that sensitive receivers in the locality are currently exposed to an 'Instantaneous Maximum Noise Level' of 67dB(A) due to vehicles travelling on Main North Road. Sonus notes that in this case, the Noise Policy enables the predicted noise levels to exceed the 'Instantaneous Maximum Noise Level' of 60 dB(A) and that the predicted maximum noise levels at sensitive receivers fronting Main North Road will be 64 dB(A) and 58dB(A) at all the other sensitive receivers in the locality.

The report concludes that the noise criteria can be achieved with the following measures implemented:

- Boundary fencing that are sealed airtight at all junctions including between panels where it adjoins the canopy and at the ground and be installed with material such as 0.35mm BMT sheet steel (such as Colourbond) for the extent shown in the figure below. The height of the fences includes the orange at 1.8 metres, red at 2.1 metres, blue 2.4 metres and a height of 2.7 metres for the extent shown in green (extracted from Planning Report contained within the Attachment).



- A canopy will be constructed within the shaded purple area in the figure above (with a wall height of 2.7 metres).
- Mechanical plant suitably located within the 'Dry Court'.
- Rubbish collection to occur between the hours of 9am to 7pm on a Sunday or public holiday or 7am to 7pm any other day.

Based on the above, it is considered that the proposal has been designed appropriately to minimise impacts of the adjacent dwellings in the locality, in accordance with PO 4.1 and a condition to ensure compliance with the Report is contained within the Recommendation.

In summary, it is considered that the proposed development has satisfactorily addressed the interface between land uses requirements in respect to noise and light spill subject to the inclusion of a number of conditions of consent.

6.5 Fences and Retaining Walls

Design in Urban Areas Module PO 9.1 requires fences, walls and retaining walls to be of sufficient height to maintain privacy and security without unreasonably impacting the visual amenity and adjoining land's access to sunlight or the amenity of public places. To ensure that the child care facility can maintain the required gradients to meet the relevant compliance, the land requires site works as shown on the CPR Stormwater Management Plan. These site works, in addition to the acoustic treatments for boundary fencing ranging in height from 1.8 to 2.7 metres as detailed above, will result in a combined fence and retaining wall height of:

- 2.7-2.9 metres on the western (rear) boundary
- 2.1-2.7 metres on the northern (side) boundary
- 2.1-3.0 metres on the southern (side) boundary

It is considered that the site works are reasonable and generally anticipated with development in the Zone, based on the existing ground levels shown on the Stormwater Management Plan and topography of the site. The visual impacts of the fencing and retaining walls are considered to be acceptable as the dwellings on adjacent land are well separated from the boundary, will not result in overshadowing of the primary area of private open space to the adjacent dwellings and will maintain privacy and security for the subject land in the outdoor play area in accordance with Interface Between Land Uses PO 3.2.

6.6 Advertising

The proposed non illuminated advertisements are considered to be of appropriate size and scale, will identify the business and do not detract from the residential character of the locality therefore satisfying PO 12.1.

6.7 Landscaping

The Applicant has provided a Landscaping Plan by Das Studio that proposes the planting of a number of native species in the form of tall trees, low level shrubs and groundcovers adjacent the building and car park area satisfying Design in Urban Areas PO 3.2. It is noted that tall screening shrubs are proposed on the southern site boundary which will assist in screening the boundary fencing and waste enclosure from the public realm, in accordance with PO 7.5.

The proposed landscaped areas soften the impact of the proposed built form and extensive hardstand areas that are accommodating car parking areas. It is considered that there is an appropriate level of landscaping to enhance the visual appearance of the development and overall amenity of the site and locality, in accordance with PO 7.2 and 7.6.

6.8 Stormwater

Council raised a number of queries in relation to stormwater issues resulting from the proposed development, which were clarified within the Applicant's Response to Further Information by CPR Engineers.

This detail has been reviewed by Council's Civil Engineer who has confirmed that the stormwater detail provided by CPR Engineers can be supported.

7. Conclusion

The proposed development is considered to be consistent with the desired outcomes of the General Neighbourhood Zone, relevant overlays and generally accords with the relevant general provisions.

The proposed land use is considered appropriate in the context of the subject site. The development will provide additional childcare services for the growing community of the Smithfield, Munno Para and Blakeview areas.

The built form and design contain merit given the existing mixed residential and commercial character in the locality. The location, size and scale of the proposed advertising in the context of the locality is considered appropriate. The proximity of the subject land to the commercial activity, being situated on an arterial road lends itself for signage to be included and the size and scale is considered appropriate in the context of the immediate residential character.

Traffic impacts and movements onsite are acceptable. The Commissioner of Highways has reviewed the application and provided relevant comments relating to the new crossover location onto Main North Road. The proposal has sufficient onsite car parking and has demonstrated that vehicle movements can be accommodated.

For the reasons outlined above, the proposed development is not considered to be seriously at variance with the relevant provisions of the Planning and Design Code and the proposal is considered to generally comply with the provisions that Planning Consent is warranted.

8. Staff Recommendation

That pursuant to the authority delegated to the Council Assessment Panel by the Council, it is recommended that the Council Assessment Panel:

- a) DETERMINES that the proposed development is not seriously at variance with the policies in the Planning and Design Code; and
- b) GRANTS Planning Consent to the application by Development Holdings Pty Ltd for Child care facility with ancillary acoustic boundary fencing, retaining wall and associated advertisement at 37 Main North Road, Smithfield as detailed in Development Application ID 23024476 subject to the conditions and notes:

Conditions

Council:

1. The development must be undertaken, completed and maintained in accordance with the plan(s) and information detailed in this Application except where varied by any condition(s) listed below.
2. All recommendations contained within the Environmental Noise Assessment Report by Sonus Document Reference S78196C2 dated July 2023, shall be implemented prior to occupation of the site, and complied with at all times.
3. The hours of operation herein approved are as follows:
 - Monday to Friday 6:30am to 6:30pm

Any variation to these hours of operation will require a further consent.

4. The child care facility herein approved with a maximum capacity of 84 children.
5. All loading and unloading of goods and merchandise shall be carried out upon the subject land and no loading of any goods or merchandise shall be permitted to be carried out in the street in conjunction with the consent herein granted.
6. All driveways, parking and manoeuvring areas must be formed, sealed with concrete, bitumen or paving, and be properly drained. They must be maintained in good condition thereafter.
7. The planting and landscaping identified on the Landscape Design prepared by Das Studio, submitted with the application must be completed in the first planting season concurrent with or following the commencement of the use. Such planting and landscaping must not be removed nor the branches of any tree lopped and any plants which become diseased or die must be replaced by suitable species.

Commissioner of Highways:

8. Any lights on the subject land must be directed and screened so that overspill of light into the nearby properties is avoided and motorists are not distracted.
9. All access to the development shall be gained in accordance with the Site Plan produced by Brown Falconer, drawing no. 3602 DA04, Revision 2, dated 03/08/2023; except where varied by conditions below.
10. Clear sightlines, as shown in Figure 3.3 'Minimum Sight Lines for Pedestrian Safety' in AS/NZS 2890.1:2004, shall be provided at the property line to ensure adequate visibility between vehicles leaving the site and pedestrians on the adjacent footpath. This will require the access to be located further to the south and all vegetation either side of the access being less than 1m in height.
11. All vehicles shall enter and exit the site in a forward direction. All on-site vehicle manoeuvring areas shall remain clear of any impediments.
12. The redundant crossover on Main North Road shall be closed and reinstated to Council's kerb and gutter standards at the applicant's expense prior to operation of the development.
13. The bus stop (including tactiles) adjacent the subject site shall be relocated to the satisfaction of DIT/SAPTA prior to the operation of the development with all costs borne by the applicant. The applicant shall contact Mr Wayne Stewart, SAPTA, Senior Project Officer on ph. 7133 2560, mobile 0401 713 311, or email. Wayne.stewart@sa.gov.au to progress this.
14. Any infrastructure within the road reserve that is demolished, altered, removed or damaged during the construction of the project shall be reinstated to the satisfaction of the relevant asset owner, with all costs being borne by the applicant.
15. Stormwater run-off shall be collected on-site and discharged without impacting the safety and integrity of the adjacent road network. Any alterations to the road drainage infrastructure required to facilitate this shall be at the applicant's cost.

Commissioner of Highways Notes:

1. The Metropolitan Adelaide Road Widening Plan shows a possible requirement for a strip of land up to 4.5 metres in width from the Main North Road frontage of this site for future upgrading of the Main North Road/Curtis Road/Craigmore Road intersection. The consent of the Commissioner of Highways under the Metropolitan

Adelaide Road Widening Plan Act is required to all building works on or within 6.0 metres of the possible requirement. As the built form does not encroach within the above areas, consent is not required in this instance.

2. This property abuts a section of Main North Road that was proclaimed as controlled access road on 12 December 1960 pursuant to Part 2A of the Highways Act 1926. Departmental records show that there is a permitted means of access by which persons and vehicles may directly enter or leave the controlled access road from/to this site. Upon construction of the new access, the applicant will need to contact the Department via dit.landusecoordination@sa.gov.au to arrange for the existing permit records to be updated.

ekistics



Smithfield Child Care Centre

Planning Statement

AUG 2023

REF#1475-001



ACKNOWLEDGEMENT TO COUNTRY

Ekistics respectfully acknowledges the traditional owners and custodians of the land on which we work and we pay our respects to Elders past and present.



PROPRIETARY INFORMATION STATEMENT

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Revision	Description	Author	Date
DRAFT	Planning Statement	JR	17 August 2023
Version 1	Update and issue to client	RM / JR	18 August 2023



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APPENDICES

1. CERTIFICATE OF TITLE

2. ARCHITECTURAL DRAWINGS BROWN FALCONER

3. LANDSCAPE PLAN DAS STUDIO

4. ENVIRONMENTAL NOISE ASSESSMENT SONUS

5. TRAFFIC AND PARKING REPORT CIRQA

6. STORMWATER MANAGEMENT PLAN CPR ENGINEERS



1. EXECUTIVE SUMMARY

Category	Details	
ADDRESS OF SITE	37 Main North Road, Smithfield	
FIRST NATIONS COUNTRY	Kurna	
CERTIFICATES OF TITLE	<ul style="list-style-type: none">• Certificate of Title Volume 5819 Folio 112 (Filed Plan 210849 Allotment 873)• Certificate of Title Volume 5819 Folio 113 (Filed Plan 210848 Allotment 872)	
ALLOTMENT AREA	Approximately 2,026m².	
ALLOTMENT FRONTAGE	Approximately 31.78m to Main North Road	
RELEVANT AUTHORITY	Assessment Panel or Assessment Manager at The Rural City of Murray Bridge	
PLANNING AND DESIGN CODE	Version 2023.12 (Published on 17 August 2023)	
ZONE	General Neighbourhood Zone	
OVERLAYS	<ul style="list-style-type: none">• Defence Aviation Area (All structures over 90 metres)• Future Road Widening• Major Urban Transport Routes• Prescribed Wells Area• Regulated and Significant Tree• Stormwater Management• Traffic Generating Development• Urban Tree Canopy	
TECHNICAL & NUMERIC VARIATIONS (TNVs)	Concept Plan (81 - Edinburgh Defence Airfield Lighting Constraints)	
EXISTING USE	Residential – single storey detached dwelling	
PROPOSAL DESCRIPTION	Child care centre with associated car parking, landscaping, signage, retaining walls and fencing	
CLASSIFICATION OF DEVELOPMENT	Child care centre (child care facility)	Code Assessed - Performance Assessed
	Advertisement	Code Assessed - Performance Assessed
	Retaining wall	Code Assessed - Performance Assessed
	Fence	Code Assessed - Performance Assessed
PUBLIC NOTIFICATION	Required	
REFERRALS	Commissioner of Highways	
APPLICANT	Development Holdings Pty Ltd	
CONTACT PERSON	James Rhodes – Ekistics Planning and Design – (08) 7231 0286	



2. INTRODUCTION/BACKGROUND

This planning statement has been prepared in support of a Development Application by Development Holdings Pty Ltd to establish a child care centre on land located at 37 Main North Road, Smithfield (the 'subject site').

This planning statement provides information about the subject site and proposed development. The planning statement will address the merits of the development application against the relevant provisions of the Planning and Design Code (Version 2023.12).

For the purposes of this statement, the Planning, Development and Infrastructure Act 2016 will be referred to as the 'PDI Act', the Planning, Development and Infrastructure (General) Regulations 2017 will be referred to as the 'PDI Regulations' and the Planning and Design Code will be referred to as the 'Code'.

This planning statement has been prepared on the basis of the plans, elevations and supporting documentation summarised below:

- **Appendix 1** – Certificate of Title
- **Appendix 2** – Architectural Drawings prepared by Brown Falconer
- **Appendix 3** – Landscape plan prepared by Das Studio
- **Appendix 4** – Environmental Noise Assessment prepared by Sonus
- **Appendix 5** – Traffic and Parking Report prepared by CIRQA
- **Appendix 6** – Stormwater Management Plan prepared by CPR Engineers



3. THE SITE AND LOCALITY

3.1. The Site

The subject site is located at 37 Main North Road, Smithfield and is formally recognised by the two following titles:

- Certificate of Title Volume 5819 Folio 112 (Allotment 873 Deposited Plan 210849); and
- Certificate of Title Volume 5819 Folio 113 (Allotment 872 Deposited Plan 210848).

The two Certificates of Title (refer to Appendix 1) are not affected by way of easements, rights of way or dealings which may affect the development potential of the site.

The site measures approximately 2,026m² in area with an approximate frontage of 31.78m to Main North Road and depth of 63.77m. Main North Road is a State Maintained Road ('Major Urban Transport Route') under the care and control of the Commissioner of Highways/Department for Infrastructure and Transport.

The subject site is depicted in Figure 3.1 below.



Figure 3-1 Satellite image of the subject site

The site is currently used for residential purposes, containing a single storey detached dwelling and outbuilding positioned to the southern end of the site. Landscaping is primarily limited to several olive trees located in the rear half of the site. A portion of the



front boundary includes iron fencing. The land does not contain any Regulated nor Significant trees. The site has a fall of approximately 1.4-1.8m from the front to the rear of the site.

Vehicle access to the site is currently provided via a single crossover to Main North Road, adjacent the southern site boundary.

Photographs of the subject site were taken on 19 July 2023 and are provided in Figure 3-2 to Figure 3-3 below.



Figure 3-2 View to the site facing north-west from the Main North Road median



Figure 3-3 View inside the site facing north-west to the shrubs and distant olive trees



3.2.The Locality

The immediate locality is residential in nature, comprising single storey detached dwellings with the exclusion of the allotment to the immediate south which contains a two storey detached dwelling. Notably, this two storey detached dwelling does not include upper level windows which face the Site. The front setbacks of the adjoining residential properties to the north and south are predominantly used for car parking (35 Main North Road entirely in concrete, and 41 Main North Road primarily being soft landscaping). Existing dwellings generally comprise pitched roofing and brick facades, whereas commercial developments (retail fuel outlets) comprise rendered solid facades, with discreet uses of glazing fronting Main North Road.

Allotments adjoining the site (excluding 41 Main North Road) contain dwellings featuring large rear boundary setbacks, with several including structures positioned on or close to the common boundary shared with the site.

The wider locality includes commercial uses fronting Main North Road. Approximately 60m north of the site is an 'OTR' retail fuel outlet featuring surrounded by residential development. Land approximately 60m to the south of the site includes a variety of commercial uses, including another 'OTR' retail fuel outlet (including a car wash) and other commercial uses further south. Both uses are open in nature with a large extent of hardstand area.

At the front of the site, along Main North Road, there is provision of on-street parking. Main North Road includes a slip lane and turn lane within the median permitting 'U turn' movements to the north of the site and to the south, approximately 250m away. Mature trees exist within the Main North Road median, with land to the east screened by a watercourse and mature trees.

Overhead powerlines and a bus stop exist within the Main North Road verge forward of the subject site. The bus stop features a pole and tactile paving.

Photographs of the locality were taken on 19 July 2023 and are provided in Figure 3-4 to Figure 3-7 below.



Figure 3-4 View to residential development and bus stop, facing south-east from Main North Road



Figure 3-5 View to residential development and distant retail fuel outlet, facing south from the Main North Road median



Figure 3-6 View to the 'U turn' slip lane, mature vegetation and bus shelter opposite the site, facing east



Figure 3-7 View to adjacent residential development, facing north from the Main North Road median



4. PROPOSED DEVELOPMENT

4.1. Land Use

The application proposes to develop a child care centre at 37 Main North Road, Smithfield, and includes a car park at the front of the site and 591m² of outdoor play areas for children at the rear and sides of the building envelope.

The child care centre will have a capacity to accommodate up to 84 children and will operate between the hours of 6.30am and 6.30pm on Monday to Friday.

4.2. Building Design

Architectural plans, including site plans, floor plans, elevations and perspectives have been prepared by Brown Falconer (refer to Appendix 2).

The proposed child care centre building is centrally located within the site, with the car park located between the building and Main North Road, with the remaining curtilage used as outdoor play area for the children. The building will be positioned approximately 19.1m from the street boundary and 9m from the rear boundary. The minimum side setbacks of the building are approximately 1.1m from the northern boundary and 6.3m from the southern boundary.

An outbuilding (shed) is located on the southern boundary for a length of 4.2m and has a width of 2.5m. It is noted that the proposed outbuilding does not constitute 'development' as per schedule 4, clause 4(1)(a) of the PDI Regulations.

The child care centre will have a total floor area of 690m². The building's main entrance is orientated to face southward behind the front facade, and immediately adjacent the pedestrian pathway and car park. The internal layout of the building includes a reception, staff rooms, kitchen, laundry and seven play rooms for children grouped by age, with associated amenities.

The single storey building primarily features a flat roof design presenting to the street, with a parapet reaching up to 4.0m above ground level. Behind the flat roof design, is a pitched roof with gable ends to the east and west elevations. The pitched roof will have a pitch of 15 degrees and reaching up to 5.83m above finished floor level at the roof peak. The west elevation also includes a 2.0m long verandah to provide shade and shelter.

The front façade features articulation and projecting canopies to create a degree of visual interest from the streetscape. These canopies will provide shelter over the footpath and a curved canopy will be set higher (3.2m) above ground level to define the building entrance.

Figure 4.1 over-page details and illustrates the material palette for the proposed development.

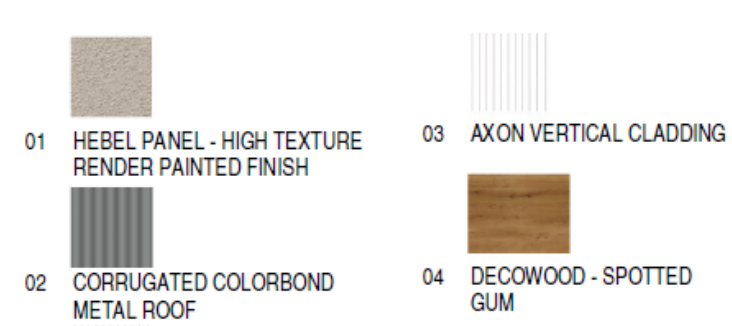


Figure 4-1 Schedule of materials and colours proposed

External finishes and colours have been selected in response to materiality within the surrounding residential area and the commercial elements along this portion of Main North Road. As illustrated in the above materials palette, street-facing wall cladding comprises white axon vertical cladding with feature timber cladding to articulate the façade and identify the building entrance. All other building elevations will be clad in Hebel panelling finished in Beige coloured render.

4.3. Advertisements

All proposed advertisements are depicted on the elevations (DA07) contained within Appendix 2.

The development will include two wall signs featuring the 'Imagine' branding (logo & text). One sign will be affixed to the primary façade and have dimensions of 0.81m in height and 1.76m in length. The other sign will be affixed to a small masonry wall used as a signage panel comprising dimensions of 1.35m in height (approx.) and 2.82m in width. Both signs will be non-illuminated.

4.4. Fencing and Retaining Walls

Due to the difference in site levels at the front and rear of the site, there is an extent of earthworks and associated retaining walls required, particularly in the context of child care centre use and the need to provide for an 'at-grade' level throughout and useable outdoor play spaces. The proposed outdoor play area at the rear of the site has a gradient of 1 in 50, the maximum grade permitted by the operator, to reduce the extent of earthworks required.

The civil plan prepared by CPR and contained within Appendix 7 illustrates that fill will be required at the rear of the site, with the retaining being the tallest at the north-western corner of the site; being at 1.1m in height.

Fencing on site boundaries (excl. the street boundary) will generally reach 1.8m or 2.1m in height. To manage acoustic impacts on the adjacent two storey dwelling to the south, a portion of the southern boundary will include 2.4m and 2.7m tall fencing, but will not be sited atop retaining walls. The 2.7m tall section will be limited to a length of approx. 7.3m and includes an acoustic canopy connecting to the proposed building. This results in a combined fence & retaining wall height of 2.7-2.9m on the rear boundary, 2.1-2.7m on the northern boundary, and 2.1-3.0m on the southern boundary.

The finished floor level of the building will be set at 52.15m AHD.



4.5.Landscaping

A landscape plan has been prepared by Das Studio for the development and is provided within Appendix 4.

The landscape plan includes the planting of a number of native species in the form of tall trees, low-level shrubs, and groundcovers adjacent the building and car park to soften their appearance and provide shade. The planting palette includes species endorsed by the City of Playford and those which are suitable underneath power lines. Fencing at the northern and southern ends of the car park will be screened by tall shrubs.

The outdoor play spaces (comprising 591m² in area) will be landscaped to the operator's requirements (to be determined at a later date).

Further, no Regulated or Significant Trees exist on-site.

4.6.Transport, Parking and Access

CIRQA have undertaken a traffic and parking review of the proposed development (refer to Appendix 5).

A pedestrian path will provide direct access to the site and building entrance from the Main North Road footpath.

A car park to service the proposed child care centre will be positioned forward of the building, fronting Main North Road. The car park will be accessed via one, two-way crossover to Main North Road. The 6.2m wide crossover will be located at the northern end of the site with the existing crossover reinstated as upright kerbing to the satisfaction of Council. The access point requires the minor relocation of the existing Bus Stop 81c (West Side) likely further south.

The proposed car park will accommodate 21 vehicle spaces, including one (1) disabled parking space (with associated shared space) positioned nearby the building entrance. Five (5) car parking spaces will be designated for staff in a double stacked/tandem arrangement.

In the event that the Commissioner of Highways (via the Department for Infrastructure and Transport ('DIT')) acquire all land designated for future road widening, CIRQA have advised that two car parking spaces would be removed, and site can still be accessed in a safe and convenient manner.

Waste will be collected by a private contractor using a Medium Rigid Vehicle (up to 8.8m in length) which will enter the site outside of peak operating hours, collect waste from the designated bin store and exit the site (in a forward direction) onto Main North Road.

4.7.Stormwater Management

CPR Engineers have prepared a stormwater management plan and civil works plan (Appendix 7).

Stormwater from the car park will be collected via grated inlet pits then treated via a gross pollutant trap in accordance with Council's Water Quality requirement and EPA guidelines.



Stormwater run-off from the car park and building roof will be detained on-site within two underground tanks with a capacity of 46,000L. Stormwater will then be discharged to the existing stormwater infrastructure on Main North Road via three pipes at restricted rates of 17.9L/s. This demonstrates that post-development discharge rates will not exceed pre-development discharge rates.



5. PROCEDURAL REQUIREMENTS

5.1. Applicable Policies

The Planning and Design Code (Version 2023.12), in conjunction with the SA Property and Planning Atlas (SAPPA), identifies that the General Neighbourhood Zone applies to the subject site, with no applicable Subzones. Zoning for the site and immediate locality is illustrated in Figure 5-1 below.

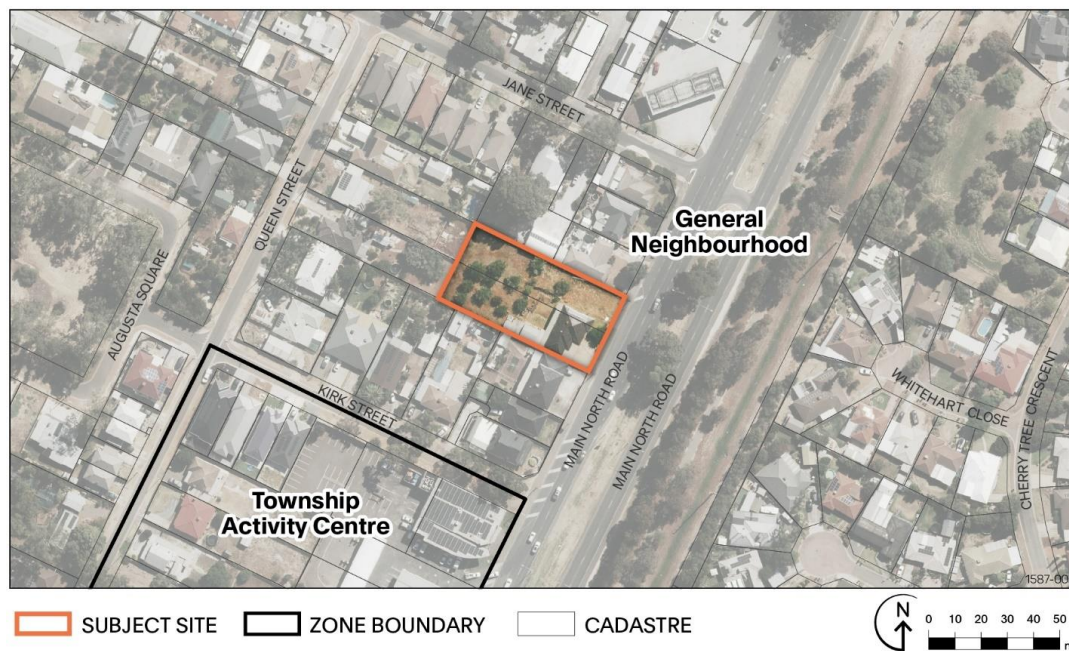


Figure 5-1 Zoning framework applicable to the site and locality

The Technical and Numeric Variation of Concept Plan (81 - Edinburgh Defence Airfield Lighting Constraints) applies to the site as well as the following Overlays:

Overlays

- Defence Aviation Area (All structures over 90 metres)
- Future Road Widening
- Major Urban Transport Routes
- Prescribed Wells Area
- Regulated and Significant Tree
- Stormwater Management
- Traffic Generating Development
- Urban Tree Canopy



5.2.Relevant Authority

The relevant authority to determine the development application will be the Council Assessment Panel or the Assessment Manager at the City of Playford as per Section 93(1)(a) and section 96 of the PDI Act.

5.3.Nature of Development

The proposed development is best described as:

Child care centre with associated car parking, landscaping, signage, retaining walls and fencing

Accordingly, the proposed development comprises the following elements:

- Child care facility (child care centre)
- Retaining wall
- Advertisement
- Fence

Part 7 of the Code defines a 'child care facility' as:

Child care facility Means a place primarily for the care or instruction of children of less than primary school age, children with special needs or out-of-school-hours care (including vacation care) and not resident on the site.

Includes: Pre-school; Child care centre; Early learning centre; Kindergarten; Nursery.

5.4.Classification of Development

A child care centre facility, advertisements, retaining walls and fencing will be Code Assessed, 'Performance Assessed' classes of development within the General Neighbourhood Zone.

5.5.Public Notification

A 'child care facility' and 'advertisement' are not listed within Neighbourhood Zone Table 5 as excluded from notification and accordingly, the development application will be subject to public notification.

5.6.Agency Referrals

We expect a referral to the Commissioner of Highways (via the Department for Infrastructure and Transport) will be required as the proposal includes the establishment of a new access point to a state-maintained road (Main North Road).



6. CODE ASSESSED DEVELOPMENT

The following section provides an assessment of the proposal against the relevant Planning and Design Code Desired Outcomes (DOs) and Performance Outcomes (POs). This assessment is grouped under a series of headings which address specific aspects of the proposed development.

It is noted that Concept Plan (81 - Edinburgh Defence Airfield Lighting Constraints) has not been considered in the following assessment given no upward lighting is proposed and the site appears to be located outside the Controlled Light Installation Area.

6.1.Land Use

General Neighbourhood Zone PO 1.1 seeks to accommodate “Predominantly residential development with complementary non-residential uses that support an active, convenient, and walkable neighbourhood.” The corresponding DPF explicitly lists the development of a ‘pre-school’ as a contemplated land use.

Further to the above discussion, we are of the opinion that the proposed use, scale and intensity is aligned with the provisions of the Code.

6.2.Building Design

6.2.1. BUILDING SITING AND SCALE

As discussed above, PO 1.3 seeks to ensure non-residential development is ‘sited and designed’ to complement the residential character and amenity of the neighbourhood. With this provision in mind, the design approach has been informed by the relevant Zone POs and DPFs pertaining to site coverage, building height, and building setbacks, as detailed in the analysis provided in Table 6-1 below.

Table 6-1 Site Coverage, Building Height and Building Setback Analysis

Performance Outcome and Designated Performance Feature	Assessment
PO 3.1 Building footprints allow sufficient space around buildings to limit visual impact, provide an attractive outlook and access to light and ventilation	The site coverage of the proposed development will be 43%, well below the recommended maximum of 60%, thereby satisfying PO 3.1 and providing sufficient space around buildings.
DPF 3.1 The development does not result in site coverage exceeding 60%.	
PO 4.1 Buildings contribute to a low-rise suburban character	The development comprises a single building level with the roof peak reaching 5.826m above



<p>DPF 4.1</p> <p>Building height (excluding garages, carports and outbuildings) no greater than 2 building levels and 9m and wall height that is no greater than 7m (not including a gable end).</p>	<p>finished floor level. Accordingly, the proposed building height and scale satisfies DPF 2.1.</p>						
<p>PO 5.1</p> <p>Buildings are setback from primary street boundaries to contribute to the existing/emerging pattern of street setbacks in the streetscape.</p> <p>DPF 5.1</p> <p>Buildings setback from the primary street boundary in accordance with the following table:</p>	<p>The dwelling on adjoining land to the north at 41 Main North Road, has an approximate setback of 9m from the primary frontage, while the dwelling on adjoining land to the south at 35 Main North Road, has an approximate setback of 7m from the primary frontage.</p>						
<table> <tr> <th data-bbox="229 772 518 817">Development Context</th><th data-bbox="518 772 821 817">Minimum setback</th></tr> <tr> <td data-bbox="229 817 518 974">There is an existing building on both abutting sites sharing the same street frontage as the site of the proposed building.</td><td data-bbox="518 817 821 974">The average setback of the existing buildings on the abutting sites minus 1m.</td></tr> <tr> <td data-bbox="229 974 518 1019">...</td><td data-bbox="518 974 821 1019">...</td></tr> </table>	Development Context	Minimum setback	There is an existing building on both abutting sites sharing the same street frontage as the site of the proposed building.	The average setback of the existing buildings on the abutting sites minus 1m.	<p>The proposed street boundary setback of 19.06m satisfies and exceeds DPF 5.1.</p>
Development Context	Minimum setback						
There is an existing building on both abutting sites sharing the same street frontage as the site of the proposed building.	The average setback of the existing buildings on the abutting sites minus 1m.						
...	...						
<p>PO 8.1</p> <p>Building walls are set back from side boundaries to provide</p> <ul style="list-style-type: none"> (a) separation between buildings in a way that contributes to a suburban character and (b) access to natural light and ventilation for neighbours <p>DPF 8.1</p> <p>Other than walls located on a side boundary, building walls are set back from side boundaries in accordance with the following:</p> <ul style="list-style-type: none"> (a) where the wall height does not exceed 3m - at least 900mm (b) for a wall that is not south facing and the wall height exceeds 3m – at least 900mm from the boundary of the site plus a distance of 1/3 of the extent to which the height of the wall exceeds 3m from the top of the footings (c) for a wall that is south facing and the wall height exceeds 3m - at least 1.9m from the boundary of the site plus a distance of 1/3 of the extent to which the height of the wall exceeds 3m from the top of the footings. 	<p>The proposed child care centre will be set back at least 1.1m and 6.3m from the side boundaries of the site. The proposal therefore satisfies DPF 8.1 and provides adequate side setbacks which are aligned with suburban character and enables access to light and ventilation for neighbours.</p>						

**PO 9.1**

Building walls (excluding ancillary buildings and structures) are set back from rear boundaries to provide:

- (a) separation between buildings in a way that contributes to a suburban character
- (b) access to natural light and ventilation for neighbours
- (c) private open space
- (d) space for landscaping and vegetation

The building's rear setback (8.96m) comfortably exceeds the minimum setback of 4m referenced within DPF 9.1. The building will be generously separated from adjoining residences to the rear, some 45m and 52m away.

DPF 9.1

Building walls (excluding ancillary buildings and structures) are set back from the rear boundary at least

...

- (b) if the size of the site is 301m² or more —
 - (i) 4m in relation to the ground floor of the building
 - (ii) 6m in relation to any other building level of the building.

Further to the above discussion, the proposed development has been designed to satisfy all DPF provisions pertaining to site coverage, building height and building setbacks.

6.2.2. ARCHITECTURAL EXPRESSION

The following Design in Urban Areas General Development Policies of the Code are most relevant to an assessment of the external appearance of the building.

Design in Urban Areas

- PO 1.3** Building elevations facing the primary street (other than ancillary buildings) are designed and detailed to convey purpose, identify main access points and complement the streetscape.
- PO 1.4** Plant, exhaust and intake vents and other technical equipment are integrated into the building design to minimise visibility from the public realm and negative impacts on residential amenity by:
- (a) positioning plant and equipment discretely, in unobtrusive locations as viewed from public roads and spaces
 - (b) screening rooftop plant and equipment from view
 - (c) when located on the roof of non-residential development, locating the plant and equipment as far as practicable from adjacent sensitive land uses.
- PO 1.5** The negative visual impact of outdoor storage, waste management, loading and service areas is minimised by integrating them into the building design and screening them from public view (such as fencing, landscaping and built form), taking into account the form of development contemplated in the relevant zone.



The external material palette (including Hebel panelling, axon vertical cladding, and timber Decowood cladding and Colorbond® roofing) and neutral colour tones comprising whites, beige, grey and timber are highly compatible with both the commercial and residential nature of the locality.

An articulated front façade and variation in roof forms with projecting eaves and canopies will provide visual interest, while fenestration through several glazed windows, will enable passive views into and out of the building.

The application of the timber Decowood cladding defines the building entrance, in conjunction with the raised canopy, fencing, and paved footpath across the car park.

The outdoor enclosed bin store is screened and appropriately sited adjacent landscaping at the southern end of the car park, thereby satisfying Design in Urban Areas PO 1.5. Further, the mechanical plant will not be visible from the public realm; located between the dry court and building wall.

As discussed above, in our opinion, the proposed building articulation and materiality defines the building entrance, provides visual interest and will complement the streetscape in accordance with the Design in Urban Areas provisions of the Code.

6.2.3. ENVIRONMENTAL PERFORMANCE

The environmental performance of the building is raised by the following Design in Urban Areas provisions:

- PO 4.1** Buildings are sited, oriented and designed to maximise natural sunlight access and ventilation to main activity areas, habitable rooms, common areas and open spaces.
- PO 4.2** Buildings are sited and designed to maximise passive environmental performance and minimise energy consumption and reliance on mechanical systems, such as heating and cooling.
- PO 4.3** Buildings incorporate climate responsive techniques and features such as building and window orientation, use of eaves, verandahs and shading structures, water harvesting, at ground landscaping, green walls, green roofs and photovoltaic cells.

Due to the relatively low site coverage and extensive landscaping proposed on-site, the urban heat island effect will be reduced. The building's central location and proposed fenestration enables sunlight access and ventilation to all elevations. The building features projecting eaves or canopies to all elevations. The 2m deep verandah at the west elevation will reduce the impact of the hot afternoon sun through the windows, while also shading the footpath below. Accordingly, the proposed building has been designed to minimise the reliance on mechanic heating and cooling.

6.2.4. SAFETY

The Design in Urban Areas General Development Policies seek to ensure development incorporates design techniques to discourage crime by maintaining and maximising opportunities for passive surveillance, the differentiation of public and private spaces and accommodating safe and perceptible paths of travel to a clearly defined building entrance.

The proposed development incorporates an open carpark positioned between the childcare centre and Main North Road. The landscape design includes plantings selected to soften the visual impact of the carpark whilst still maintaining lines-of-sight for



passive surveillance. This landscaping, trees and wall sign along the Main North Road frontage will create a clear demarcation between public and private spaces.

Building windows overlooking the carpark will provide opportunities for passive surveillance of the carpark and public realm.

Further to the above discussion, the proposed development is aligned with the relevant 'safety' provisions of the Code.

6.3.Fencing and Retaining Walls

CPR Engineers were engaged from the outset of the project to inform the initial conceptual design phase, and this engagement has continued in an iterative manner through to the current proposal. Multiple design options were explored for the site, with the current proposal being the preferred approach in order to reduce the extent of earthworks required on-site, particularly at the rear of the site.

The relevant Code provisions relating to fencing and retaining walls are listed below

Design in Urban Areas

PO 9.1 Fences, walls and retaining walls are of sufficient height to maintain privacy and security without unreasonably impacting the *visual amenity and adjoining land's access to sunlight or the amenity of public places*.

Interface Between Land Uses - General Development Policies

PO 3.2 Overshadowing of the primary area of private open space or communal open space of adjacent residential land uses in:

- (a) a neighbourhood type zone is minimised to maintain access to direct winter sunlight
- (b) other zones is managed to enable access to direct winter sunlight

The proposed fencing scheme has been designed in accordance with Sonus' recommendations to manage acoustic impacts.

This fence height of 1.8m on the rear boundary and northern (side) boundary atop retaining walls is of sufficient height to maintain privacy and security for the children using the outdoor play areas, as per Design in Urban Areas PO 9.1. All other fencing is typical in height for a residential locality, with the exclusion of a limited section reaching 2.7m in height. This fence has no retaining wall underneath and will be limited in length (7.29m). Subsequently, the fence has dimensions, a form and appearance alike an outbuilding positioned up to the side boundary, and satisfies the building wall provision of Zone PO 7.1 (RE maximum length and height) and is therefore not unreasonable.

PO 9.1 also seeks to ensure that boundary fencing and retaining walls do not unreasonably impact on visual amenity. The combined height of retaining and fencing is up to 2.9m at the rear of the site and it is noted the extent of earthworks required has been minimised in a manner where the maximum possible gradient for outdoor play area is utilised. The visual impact of combined fencing and retaining walls at the rear of the site is suitability mitigated, noting:

- Such fencing and retaining walls will not overshadow any adjoining residential property to the north (as per PO 3.2);
- A dwelling and mature tree on adjoining land are well separated (approx. 25m & 15m) from the northern site boundary;
- Outbuildings are positioned in close proximity (approx. 1m) to the northern and southern site boundaries;



- Dwellings on adjoining land are well separated from the western site boundary, some 35m & 43m away;
- Such fencing and retaining walls will only overshadow the private open space of these residences in the morning of the winter solstice, thereby ensuring sufficient sunlight access to private open space from midday onwards (as per PO 3.2); and
- The greatest extent of retaining and fencing is limited to the western end of the southern site boundary, with adjoining properties containing dwellings positioned some 20m from the site boundary.

For the reasons outlined above, we are of the opinion that the proposed fencing and retaining scheme will not unreasonably impact visual amenity nor sunlight access for adjoining residences.

6.4.Landscaping

The following Design in Urban Areas General Development Policies are relevant to the landscape design prepared by Das Studio (Appendix 4):

- PO 3.1** Soft landscaping and tree planting are incorporated to:
- (a) minimise heat absorption and reflection
 - (b) maximise shade and shelter
 - (c) maximise stormwater infiltration
 - (d) enhance the appearance of land and streetscapes.
- PO 3.2** Soft landscaping and tree planting maximises the use of locally indigenous plant species, incorporates plant species best suited to current and future climate conditions and avoids pest plant and weed species.
- PO 7.2** Vehicle parking areas appropriately located, designed and constructed to minimise impacts on adjacent sensitive receivers through measures such as ensuring they are attractively developed and landscaped, screen fenced and the like.
- PO 7.4** Street-level vehicle parking areas incorporate tree planting to provide shade, reduce solar heat absorption and reflection.
- PO 7.5** Street level parking areas incorporate soft landscaping to improve visual appearance when viewed from within the site and from public places.
- PO 7.6** Vehicle parking areas and associated driveways are landscaped to provide shade and positively contribute to amenity.

The proposed landscaping scheme, and in particular, the planting of five trees surrounding the carpark will enhance the appearance of the streetscape in the context of the site's location on Main North Road and the existing condition within the locality.

The planting surrounding the carpark incorporates low level shrubs and groundcovers interspersed between small and medium sized trees selected to soften the visual impact of hard-stand areas, provide shade to parked vehicles and soften the visual impact of the childcare centre whilst still maintaining sightlines for passive surveillance (PO 3.1, 7.2, 7.4, 7.5, 7.6, 7.7). In addition,



tall screening shrubs along the southern site boundary, will assist in screening the boundary fencing and waste enclosure from external view.

The planting palette (refer Appendix 2) proposes a range of native species and species endorsed by the City of Playford, and thereby satisfies PO 3.2.

Importantly, the landscaping will be supplemented by generously sized Outdoor Play Areas comprising 591m² which will be landscaped in accordance with operator requirements. Indicative boundary plantings including medium and large trees are depicted in the landscape plan (refer to Appendix 3).

Further the above discussion, we are of the opinion that the proposed landscape design is well considered and is aligned with the relevant landscape provisions of the Code.

6.5. Advertisements

Zone PO 12.1 desires advertisements which are used for business identification purposes where they do not detract from the residential character of the locality. In this case, only two (2) signs are proposed with both limited in scale. The wall sign will be integrated with the design of the building, thereby avoiding visual clutter (Advertisements PO 1.5, PO 1.2, PO 2.1 & PO 2.3) and maintaining residential amenity (Zone PO 1.3).

The 'Imagine' logo and business name sign adjacent the Main North Road verge will assist with wayfinding for those travelling along Main North Road, with the other wall sign identifying the building entrance within the car park, thereby satisfying Advertisements PO 3.1. The two proposed signs will be non-illuminated and will be flush with the wall (PO 1.1 & PO 5.2).

6.6. Acoustic Impacts

An Environmental Noise Assessment has been prepared by Sonus and is contained within Appendix 4. Noise source features/activities generated by the operation of the childcare centre includes:

- Children playing in the designated outdoor play spaces;
- Carpark activity including:
 - People talking as they vacate or approach vehicles, the opening and closing of vehicle doors, vehicles idling and vehicles moving into and accelerating from parking spaces; and
 - Vehicle movements onto the site;
- The operation of air conditioning units; and
- Refuse collection.



6.6.1. OUTDOOR PLAY AREA

The Interface between Land Uses General Development Policies refer to the relevant Environment Protection (Noise) Policy (the 'Noise Policy') as a guide for the management of noise related impacts. However, as the Policy does not address noise generated by the use outdoor play spaces within child care centres, Sonus has assessed such impacts against the provisions of the Guidelines for Community Noise (the 'Guidelines') published by the World Health Organisation (WHO) which prescribe a maximum 'sound pressure level' of 50 dB L_{aeq}.

Sonus have recommended the following acoustic treatments to ensure the highest predicted level of noise generated by outdoor play activities does not exceed 50 dB(A) at any existing sensitive receiver:

- Ensure the fences marked-up in Figure [6-1] are constructed as follows:
 - A minimum of 1.8m high above the retaining wall and from a material such as 0.35mm BMT sheet steel (such as Colorbond) for the extent shown as **ORANGE**;
 - A minimum of 2.1m high above the retaining wall and from a material such as 0.35mm BMT sheet steel (such as Colorbond) for the extent shown as **RED**;
 - A minimum of 2.4m high and from a material such as 0.35mm BMT sheet steel (such as Colorbond) for the extent shown as **BLUE**;
 - A minimum of 2.7m high and from a material such as 0.35mm BMT sheet steel (such as Colorbond) or Perspex or glass for the extent shown as **GREEN**;
- Construct a canopy over the outdoor play area for the extent shaded as **PURPLE** in Figure 2 to a minimum height of 2.7m; and,
- Ensure fences are sealed airtight at all junctions, including between panels, where it joins the canopy and at the ground.

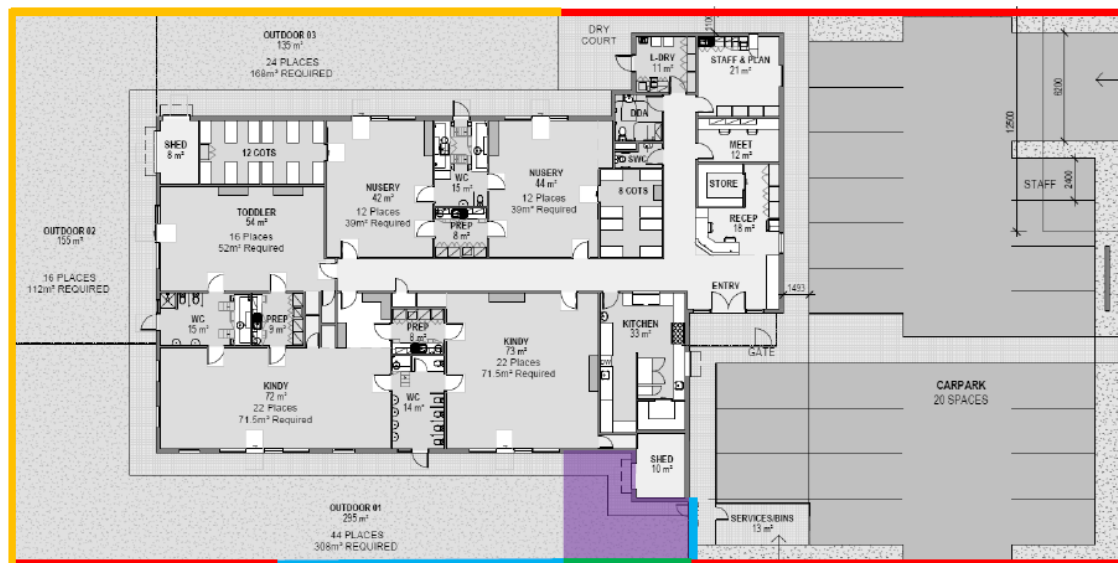


Figure 6-1 Recommended acoustic treatment (c/- Sonus)

All of Sonus' recommended acoustic treatments have been adopted, as depicted within Appendix 2 (refer to Plan 'DA04'), and on this basis, the development will achieve the noise criteria outlined within the WHO Guidelines.

6.6.2. CARPARK ACTIVITY & MECHANICAL PLANT

Interface between Land Uses PO 4.1 of the applies to the assessment of noise generated by carpark and mechanical plant:

PO 4.1 Development that emits noise (other than music) does not unreasonably impact the amenity of sensitive receivers (or lawfully approved sensitive receivers).

The corresponding DPF suggests that PO 4.1 will be satisfied where noise levels are managed to meet the criteria set out within the Noise Policy. The noise policy specifies 'Goal Noise Levels' to the closest noise sensitive receivers, as listed below:

- An equivalent noise level (LAeq) of 47 dB(A) during the day (7:00am to 10:00pm);
- An equivalent noise level (LAeq) of 40 dB(A) during the night (10:00pm to 7:00am); and,
- An instantaneous maximum noise level (Lmax) of 60 dB(A) during the night (10:00pm to 7:00am).

Noise level predictions for the carpark assumes a particular level of activity and a particular number of vehicle movements over a 15-minute period (as set out in page 11 of the Sonus report).

The selection of mechanical plant generally occurs during the detailed design phase of commercial projects. Accordingly, noise levels associated with mechanical plant operation is based on 'typical' plant selected for a land use of this nature. Sonus have



assumed two air conditioning condensing units located at ground level between the Dry Court fence and building wall. It is acknowledged that any mechanical plant generating noise levels exceeding those levels referenced within the Sonus report would be subject to a separate assessment as part of a variation application pursuant to section 128 of the PDI Act.

Applying a 5dB(A) penalty to all residences exposed to the character of noise from the on-site vehicles, the equivalent noise levels predicted will be up to 44dB(A) during the day (7:00am to 10:00pm) and up to 39dB(A) during the night (10:00pm to 7:00am).

Sonus noise measurements confirmed that sensitive receivers in the locality are currently exposed to an 'Instantaneous Maximum Noise Level' of 67dB(A) due to vehicles travelling on Main North Road. Sonus notes that in this case, the Noise Policy enables the predicted noise levels to exceed the 'Instantaneous Maximum Noise Level' of 60 dB(A) and that the predicted maximum noise levels at sensitive receivers fronting Main North Road will be 64 dB(A) and 58dB(A) at all the other sensitive receivers in the locality.

As per the fencing indicated within **Appendix 2** (refer 'Plan DA04'), the development has been designed in accordance with the acoustic treatments recommended by Sonus (refer to Figure 6-1) to manage noise related impacts on sensitive receivers. Accordingly, the development is aligned with the applicable 'Goal Noise Levels' specified within the Noise Policy, and satisfies Interface Between Uses PO 4.1.

6.6.3. REFUSE COLLECTION

To manage noise generated by waste collection activities, the Noise Policy prescribes waste collection hours of between 9:00am and 7:00pm on a Sunday or public holiday, and 7:00am and 7:00pm on any other day. As waste is to be collected on-site by a private waste contractor, collection hours can be managed to ensure compliance with the Noise Policy.

6.7. Traffic and Parking Impacts

CIRQA have undertaken a detailed traffic and parking assessment (refer to Appendix 5). The findings of the CIRQA assessment have been considered with reference to the relevant Transport, Access and Parking General Development Policies.

6.7.1. ACCESS AND MANOEUVRING

The following provisions are relevant to an assessment of the proposed access and manoeuvring arrangements.

Major Urban Transport Routes Overlay

- PO 2.1** Sufficient accessible on-site queuing adjacent to access points is provided to meet the needs of development so that all vehicle queues can be contained fully within the boundaries of the development site, to minimise interruption of the functional performance of the road and maintain safe vehicle movements.
- PO 5.1** Access points are located and designed to accommodate sight lines that enable drivers and pedestrians to navigate potential conflict points with roads in a controlled and safe manner.



Transport, Access and Parking

- PO 1.4** Development is sited and designed so that loading, unloading and turning of all traffic avoids interrupting the operation of and queuing on public roads and pedestrian paths.
- PO 3.8** Driveways, access points, access tracks and parking areas are designed and constructed to allow adequate movement and manoeuvrability having regard to the types of vehicles that are reasonably anticipated.
- PO 6.4** Pedestrian linkages between parking areas and the development are provided and are safe and convenient.
- PO 6.6** Loading areas and designated parking spaces for service vehicles are provided within the boundary of the site.

In accordance with the above provisions, the access point and car park have been designed facilitate safe and convenient vehicle movements as per the assessment (incl. turn paths) prepared by CIRQA. In particular:

- The parking area and spaces have been designed in accordance with the relevant Australian Standards;
- The parking aisle will be at least 6.2 metres in width to facilitate two-way vehicle movements, including at the site entrance to avoid queuing along Main North Road;
- A 1.0 m end-of-aisle extension will be provided beyond the last parking space in the aisle;
- A turn-around bay will be provided at the end of the parking aisle; and
- The driveway access point incorporates compliant pedestrian sightlines.

CIRQA also confirms that vehicles will be capable of entering and exiting the subject site in a forward direction, therefore satisfying Transport, Access and Parking PO 1.4 and PO 3.1. The access will facilitate left-in, left out movements for northbound traffic, with the U turn bays within the Main North Road median (relatively close to the site) facilitating access for southbound traffic.

The proposed access point is located 0.5m from an existing stobie pole, thus satisfying does not satisfy Transport, Access and Parking PO 3.5. While the proposal requires as the relocation of the existing bus stop and associated tactile paving, this access point location adequately balances the need to provide sufficient car parking on-site in the event of road widening. The alternative of widening the existing access point would involve the removal of four to five vehicle parking spaces. The intent of PO 3.5 is satisfied in that the development enhances the streetscape appearance and amenity due to the planting of four trees immediately adjacent the front boundary, and there will be minimal disruption for the general public with adequate space to accommodate the standard bus pad south of the access point (which we expect will be erected prior to the removal of the existing bus stop).

Transport Access and Parking PO 6.6 seeks to ensure carparks are designed to accommodate the onsite loading and unloading of service vehicles. Swept turning paths for a medium rigid vehicle (of up to 8.8m in length) provided within the CIRQA report demonstrates that waste vehicles are capable of entering and exiting the subject site in a forward direction (following collection of waste from the dedicated and screened waste storage area). As all waste will be collected by a private waste contractor, timing for collection is capable of being controlled by the operator and will occur whilst the centre is not in operation.



Consistent with Transport, Access and Parking PO 6.4, the development provides pedestrian linkages into the site from Main North Road, with footpaths providing safe and convenient access from the nearby pedestrian crossing directly to the building entrance.

As outlined above, the development has been designed to accommodate safe and convenient vehicle and pedestrian movements in accordance with the relevant provisions of the Code.

6.7.2. PARKING

Transport, Access and Parking PO 5.1 seeks to ensure development is provided with sufficient on-site parking to meet anticipated demands. The corresponding DPF outlines one way to achieve the Performance Outcome; suggesting that parking should be provided in accordance with the rates expressed in Transport, Access and Parking Table 1 – General Off-Street Car Parking Requirements. The proposed on-site car parking provision of 21 spaces for an 84 place child care centre meets the Table 1 parking rate of 0.25 spaces per child.

In accordance with PO 4.1, the carpark also incorporates a designated and conveniently positioned disabled parking space, located directly adjacent the main entrance to the building.

6.7.3. FUTURE ROAD WIDENING

Future Road Widening Overlay PO 1.1 seeks that “Development does not compromise or is located and designed to minimise its impact on future road widening requirements.” To this end, it is noted that the car park, access point and landscaping encroach within the designated road widening area (as shown on Dwg. No. 3602 DA 04 in Appendix 2).

Should all land designated for future road widening strip be acquired by DIT Further, CIRQA have advised the site can still be accessed in a safe and convenient manner, and that while not having a minor shortfall of two spaces from the suggested parking rate within the Code, the provision 19 spaces on-site will still meet car parking demands (satisfying Transport, Access and Parking PO 5.1).

Subsequently, the impact of future road widening on the development will be acceptable, with sufficient access arrangements and sufficient parking provision. It is noted a worst case scenario has been assumed for purposes of this proposal, whereby DIT acquire the full road widening area, rather than a portion of the designated strip.

6.7.4. TRAFFIC GENERATION

The following provisions of the Code are relevant to this aspect of the traffic assessment:

Major Urban Transport Routes Overlay

- PO 1.1** Access is designed to allow safe entry and exit to and from a site to meet the needs of development and minimise traffic flow interference associated with access movements along adjacent State Maintained Roads.



Traffic Generating Development Overlay

- PO 1.1** Development designed to minimise its potential impact on the safety, efficiency and functional performance of the State Maintained Road network.

Transport, Access and Parking

- PO 1.1** Development is integrated with the existing transport system and designed to minimise its potential impact on the functional performance of the transport system.

CIRQA's conservative estimate is that the development will generate an additional 77 am and 69 pm trips at peak hour on the surrounding road network. CIRQA have forecasted the distribution of generated vehicle movements within the surrounding road network during peak period and conclude that "Such movements will be readily accommodated at the proposed site access and on the adjacent road network". Further, these estimates assume that all movements associated with the child care centre are new trips on the network when, in reality, a portion of traffic generated by the child care may be existing with parents/caregivers potentially dropping-off/picking-up their children as part of their commute/school run.

6.8.Stormwater Management

The stormwater management plan and associated stormwater methodology prepared by CPR is provided in Appendix 7. The below Design in Urban Areas provisions are relevant to the assessment of the proposed stormwater management system:

- PO 42.1** Development likely to result in risk of export of sediment, suspended solids, organic matter, nutrients, oil and grease include stormwater management systems designed to minimise pollutants entering stormwater.
- PO 42.2** Water discharged from a development site is of a physical, chemical and biological condition equivalent to or better than its pre-developed state.
- PO 42.3** Development includes stormwater management systems to mitigate peak flows and manage the rate and duration of stormwater discharges from the site to ensure that development does not increase peak flows in downstream systems.

In accordance with Design in Urban Areas PO 42.1 and 42.2, stormwater collected from the car park will be diverted to a gross pollutant trap underneath the car park to minimise pollutants and preserve water quality prior to stormwater detention.

In accordance with PO 42.3, the development incorporates a minimum of 42kL of detention (underground) with discharge rates from these tanks restricted to ensure post-development stormwater flows do not exceed pre-development rates.

Further to the above discussion, it is our view the stormwater system has been designed to appropriately manage the collection and disposal of stormwater in accordance with the relevant Code provisions.



7. CONCLUSION

This development application seeks Planning Consent to establish an 84-place childcare centre with associated car parking, landscaping, signage, retaining walls and fencing at 37 Main North Road, Smithfield.

Following an inspection of the subject site and locality, a review of the proposed plans and associated specialist reports accompanying the application, and a detailed assessment of the proposed development against the relevant provisions of the Planning and Design Code, we have formed the opinion that the proposed development represents appropriate and orderly development which accords with the relevant provisions of the Code for the reasons summarised below:

- The General Neighbourhood Zone contemplates the development of a 'child care facility' under DPF 1.1.
- The proposed development satisfies all zone DPF's relating to building height, scale and setbacks, to achieve a built form outcome which is compatible with the established character of the locality, which comprises both residential and commercial development.
- The external appearance (including colours, materiality and architectural features) of the building is generally aligned with the relevant Design in Urban Areas General Development Policies.
- The proposed scheme of fencing and retaining walls proposed on-site will not have an unreasonable visual impact for adjoining land owners/occupiers noting the location of outbuildings on adjoining land in close proximity to the site boundary and the substantial separation of dwellings to the site boundary.
- The comprehensive planting of small, medium and tall trees, in conjunction with the overall landscaping scheme, will soften and enhance the appearance of the building and car park and be enhance the Main North Road streetscape.
- The proposed development will achieve the relevant objective noise criteria (the Noise Policy and WHO guidelines) and accordingly, the development will not result in unreasonable noise impacts for adjacent sensitive receivers.
- The traffic and parking assessment performed by CIRQA confirms that:
 - The carpark and access arrangements have been designed in accordance with the relevant Australian Standards;
 - The development has been designed with sufficient on-site parking as per the rate outlined within the Code, and in the scenario where the land designated for road widening land is acquired by DIT; and
 - Projected traffic generation rates and traffic distribution will have negligible impact on the function and/or capacity of the surrounding road network.
- The development will include on-site stormwater detention and a filtration system to appropriately manage the quantity and quality of stormwater discharged from the site.

On this basis, the proposed development is highly aligned with the most relevant provisions of the Planning and Design Code and warrants Planning Consent, subject to reasonable and relevant conditions.



SMITHFIELD CHILDCARE CENTRE

37 MAIN NORTH ROAD, SMITHFIELD, SA 5114

SHEET LIST - DA			
Sheet Number	Sheet Name	Revision	Revision Date
DA01	COVER SHEET	3	18/10/2023
DA02	EXISTING CONDITIONS	2	03/08/2023
DA03	CONTEXT & SITE ANALYSIS	2	03/08/2023
DA04	SITE PLAN	2	03/08/2023
DA05	FLOOR PLAN	2	03/08/2023
DA06	ROOF PLAN	2	03/08/2023
DA07	ELEVATIONS	2	03/08/2023
DA08	STREET ELEVATIONS	2	03/08/2023
DA09	SECTIONS	2	03/08/2023
DA10	3D IMAGES	3	18/10/2023

DA ISSUE		
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Rev	Amendment	Date
1	DA SET DRAFT	07/07/2023
2	DA SET	03/08/2023
3	UPDATED RENDERS	18/10/2023

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COVER SHEET

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1 : 1

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Author

Date

JULY 2023

Job No.

2023029

Dwg No.

3602 DA01

Rev: 3

A3 SHEET



ISSUED FOR DEVELOPMENT APPROVAL

Dwg No. **3602 DA02** Rev: **2** A3 SHEET



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1	DA SET DRAFT	07/07/2023
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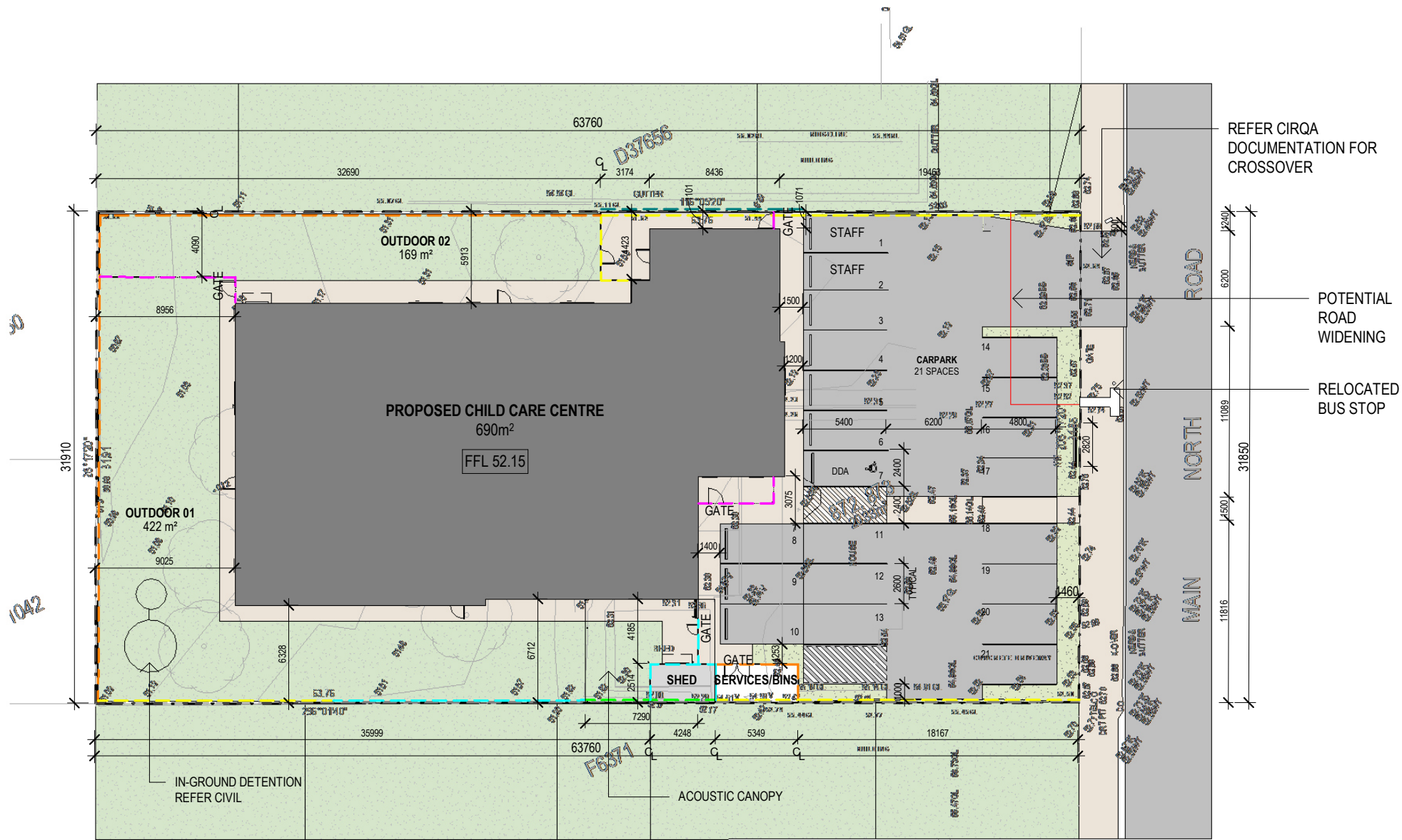
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CONTEXT & SITE ANALYSIS

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Date JULY 2023
Job No. 2023029
Dwg No. 3602 DA03 Rev: 2 A3 SHEET



SITE PLAN

1 : 300

NUMBER OF PLACES 84

SITE AREA 2026m²
SITE AREA PER PLACE 24.1m²

BUILDING AREA 690m²
BUILDING AREA PER PLACE 8.2m²

OUTDOOR PLAY AREA 591m²
NUMBER OF CARPARKS 21

DA ISSUE

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Rev	Amendment	Date
1	DA SET DRAFT	07/07/2023
2	DA SET	03/08/2023

FENCES

FT01 FENCE TYPE 01
COLORBOND METAL 'GOOD NEIGHBOUR' TYPE FENCE
ON TOP OF RETAINING WALL AS REQUIRED
1800 MINIMUM HEIGHT
COLOUR: MONUMENT

FT02 FENCE TYPE 02
COLORBOND METAL 'GOOD NEIGHBOUR' TYPE FENCE
ON TOP OF RETAINING WALL AS REQUIRED
2100 MINIMUM HEIGHT
COLOUR: MONUMENT

FT03 FENCE TYPE 03
COLORBOND METAL 'GOOD NEIGHBOUR' TYPE FENCE
2400 MINIMUM HEIGHT
COLOUR: MONUMENT

FT04 FENCE TYPE 04
COLORBOND METAL 'GOOD NEIGHBOUR' TYPE FENCE
2700 MINIMUM HEIGHT
COLOUR: MONUMENT

FT05 FENCE TYPE 05
VERTICAL SQUARE BALUSTERS
SPACED AT 125mm CENTRES
1500 MINIMUM HEIGHT
COLOUR: BLACK

NOTE:
FENCES SEALED AIRTIGHT AT ALL JUNCTIONS,
INCLUDING BETWEEN PANELS, WHERE IT JOINS THE
CANOPY AND AT THE GROUND

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SITE PLAN

Scale 1 : 300
Drawn Author
Date JULY 2023
Job No. 2023029



Dwg No. 3602 DA04 Rev: 2 A3 SHEET



GROUND PLAN

1 : 200	
NUMBER OF PLACES	84
SITE AREA	2032m ²
SITE AREA PER PLACE	24.1m ²
BUILDING AREA	690m ²
BUILDING AREA PER PLACE	8.2m ²
OUTDOOR PLAY AREA	591m ²
NUMBER OF CARPARKS	21

DA ISSUE

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FLOOR PLAN

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Job No. 2023029

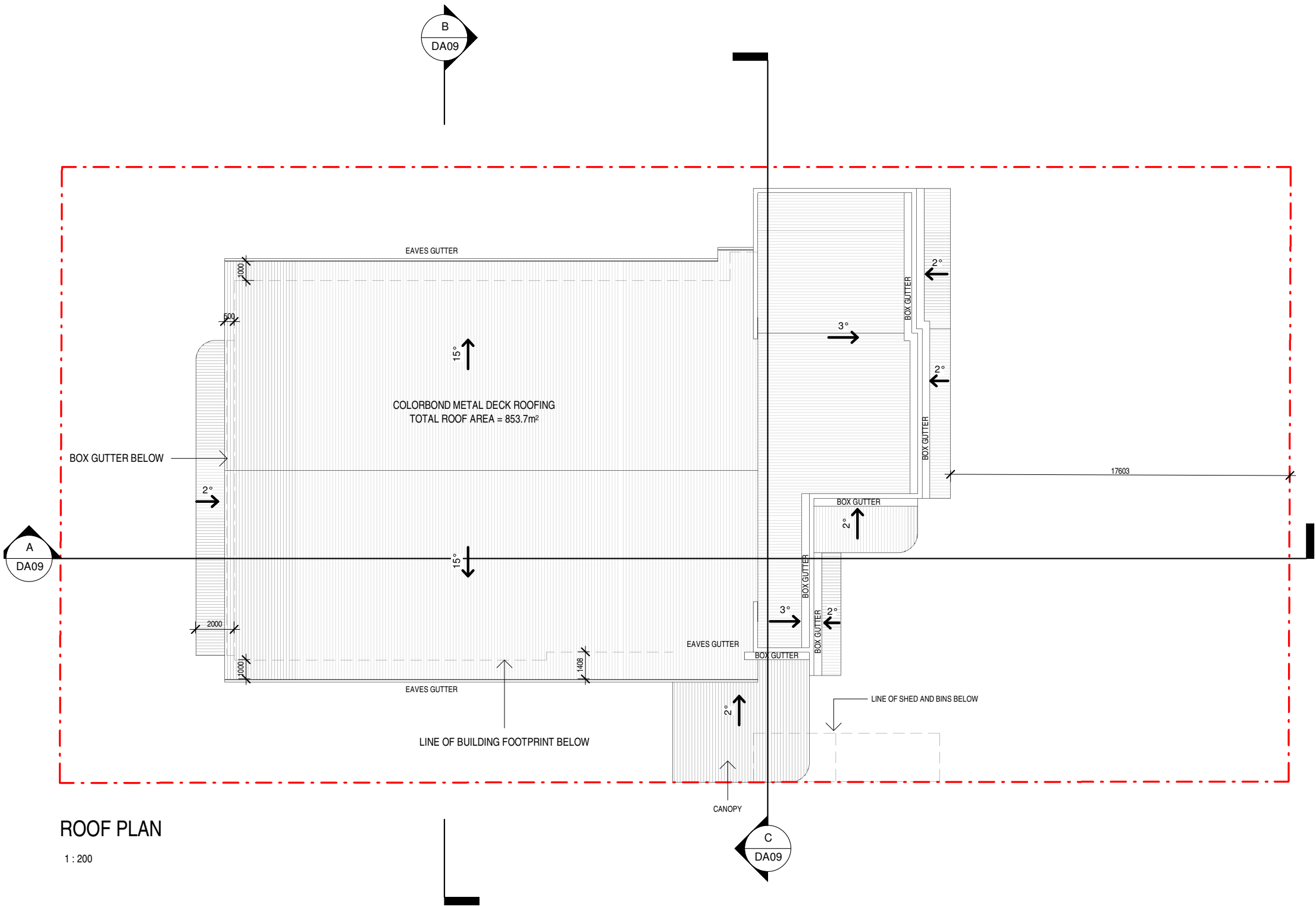


Dwg No. 3602 DA05 Rev: 2 A3 SHEET

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ROOF PLAN

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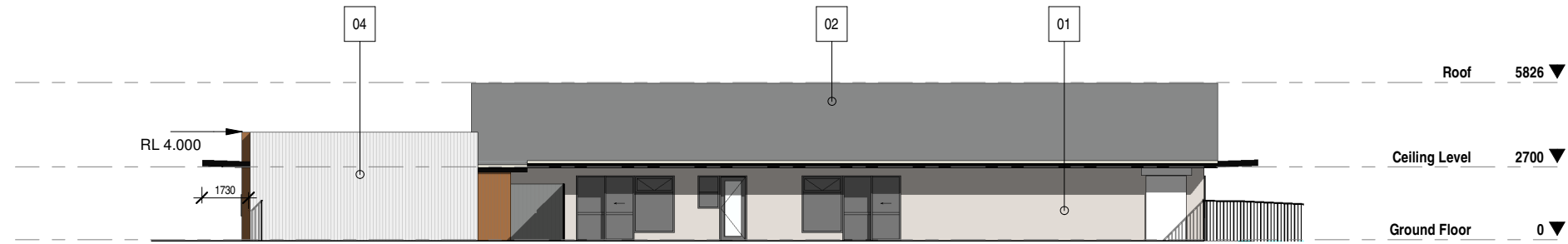
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ROOF PLAN

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Date JULY 2023
Job No. 2023029

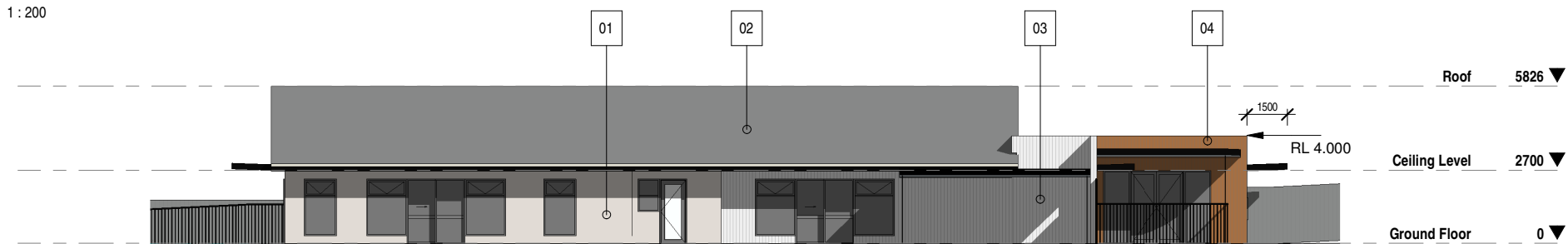


Dwg No. 3602 DA06 Rev: 2 A3 SHEET



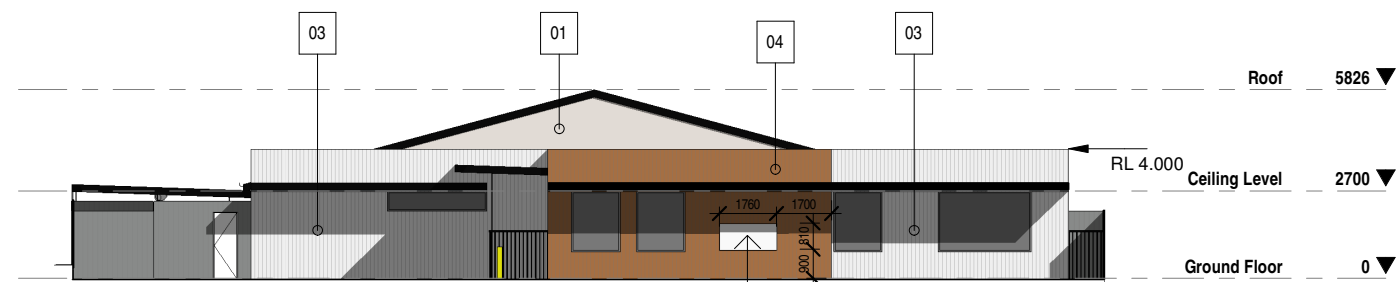
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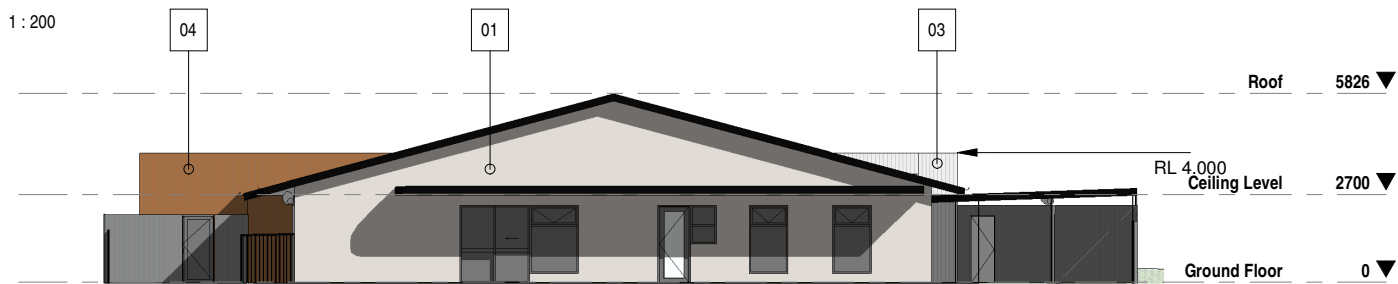
SOUTH ELEVATION

1 : 200



EAST ELEVATION

1 : 200



WEST ELEVATION

1 : 200

- 01
- HEBEL PANEL - HIGH TEXTURE
RENDER PAINTED FINISH
- 02
- CORRUGATED COLORBOND
METAL ROOF
- 03
- AXON VERTICAL CLADDING
- 04
- DECOWOOD - SPOTTED
GUM

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ELEVATIONS

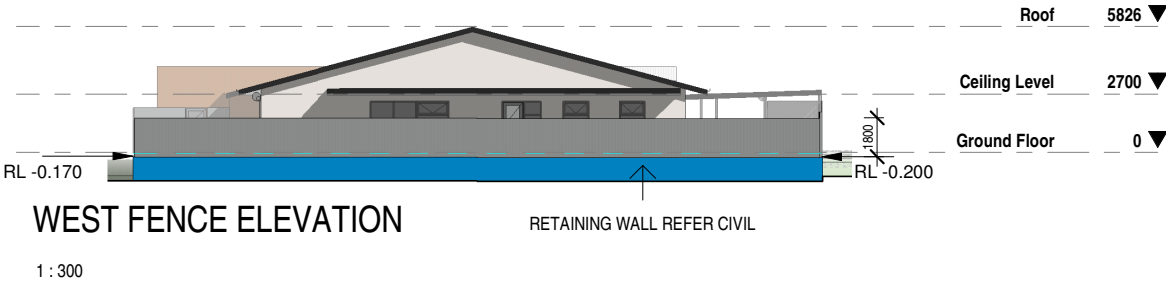
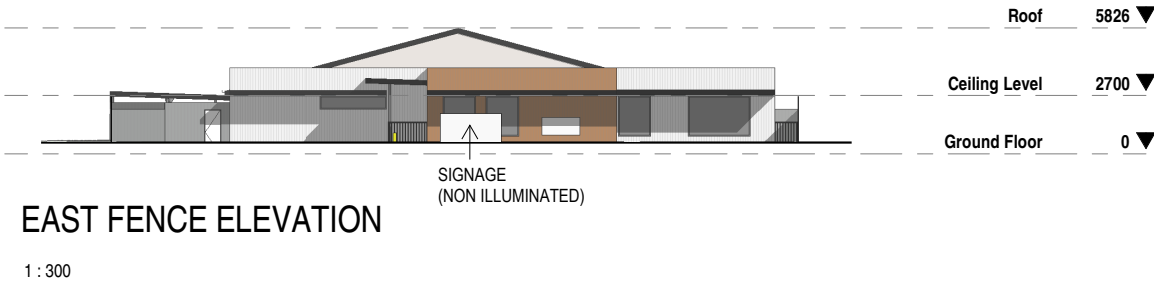
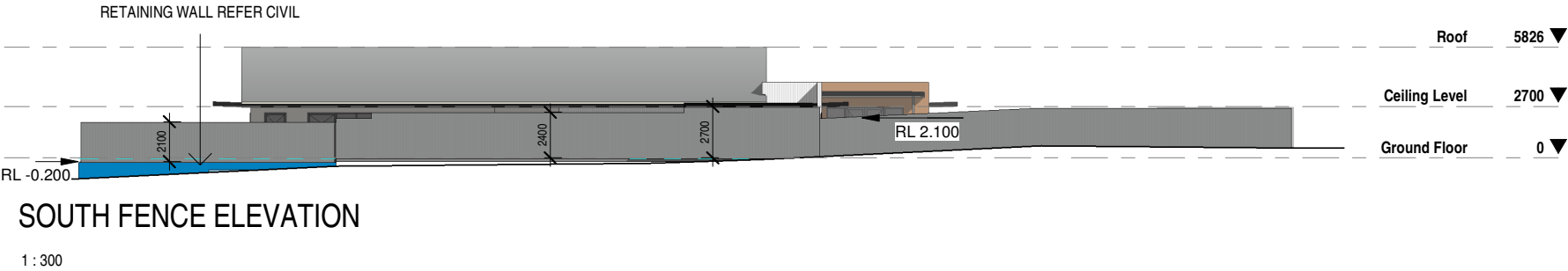
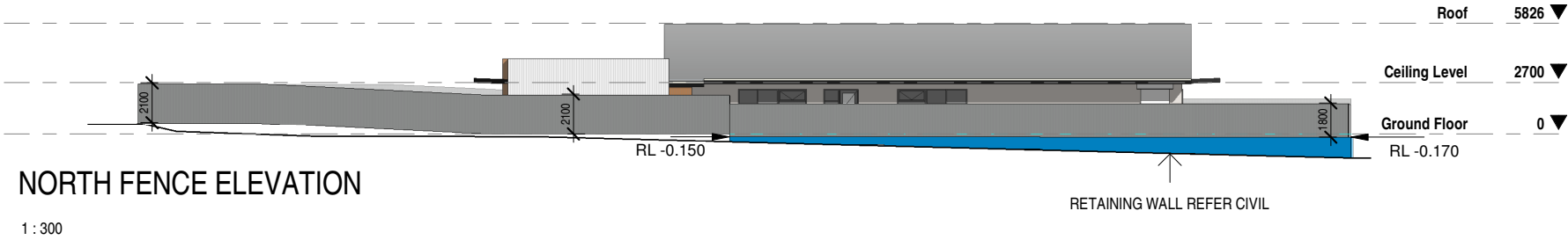
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STREET ELEVATIONS

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DA ISSUE

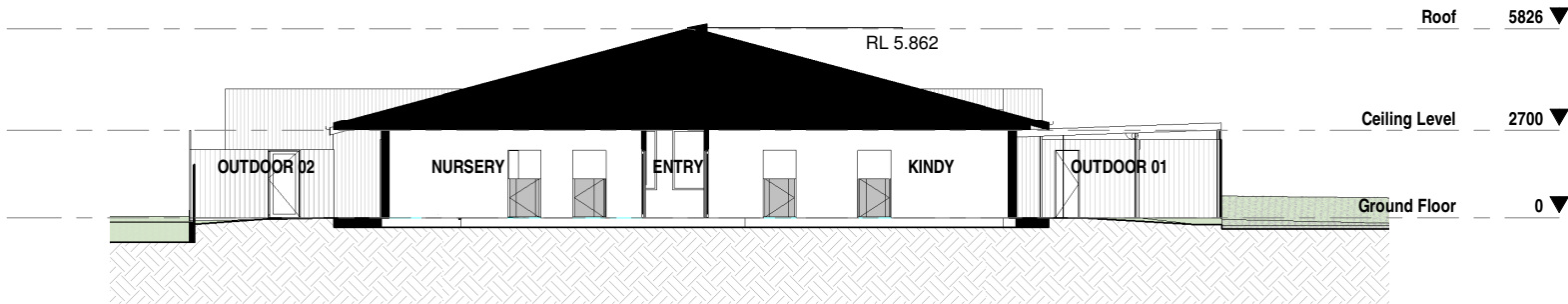
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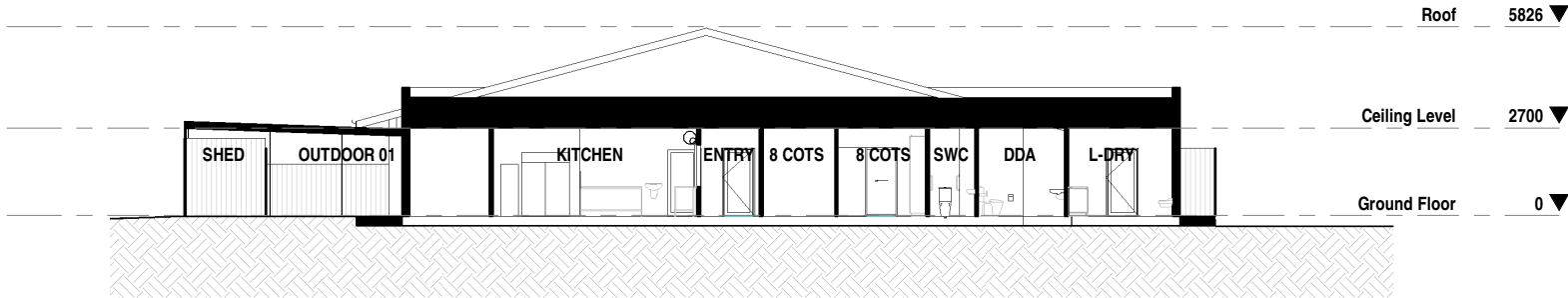
SECTION A

1 : 200



SECTION B

1 : 200



SECTION C

1 : 200

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Smithfield Child Care Centre

Environmental Noise Assessment

S7819C2

July 2023

The logo for Sonus Pty Ltd, featuring the word "sonus." in a bold, red, lowercase sans-serif font. The period at the end of the word is a solid white circle.

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Smithfield Child Care Centre
Environmental Noise Assessment
S7819C2
July 2023

sonus.

Document Title : Smithfield Child Care Centre
Environmental Noise Assessment
Client : Accord
Document Reference : S7819C2
Date : July 2023
Author : Chris Turnbull, MAAS

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Smithfield Child Care Centre
Environmental Noise Assessment
S7819C2
July 2023

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1 INTRODUCTION

An environmental noise assessment has been conducted for the proposed child care centre to be located at 37 Main North Road, Smithfield (the **Development**).

The proposed child care centre will include three outdoor play areas, with associated car parking areas accessed via Main North Road. The closest existing noise sensitive receivers to the Development are located to the south (including a 2 storey house fronting Main North Road), the west, the north and across Main North Road to the east. The site and its surroundings are shown in Figure 1 and the layout of the site is shown in Appendix A.



Figure 1: The Site and its Surroundings

Smithfield Child Care Centre
Environmental Noise Assessment
S7819C2
July 2023

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The assessment considers the noise levels at the surrounding sensitive receivers from children playing in outdoor areas, rubbish collection, car park activity, and mechanical plant operation associated with the Development.

The assessment has been based on the following:

- Brown Falconer drawing for the proposed site, Job No: 2023029, Drawing No: 3602 SK01, Dated May 2023;
- CPR Engineers ground elevation drawings of the site, Sketch Ref 230224-SK1-A, dated 20 July 2023;
- Previous noise measurements and noise data from similar sites for mechanical plant and car parking activity;
- The understanding that the total number and age of the children at the centre will be:
 - 24 x 0–2-year-olds;
 - 16 x 2–3-year-olds; and,
 - 44 x 3–5-year-olds.
- The understanding that children will be outside for an average of 6 hours per day.

Smithfield Child Care Centre
Environmental Noise Assessment
S7819C2
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2 PLANNING AND DESIGN CODE

The site and the closest receivers are located within the *General Neighbourhood Zone* of the *South Australian Planning and Design Code* (the **Code**).

The Code has been reviewed and the provisions considered most relevant to the noise assessment are included in Appendix B.

Smithfield Child Care Centre
Environmental Noise Assessment
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3 OUTDOOR PLAY AREAS

Preschools, schools, child care centres and playgrounds are often located immediately adjacent to residences and the sound of children playing during the day is rarely a concern. However, in some situations, where adjacent residences are sensitive to the sound of children's voices, the noise can be annoying. For the purposes of this assessment, it has been assumed that the existing residents in the vicinity of the proposed development are sensitive to the sound of children's voices.

3.1 Criteria

The *Deemed-to-Satisfy / Designated Performance Feature* provision for PO4.1 of the Code references the *Environment Protection (Noise) Policy 2007* (the **Policy**). However, the noise from children playing is specifically excluded from assessment under the Policy. In these circumstances, reference is made to the recommendations of the *Guidelines for Community Noise* (the **Guidelines**) published by the *World Health Organisation* (the **WHO**) with regard to annoyance during the day.

The Guidelines include:

"To protect the majority of people from being seriously annoyed during the daytime, the sound pressure level on balconies, terraces and outdoor living areas should not exceed 55 dB L_{Aeq} for a steady continuous noise. To protect the majority of people from being moderately annoyed during the daytime, the outdoor sound pressure level should not exceed 50 dB L_{Aeq} ."

Based on the above, it is proposed that noise reduction measures be designed for the proposal such that the equivalent noise levels (L_{Aeq}) during the daytime hours from children playing are no greater than 50 dB(A) at the closest sensitive receivers.

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3.2 Assessment

The noise from children in outdoor areas has previously been measured at similar child care facilities. Based on these measurements, the noise from the proposed facility has been predicted for the centre operating at full capacity in all age groups, totalling 84 children.

The noise levels generated from children of various ages playing in outdoor areas, that have been used as the basis of this assessment, are provided in Appendix C.

In order to achieve the assessment criterion, the following treatments are recommended:

- Ensure the fences marked-up in Figure 2 are constructed as follows:
 - A minimum of 1.8m high above the retaining wall and from a material such as 0.35mm BMT sheet steel (such as Colorbond) for the extent shown as **ORANGE**;
 - A minimum of 2.1m high above the retaining wall and from a material such as 0.35mm BMT sheet steel (such as Colorbond) for the extent shown as **RED**;
 - A minimum of 2.4m high and from a material such as 0.35mm BMT sheet steel (such as Colorbond) for the extent shown as **BLUE**;
 - A minimum of 2.7m high and from a material such as 0.35mm BMT sheet steel (such as Colorbond) or Perspex or glass for the extent shown as **GREEN**;
- Construct a canopy over the outdoor play area for the extent shaded as **PURPLE** in Figure 2 to a minimum height of 2.7m; and,
- Ensure fences are sealed airtight at all junctions, including between panels, where it joins the canopy and at the ground.

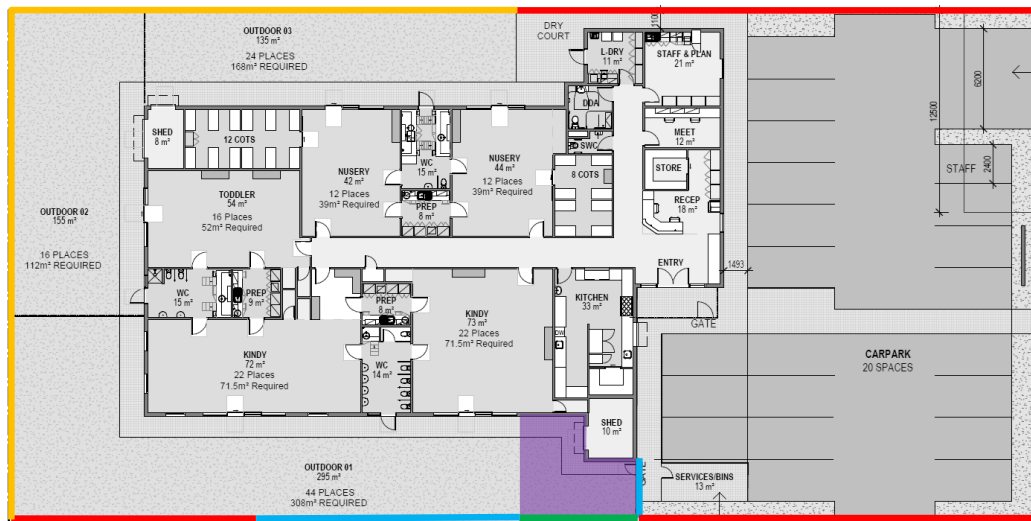


Figure 2: Treatment Summary

With the treatments incorporated, the highest predicted noise level resulting from children playing is 50 dB(A) at the closest sensitive receivers.

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4 RUBBISH COLLECTION

4.1 Criteria

The *Deemed-to-Satisfy / Designated Performance Feature* provision for *PO4.1* of the Code references the *Environment Protection (Noise) Policy*. The Policy deals with rubbish collection by effectively limiting the hours to the least sensitive period of the day. Division 3 of the Policy requires rubbish collection to only occur between the hours of 9:00am and 7:00pm on Sundays or public holidays and between 7:00am and 7:00pm on any other day, unless it can be shown that the instantaneous maximum noise level (L_{max}) from the rubbish collection activity is less than 60 dB(A).

4.2 Assessment

In order to satisfy the requirements of the Policy, it is recommended that rubbish collection only occur between the hours of 9:00am and 7:00pm on a Sunday or public holiday and between 7:00am and 7:00pm on any other day. The hours correspond to the least sensitive period of the day and when noise levels from other activity in the environment would be highest.

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5 CAR PARK ACTIVITY AND MECHANICAL PLANT

5.1 Criteria

The *Deemed-to-Satisfy / Designated Performance Feature* provision for PO4.1 of the Code references the *Environment Protection (Noise) Policy*. The Policy is based on preventing adverse impacts on the amenity of a locality and it is therefore considered that where the noise from car park activity and mechanical plant at the facility achieve the Policy, other *Performance Outcomes* are also achieved.

The Policy provides goal noise levels to be achieved at sensitive receivers, based on the principally promoted land uses of the zones within the Code in which the noise source (child care centre) and the noise receivers are located. The Policy applies noise goals that are 5 dB(A) lower for proposed developments, resulting in the following goal noise levels:

- An equivalent noise level (L_{Aeq}) of 47 dB(A) during the day (7:00am to 10:00pm);
- An equivalent noise level (L_{Aeq}) of 40 dB(A) during the night (10:00pm to 7:00am); and,
- An instantaneous maximum noise level (L_{max}) of 60 dB(A) during the night (10:00pm to 7:00am).

It is noted that where the existing ambient noise environment already includes maximum instantaneous noise levels greater than 60 dB(A), the Policy allows the 60 dB(A) criterion to be exceeded.

When measuring or predicting noise levels for comparison with the Policy, adjustments may be made for each “annoying” characteristic of tonality, impulsiveness, low frequency, and modulation of the noise sources. The characteristic must be considered dominant in the acoustic environment and therefore the application varies depending on the assessment location, time of day, the noise source being assessed and the predicted noise levels. The application of penalties is discussed further in the following section.

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5.2 Assessment

The noise levels at the closest sensitive receivers from the proposed site activity have been predicted based on a range of previous noise measurements and observations at similar facilities. These include:

- General car park activity such as people talking as they vacate or approach their vehicles, the opening and closing of car doors, vehicles starting, vehicles idling, and vehicles moving into and accelerating away from parked positions; and,
- Mechanical plant serving the building.

As is typical at the Development Application stage, the proposed mechanical plant units have not yet been designed or selected. Therefore, the assessment of the mechanical plant has been based on a typical selection, consisting of two outdoor air conditioning units. The sound power level of the equipment is as shown in Appendix C.

The predictions have been based on the fences in Figure 2 being installed and the following assumed activity levels within any 15-minute period (the default assessment period of the Policy):

- Day Time (7:00am to 10:00pm):
 - 10 vehicles movements into or out of the car park and corresponding general car park activity at the available car parks; and,
 - Continuous operation of the mechanical plant.
- Night Time (10:00pm to 7:00am):
 - 5 vehicles movements into or out of the car park and corresponding general car park activity at the available car parks; and,
 - Continuous operation of the mechanical plant.

In order to achieve the assessment criterion, the mechanical plant should be located within the **YELLOW** area designated in the Figure 3.

A 5dB(A) penalty associated with the modulation of noise has been applied at all sensitive receivers exposed to the character of noise from the onsite vehicles.

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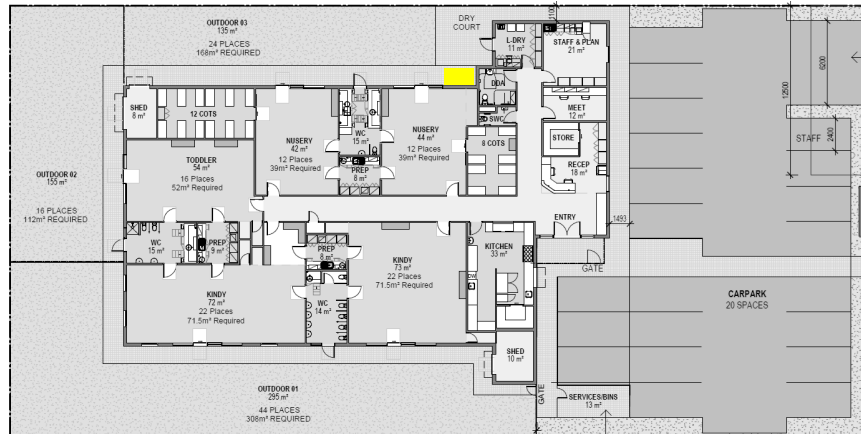


Figure 3: Treatment Summary

With the application of the penalty and the assumed level of activity at the site, the equivalent noise levels (L_{eq}) predicted are no higher than 44 dB(A) during the day time period and no higher than 39 dB(A) during the night time period.

The instantaneous maximum noise levels have also been predicted for all surrounding sensitive receivers. Predicted maximum noise levels have been based on measurements at a variety of different similar sites and include noise sources such as car doors slamming and vehicles accelerating.

The predicted maximum noise levels at sensitive receivers fronting Main North Road are no more than 64 dB(A) and no more than 58dB(A) at all the other sensitive receivers in the vicinity. An attended noise measurement was conducted at location equivalent to the closest residences fronting Main North Road which indicates that the receivers are already exposed to a maximum noise level of (L_{max}) of 67 dB(A) for typical vehicle passbys on Main North Road. Therefore, the noise from vehicle activity on the site will be lower in level and less frequent than the vehicle noise from Main North Road. The activity therefore cannot cause an unreasonable interference and achieves the intent of the Policy.

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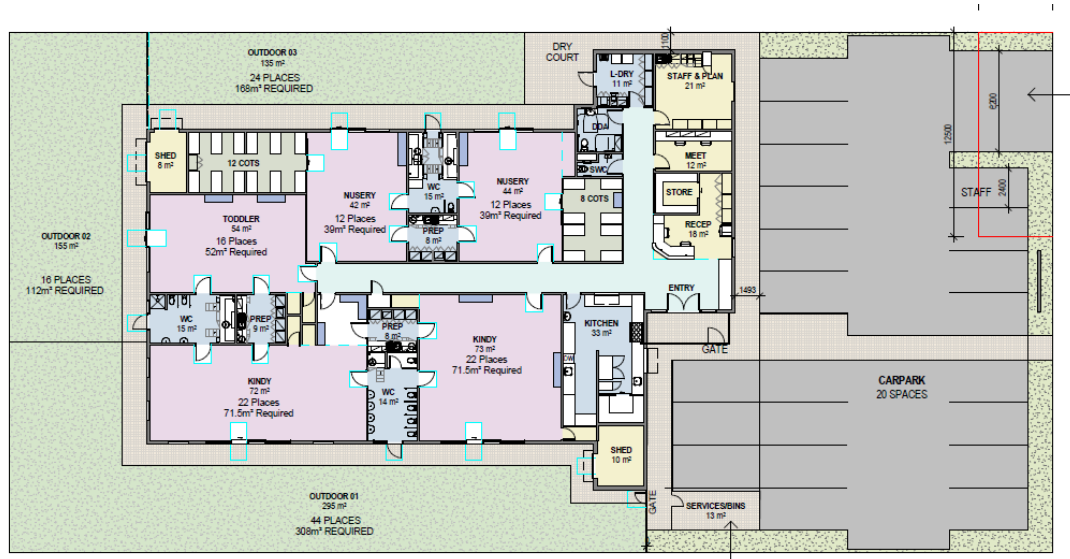
6 CONCLUSION

An environmental noise assessment has been conducted for the proposed child care centre to be located at 37 Main North Road, Smithfield.

The assessment has considered noise at noise sensitive receivers in the vicinity, from children playing in outdoor areas, rubbish collection, car park activity and mechanical plant operation.

Relevant assessment criteria have been established based on the *South Australian Planning and Design Code*, the *Environment Protection (Noise) Policy 2007* and the *World Health Organisation* recommendations to protect against annoyance. Specific fence constructions, canopy and mechanical plant location have been recommended in order to achieve the noise criteria.

Based on the above, the Development has been designed to *not unreasonably impact the amenity of sensitive receivers*, thereby achieving the relevant provision of the *South Australian Planning and Design Code* related to environmental noise.



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APPENDIX B: SOUTH AUSTRALIAN PLANNING AND DESIGN CODE – RELEVANT PROVISIONS

PART 4 – GENERAL DEVELOPMENT POLICIES

Interface between Land Uses

Desired Outcome (DO)

DO 1 Development is located and designed to mitigate adverse effects on or from neighbouring and proximate land uses.

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature								
General Land Use Compatibility									
PO 1.2 Development adjacent to a site containing a sensitive receiver (or lawfully approved sensitive receiver) or zone primarily intended to accommodate sensitive receivers is designed to minimise adverse impacts.	DTS/DPF 1.2 None are applicable.								
Hours of Operation									
PO 2.1 Non-residential development does not unreasonably impact the amenity of sensitive receivers (or lawfully approved sensitive receivers) or an adjacent zone primarily for sensitive receivers through its hours of operation having regard to: <ul style="list-style-type: none"> (a) the nature of the development (b) measures to mitigate off-site impacts (c) the extent to which the development is desired in the zone (d) measures that might be taken in an adjacent zone primarily for sensitive receivers that mitigate adverse impacts without unreasonably compromising the intended use of that land. 	DTS/DPF 2.1 Development operating within the following hours: <table border="1"> <thead> <tr> <th>Class of Development</th><th>Hours of operation</th></tr> </thead> <tbody> <tr> <td>Consulting room</td><td>7am to 9pm, Monday to Friday 8am to 5pm, Saturday</td></tr> <tr> <td>Office</td><td>7am to 9pm, Monday to Friday 8am to 5pm, Saturday</td></tr> <tr> <td>Shop, other than any one or combination of the following: (a) restaurant (b) cellar door in the Productive Rural Landscape Zone, Rural Zone or Rural Horticulture Zone </td><td>7am to 9pm, Monday to Friday 8am to 5pm, Saturday and Sunday</td></tr> </tbody> </table>	Class of Development	Hours of operation	Consulting room	7am to 9pm, Monday to Friday 8am to 5pm, Saturday	Office	7am to 9pm, Monday to Friday 8am to 5pm, Saturday	Shop, other than any one or combination of the following: (a) restaurant (b) cellar door in the Productive Rural Landscape Zone, Rural Zone or Rural Horticulture Zone	7am to 9pm, Monday to Friday 8am to 5pm, Saturday and Sunday
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Consulting room	7am to 9pm, Monday to Friday 8am to 5pm, Saturday								
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Activities Generating Noise or Vibration	
<p>PO 4.1 <i>Development that emits noise (other than music) does not unreasonably impact the amenity of sensitive receivers (or lawfully approved sensitive receivers).</i></p>	<p>DTS/DPF 4.1 <i>Noise that affects sensitive receivers achieves the relevant Environment Protection (Noise) Policy criteria.</i></p>
<p>PO 4.2 <i>Areas for the on-site manoeuvring of service and delivery vehicles, plant and equipment, outdoor work spaces (and the like) are designed and sited to not unreasonably impact the amenity of adjacent sensitive receivers (or lawfully approved sensitive receivers) and zones primarily intended to accommodate sensitive receivers due to noise and vibration by adopting techniques including:</i></p> <ul style="list-style-type: none"> <i>(a) locating openings of buildings and associated services away from the interface with the adjacent sensitive receivers and zones primarily intended to accommodate sensitive receivers</i> <i>(b) when sited outdoors, locating such areas as far as practicable from adjacent sensitive receivers and zones primarily intended to accommodate sensitive receivers</i> <i>(c) housing plant and equipment within an enclosed structure or acoustic enclosure</i> <i>(d) providing a suitable acoustic barrier between the plant and / or equipment and the adjacent sensitive receiver boundary or zone.</i> 	<p>DTS/DPF 4.2 <i>None are applicable.</i></p>

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APPENDIX C: SOUND POWER LEVELS

Activity		Sound Power Level
Car Park Activities	General Activity	83 dB(A)
Mechanical Plant	A/C Condenser Unit	73 dB(A)
Children	0–2-year-old (per child)	68 dB(A)
	2–3-year-old (per child)	75 dB(A)
	3–5-year-old (per child)	77 dB(A)



**PROPOSED CHILD CARE CENTRE
37 MAIN NORTH ROAD, SMITHFIELD**

TRAFFIC AND PARKING REPORT





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DOCUMENT CONTROL

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Traffic and Parking report

Project number: 23105

Client: Development Holdings Pty Ltd

Client contact: Harry Viney

Version	Date	Details/status	Prepared by	Approved by
Draft	18 Jul 23	For review	JJB	BNW
V1.0	17 Aug 23	For submission	JJB	BNW
V1.1	18 Aug 23	Minor update	JJB	BNW

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1. INTRODUCTION

CIRQA has been engaged to provide design and assessment advice for a proposed child care centre at 37 Main North Road, Smithfield. Specifically, CIRQA has been engaged to provide advice in respect to traffic and parking aspects of the proposal.

This report provides a review of the subject site, the proposed development, its access and parking provisions and the associated traffic impact on the adjacent road network. The traffic and parking assessments have been based upon plans prepared by Brown Falconer (drawing no. 3602 DA04 Rev 2, dated 03 August 2023, refer Appendix A).

2. BACKGROUND

2.1 SUBJECT SITE

The subject site is located on the western side of Main North Road. The site is bound by Main North Road to the east and residential dwellings to the remaining sides.

The Planning and Design Code identifies that the site is located within a General Neighbourhood Zone, with the following overlays applicable:

- Defence Aviation Area (All structures over 90 metres);
- Future Road Widening;
- Major Urban Transport Routes;
- Prescribed Wells Area;
- Regulated and Significant Tree;
- Stormwater Management;
- Traffic Generating Development; and
- Urban Tree Canopy.

A 4.5 m wide 'Type C' road widening requirement applies to the site's northern 12.5 m (approximate) of frontage. The Type C requirement is associated with the potential upgrade of the Curtis Road, Craigmore Road and Main North Road intersection.

The subject site is currently occupied by a residential dwelling. Vehicle access is provided via single width crossover on Main North Road, at which turning

movements are restricted to left-in and left-out by a solid median on Main North Road.

Figure 1 illustrates the location of the subject site with respect to the adjacent road network.



Figure 1 – Location of the subject site with respect to the adjacent road network

2.2 ADJACENT ROAD NETWORK

Main North Road is an arterial road under the care and control of the Department for Infrastructure and Transport (DIT). Adjacent the site, Main North Road comprises two through bound traffic lanes and a parking lane in each direction. In addition, a short-right turn lane forms adjacent the site. Traffic data obtained from DIT indicates that this section of Main North Road has an Annual Average Daily Traffic (AADT) volume in the order of 42,700 vehicles per day (vpd), of which approximately 3.5% are commercial vehicles. Adjacent the site, an 80 km/h speed limit applies on Main North Road.

2.3 WALKING AND CYCLING

Sealed footpaths are provided on both sides of Main North Road, servicing both pedestrians and cyclists. Cyclists are also able to ride on-street sharing the road with motorists.



2.4 PUBLIC TRANSPORT

Public bus services operate regularly in the vicinity of the subject site. Bus stops are located within 85 m of the subject site on both sides of Main North Road (including one bus stop directly adjacent the site). These stops are serviced by the following bus routes:

- 440 – Munno Para Station to Elizabeth Interchange;
- 443 – Elizabeth Interchange to Munno Para Loop Service; and
- N224 – After Midnight Saturday PM - Sunday AM Gawler to City.

Additional bus stops are located on Curtis Road and Craigmore Road which are serviced by additional bus routes. The Smithfield Railway Station is also located within relatively close proximity of the site (approximately 650 m to the west). The Smithfield Railway Station is serviced at a high frequency by trains operating on the Gawler/Gawler Central Railway Line.

3. PROPOSED DEVELOPMENT

3.1 LAND USE AND YIELD

The proposed development comprises the demolition of the existing infrastructure on the subject site and the construction of an 84-place child care centre.

3.2 ACCESS AND PARKING DESIGN

The site will be serviced by a 21-space parking area, of which 1 space will be reserved exclusively for use by people with disabilities. The parking area will comply with the requirements of Australian/New Zealand Standard, *Parking Facilities Part 1: Off-street car parking* (AS/NZS 2890.1:2004) and Australian/New Zealand Standard, *Parking Facilities Part 6: Off-street parking for people with disabilities* (AS/NZS 2890.6:2022) in that:

- regular parking spaces will be 2.6 m wide and 5.4 m long (or 4.8 m long with 0.6 m overhang);
- the disabled parking space will be 2.4 m wide and 5.4 m long (with an adjacent shared space of the same dimension);
- the parking aisle will be at least 6.2 m wide;
- a 1.0 m end-of-aisle extension will be provided beyond the last parking space in the aisle;
- a turn-around bay will be provided at the end of the parking aisle;



- 0.3 m clearance will be provided to all objects greater than 0.15 m in height; and
- pedestrian sightlines will be provided at the site's access point.

Vehicle access to the site will be provided via a 6.2 m wide two-way crossover on Main North Road, while all redundant crossovers will be reinstated as upright kerb. The access point will accommodate two-way movements with entering vehicles able to be driven past another vehicle stored waiting to exit the site. All vehicles will be able to enter and exit the site in a forward direction. The solid median on Main North Road will restrict movements to left-in and left-out only. The sealed shoulder will also function as a pseudo-deceleration lane for the access to the site (noting that parking is banned and the presence of a bus at the stop would be infrequent).

The proposed access location will require relocation of the existing bus stop in front of the site (there is adequate room to accommodate the standard bus pad treatment to the south of the proposed access).

Consideration was given to retention of the access point in its existing location (which was DIT's preference advised during initial discussions). However, given the MARWP requirement and potential future widening, the access point has been proposed at the northern end of the site to minimise impact on the layout and number of parking spaces achievable. If the MARWP requirement is taken in the future would result in the removal of up to two parking spaces (whereas an access in the southern location would result in four to five spaces needing to be removed if the widening requirement was taken).

3.3 REFUSE COLLECTION

Refuse collection will be undertaken via private contractor with the associated manoeuvres accommodated on-site (forward-in/forward-out). The site will be able to accommodate movements by an 8.8 m long Medium Rigid Vehicle (MRV). It is anticipated that such movements would be undertaken outside of opening hours. Figure 2 illustrates the turn path for an MRV to enter and exit the site in a forward direction.

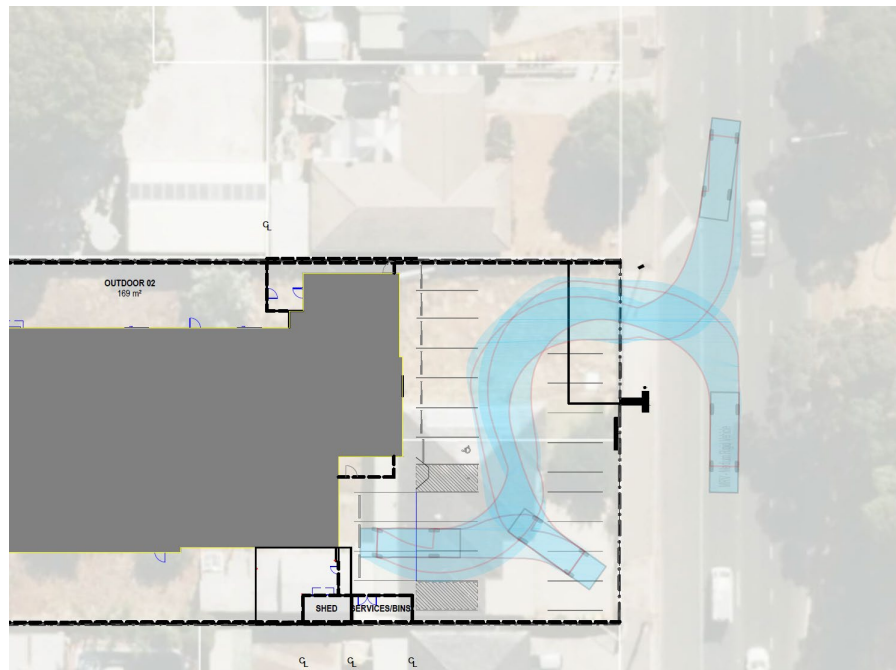


Figure 2 – MRV turning movements into and out of the site

4. PARKING ASSESSMENT

The Planning and Design Code identifies a Deemed-to-Satisfy/Designated Performance Feature (DTS/DPF) parking requirement of 0.25 spaces per child for land uses classified as 'child care centres' (equivalent to a rate of one space per four children). Based upon a capacity of 84 children, the proposed child care centre would have a theoretical requirement for 21 spaces. Given that 21 spaces will be provided, the DTS/DPF parking requirement identified within the Planning and Design Code will be satisfied.

In the event that DIT took the full MARWP requirement, there would be a loss of two parking spaces (resulting in 19 spaces being provided on site). While this would result in a level of provision below the DTS/DPF criteria, the above rate is conservative and realistic demands surveyed child care centres in metropolitan Adelaide have identified peak demand rates of one space per 4.2 to one space per 7.7 children (inclusive of both parent and staff parking demands). On the basis of these rates, there would be demands for in the order of 11 to 20 parking spaces associated with the site. Notably, the more recent surveys have indicated lower demands within this range (in the order of 1 space per 6.5 to 7.7 children) which would suggest demands towards the lower end of the above range. It is therefore anticipated that even with removal of two spaces (if the full MARWP requirement



is taken) that peak demands would still typically be accommodated wholly within the site.

5. TRAFFIC ASSESSMENT

5.1 TRAFFIC GENERATION AND DISTRIBUTION

The RTA's *"Guide to Traffic Generating Developments"* (the RTA Guide, now Transport for NSW), and its subsequent updates, are documents commonly used by traffic engineers in order to determine the forecast traffic generation of a variety of land uses.

An update to the RTA child care centre traffic generation rate was prepared by TEF Consulting and the RTA in September 2015. The updated study identified that the previously recommended rates were based on surveys from 1992, and were considered out of date. Based on detailed statistical analysis, the updated TEF Consulting report identified the following rate for assessment of peak traffic generation at child care centres (where X_1 is the number of licensed places for children):

- am peak hour trips – $0.0118 X_1^2 - 0.3585 X_1 + 22.968$; and
- pm peak hour trips – $0.004 X_1^2 + 0.4117 X_1 + 6.0276$.

On the basis of the above equations, it is forecast that the proposal will generate 77 am peak hour trips and 69 pm peak hour trips. In CIRQA's experience, such rates are higher than typically experienced at child care centres in Greater Adelaide. Nevertheless, these forecasts have been adopted for conservatism.

Vehicle movements will be distributed via the site's access point on Main North Road. All movements at the access will be restricted to left-in/left-out due to the solid median on Main North Road. For the purposes of this assessment, the following distribution assumptions have been adopted:

- am peak hour – 60% of trips are inbound and 40% of trips are outbound (based on the comparable survey data);
- pm peak hour – 50% of trips are inbound and 50% of trips are outbound (based on the comparable survey data); and
- in respect to the distribution to/from the broader road network, 20% of movements are to/from the north, 25% are to/from the east, 30% are to/from the south and 25% to/from the west.



Figure 3 illustrates the forecast am and (pm) peak hour movements at the access point.

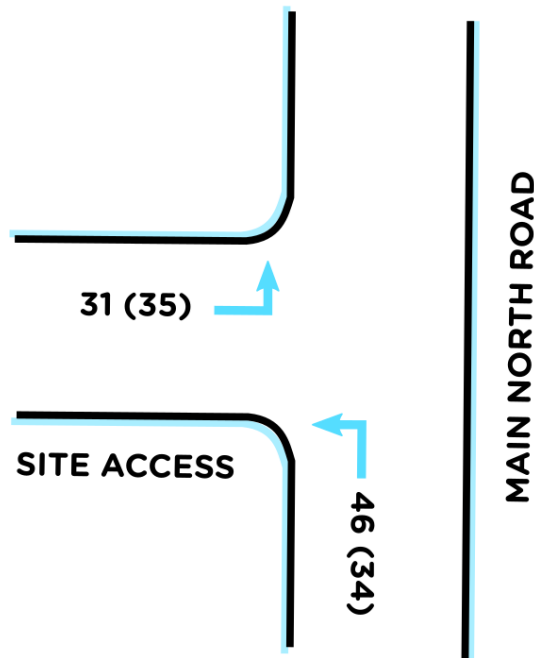


Figure 3 – Forecast am(pm) peak hour movements at the proposed access point

As right-turn movements into the site are restricted by the solid median on Main North Road. Visitors to the site will be required to approach the site from the south to turn left-in. Visitors will be able to use the U-turn facility on Main North Road when approaching from the north or by using the wider road network to (i.e. via Uley Road or Anderson Walk).

Similarly, vehicles exiting the site to travel south will be able to use the U-turn facility on Main North Road or utilise the wider road network (i.e. turning onto Curtis Road or Craigmole Road. Drivers will need to wait for an appropriate gap in the traffic stream before turning left-out and crossing the traffic lanes to reach the U-turn facility.

It is estimated that in the order of 11 to 13 vehicles may undertake the U-turn movement when entering the site and in the of 12 to 16 when exiting the site in the peak hours. It is noted that vehicles undertaking a U-turn to and from the site will utilise separate U-turn facilities on Main North Road. Additionally, prevailing traffic conditions may dictate the routes undertaken by drivers and if queuing



and delays associated with the U-turn movements are excessive, it is likely they will use alternative routes to access the site (noting that users will become familiar with conditions associated with access to/from the site).

Based upon the above, it is considered that the movements generated by the proposal can be accommodated at the access point and on the broader road network. It anticipated that the proposal will result in a minimal impact on the adjacent road network.

It is important to note that that above movement forecasts above relate to the absolute peak periods associated with the child care centre. However, such periods do not typically directly align with the commuter peak hour periods on adjacent road networks. The forecasts also do not take into account 'passing trade' (i.e. parents/carers dropping children off on the way to work who would already pass the site). In reality, a portion of movements would already exist on the adjacent road network and additional movements would be less than stated above. For the above reasons, the assessment is considered conservative.

6. SUMMARY

The proposal comprises the construction of an 84 place child care centre with associated access and parking provisions. Vehicle access to the site will be provided via a two-way access point on Main North Road. The site has been designed such that all movements can enter and exit in a forward direction.

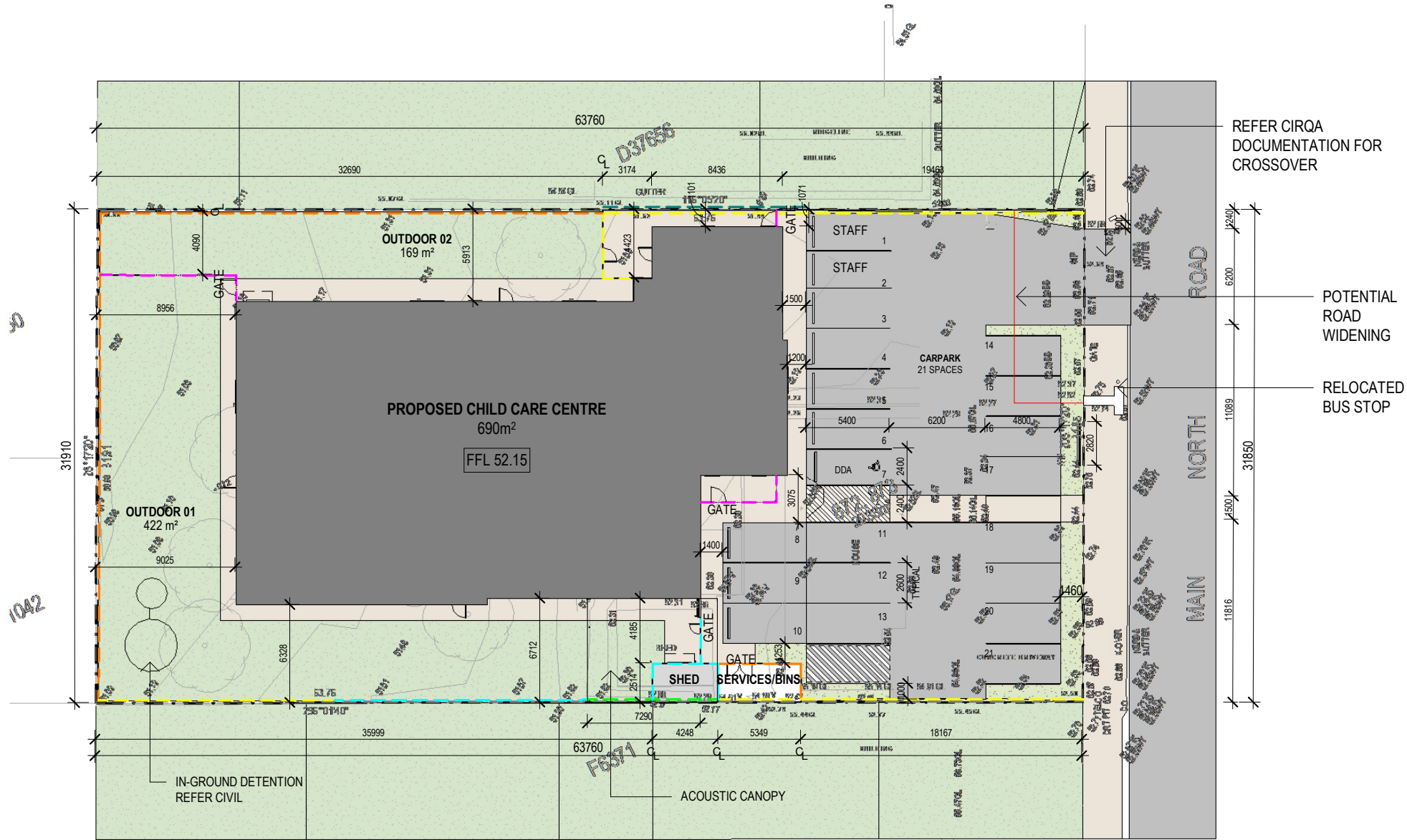
A total of 21 parking spaces will be provided on-site. Such a provision will satisfy the parking requirements of the Planning and Design Code. The parking area will be provided in accordance with the relevant Australian Standard.

The proposal is forecast to generate in the order of 77 am and 69 pm peak hour trips. Such movements will be adequately accommodated at the proposed site access and on the adjacent road network.



APPENDIX A

BROWN FALCONER PLANS



SITE PLAN

1 : 300

NUMBER OF PLACES 84

SITE AREA 2026m²
SITE AREA PER PLACE 24.1m²

BUILDING AREA 690m²
BUILDING AREA PER PLACE 8.2m²

OUTDOOR PLAY AREA 591m²
NUMBER OF CARPARKS 21

DA ISSUE

ISSUED FOR DEVELOPMENT APPROVAL

Rev	Amendment	Date
1	DA SET DRAFT	07/07/2023
2	DA SET	03/08/2023

FENCES

FT01 FENCE TYPE 01
COLORBOND METAL 'GOOD NEIGHBOUR' TYPE FENCE
ON TOP OF RETAINING WALL AS REQUIRED
1800 MINIMUM HEIGHT
COLOUR: MONUMENT

FT02 FENCE TYPE 02
COLORBOND METAL 'GOOD NEIGHBOUR' TYPE FENCE
ON TOP OF RETAINING WALL AS REQUIRED
2100 MINIMUM HEIGHT
COLOUR: MONUMENT

FT03 FENCE TYPE 03
COLORBOND METAL 'GOOD NEIGHBOUR' TYPE FENCE
2400 MINIMUM HEIGHT
COLOUR: MONUMENT

FT04 FENCE TYPE 04
COLORBOND METAL 'GOOD NEIGHBOUR' TYPE FENCE
2700 MINIMUM HEIGHT
COLOUR: MONUMENT

FT05 FENCE TYPE 05
VERTICAL SQUARE BALUSTERS
SPACED AT 125mm CENTRES
1500 MINIMUM HEIGHT
COLOUR: BLACK

NOTE:
FENCES SEALED AIRTIGHT AT ALL JUNCTIONS,
INCLUDING BETWEEN PANELS, WHERE IT JOINS THE
CANOPY AND AT THE GROUND

**BROWN
FALCONER**

28 Chesser Street, Adelaide, South Australia 5000
Telephone : 08 8203 5800 Facsimile : 08 8223 2440
ABN 65 007 846 586 brownfalconer.com.au

ACCORD PROPERTY LTD

SMITHFIELD CCC

SITE PLAN

Scale 1 : 300
Drawn Author
Date JULY 2023
Job No. 2023029



Dwg No. 3602 DA04 Rev: 2 A3 SHEET

REPORT

ACCORD PROPERTY

Smithfield Child Care Centre

37 Main North Road, Smithfield, SA 5114

> STORMWATER MANAGEMENT PLAN



SMITHFIELD CHILD CARE CENTRE STORMWATER MANAGEMENT PLAN

Site Address: 37 Main North Road, Smithfield SA 5114

Project Number: 230224

ISSUE REGISTER PROJECT:

ISSUE DATE	REASON	PREPARED	REVIEWED
1/08/2023	DRAFT	Mia Apollonio	David Reynolds
15/08/2023	PLANNING APPROVAL	Mia Apollonio	David Reynolds
13/10/2023	PLANNING APPROVAL	Mia Apollonio	David Reynolds

Visit

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E admin@cprengineers.com.au

SMITHFIELD CHILD CARE CENTRE

STORMWATER MANAGEMENT PLAN

230224 - Friday, 13 October 2023

INTRODUCTION

The following report outlines the key requirements to manage the disposal of stormwater from the post development site situated at 37 Main North Road, Smithfield, SA 5114.

The existing site currently comprises a main dwelling, some lightweight sheds, landscaped, concrete, and paved areas. The existing buildings, concrete and pavement will be removed, and a new building, carpark and landscaped areas constructed.

The stormwater concept has been based on the architectural plans prepared by Brown Falconer, and the survey provided, representing a layout as below.



Figure 1: Proposed New Site Plan (Source: Brown Falconer)

This Stormwater Management Plan has been prepared in accordance with of the City of Playford's Stormwater Requirements outlined in Appendix A of the Council's Land Division Guidelines.

This document is to be read in conjunction with:

- Brown Falconer's Architectural drawings, and site plans;
- CPR Engineers' Stormwater Management Plan 230224-C01; and
- CPR Engineers' Stormwater Calculations.

Should further details be required during this period, CPR Engineers will be able to provide these as necessary.



GENERAL STORMWATER MANAGEMENT

The new works will be designed for the following stormwater criteria for commercial developments as outlined by the City of Playford (refer attached excerpt):

- Pre-development flows are based on 1:10 ARI;
- Post-development flows are based on 1:100 ARI; and
- Post-development discharge is to be limited to pre-development rates.

It is proposed that stormwater from the development will be detained on site by use of underground stormwater detention tanks with pumped discharge.

The stormwater from the site will discharge to the water table on Main North Road at a limited rate of 18.0L/s.

FINISHED FLOOR LEVEL REQUIREMENTS

Flood overlay of the area according to the SAPPa website indicates no flooding.

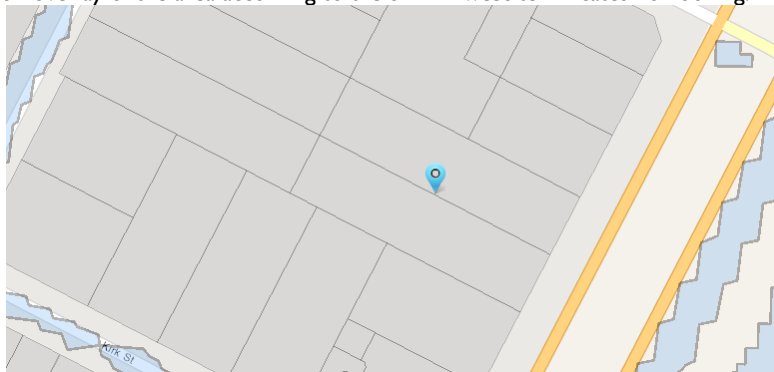


Figure 2 – Flood mapping (source: SAPPa website)

The perimeter pavements around the buildings shall grade away from the building and as such divert any chance for overland flows to elsewhere on the site.

The above measures have been addressed in order to maintain an appropriate freeboard level higher than surrounding formed ground surfaces to enable overload flows from 1:100 ARI storm events to exit the site in an appropriate manner and so as not to affect the neighbouring properties.

STORMWATER DETENTION

The pre-development condition of the primary site has the following discharge conditions:

- Existing 10-year ARI pre-development flow = 18.0L/s

In accordance with council requirements outlined within the “general stormwater management” section of this report, a minimum 42kL of detention will be provided via underground detention tanks fitted with dual alternative submersible pumps to discharging at a maximum flow rate of 18.0L/s to the water table on Main North Road.





It is acknowledged that the site does not cater for an overland flow path to the West. To address this, and in the event of power outage or failure of one or both pumps, the following has been specified:

- Audible alarm and flashing light alarm to be visible at the rear of the property; and
- The height of the retaining walls has been increased to 200mm above the finished surface level of the playspace at the rear of the property to provide additional storage over the 245 m² of the rear portion of the playspace; providing an additional storage volume of over 30m³ for the emergency power outage situation, should a storm event occur.

QUALITY OF WATER

As outlined by City of Playford, stormwater run-off is to be treated by a gross pollutant trap prior to discharging into council system to comply with the targeted values summarised below (refer attached excerpts):

- 80% reduction of the typical urban annual load for Total Suspended Solids (TSS)
- 60% reduction of the typical urban annual load for Total Phosphorus (TP)
- 45% reduction of the typical urban annual load for Total Nitrogen (TN)
- 70% reduction of the typical urban annual load for Gross Pollutants (litter)

It is proposed that an appropriately sized SPELEcoceptor inline gross pollutant trap be provided to the carpark to treat stormwater runoff prior to discharge.

These measures will improve the quality of stormwater run-off exiting the site in comparison to current predevelopment conditions which provides no treatment.

ISSUES DURING CONSTRUCTION

The management of stormwater during construction will be under constant monitoring by the appointed builder.

The builder will be employed to maintain control measures on site and to minimise run-off from the site which may contain fine earth particles and any deleterious material that washes off site will be cleaned up by the contractor.

The principal control measure will be to install the SW detention tanks at the time of constructing the retaining walls and controlling the pump discharge to the street system accordingly during the course of construction.

Prepared by

Mia Apollonio
CPR ENGINEERS
miaa@cprengineers.com.au

Attachments:

- Excerpts from Appendix A of City of Playford's Land Division Guidelines: Stormwater Requirements
- Proposed Stormwater Management Plan
- Stormwater Calculations





c. Commercial/ Industrial Development

Within the Development boundary, outside of Councils road reserve the material requirements for the drainage system are at the designers discretion. However, Drainage between the Development boundary to Councils system is to be in accordance with Councils standard details and to the satisfaction of Council.

Discharge from the Development must meet Councils requirements for flow rate and volume as defined in Section 2 – Overarching Design Principles. It is required that:

- Discharge rate from the development should not exceed the 1:10 (10%) AEP (please note point below on downstream discharge capacity)
- The development should be able to detain the stormwater generated by a maximum 1% AEP (0.01 EY) and discharge at the designed 1:10 (10%) AEP rate.

If detention is required, the storm water should be detained wholly within the Development prior to being discharged to Councils system.

Runoff from hardstand areas within the development to Councils reserve via a vehicle crossover or path is not permitted

Given the large impervious area generated by Commercial and Industrial development, consultation with Council may be required on the capacity of the downstream system and the appropriate point of discharge.

A gross pollutant trap is required prior to discharge to Council system.

7. ENVIRONMENTAL

All stormwater flows that discharge into open channels, detention systems and existing council stormwater pipes are to be treated in order to remove gross pollutants and sediments. Typical forms of control to be used should be trash racks and gross pollutant traps.

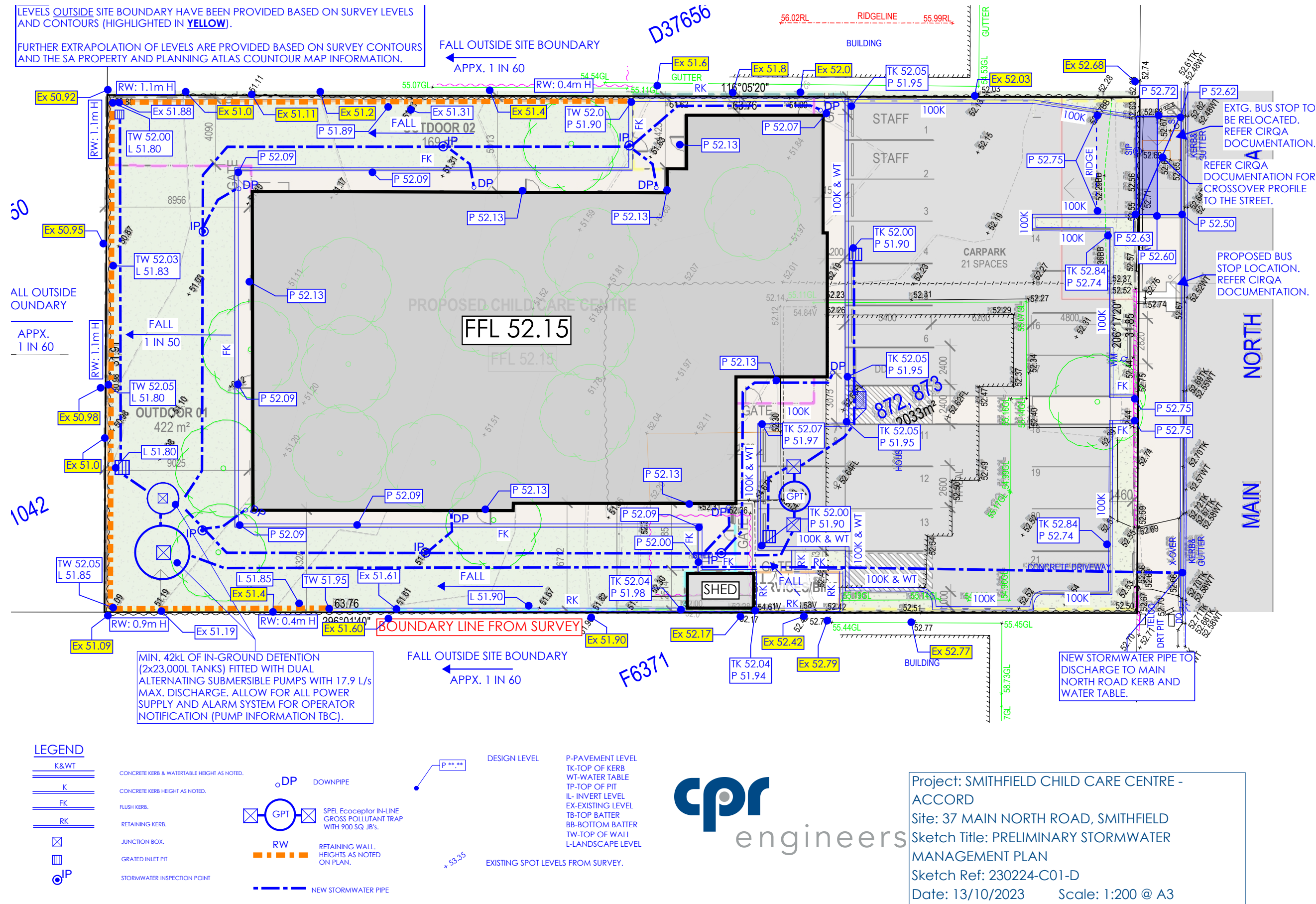
On private owned developments these systems are to be located within the property boundaries of the development and are the responsibility of the developer.

The minimum standard of treatment shall be able to remove the following percentages of pollutants from the typical annual urban load (Ref: CSIRO "Urban Stormwater Best Practice Environmental Management Guidelines"):

Suspended solids	80% reduction
Total Phosphorus	60% reduction
Total Nitrogen	45% reduction
Litter	70% reduction

All stormwater flows that discharge into open channels and detention systems are to have an end of line approved gross pollutant/ sediment trap. This is required to minimise the amount of pollutants entering the detention system. The sediment trap is to be maintained by the developer for a period of 12 months after practical completion is granted.







Job No: 230224
 Date : 13/10/23
 Design: MDA
 Page:

37 MAIN NORTH ROAD, SMITHFIELD

PRE-DEVELOPMENT - 1 IN 10 YEAR ARI FLOWS

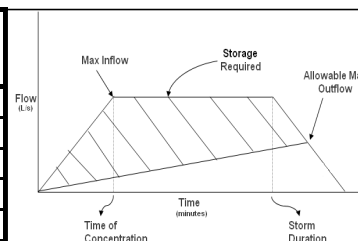
TOTAL SITE AREA = 2030 m²
 Roof Area 337 m²
 Roof Pitch 15 degrees
 Run-Off Coefficient 1

Pervious Area 1555
 Run-Off Coefficient 0.25
 Pavement Area 138
 Run-Off Coefficient 0.9

Storm Design Recurrence Interval 10 years
 Time of Concentration 10.0 minutes
 Max Allowable Outflow 18.0 L/s

Based on (AR&R 2019)

Duration (Minutes)	Intensity (mm/h)	Inflow (L/s)	Inflow Volume (m ³)	Max Storage (m ³)
5	98	24.53	7.36	
10	72	18.02	10.81	
15	59	14.77	13.29	
20	50	12.51	15.02	
25	44.4	11.11	16.67	
30	39.9	9.99	17.97	
35	36.3	9.09	19.08	
40	33.4	8.36	20.06	
50	29	7.26	21.77	
55	27	6.76	22.30	
60	25	6.26	22.52	
65	23	5.76	22.45	
70	21	5.26	22.07	
75	19	4.76	21.40	
80	17	4.25	20.42	
85	15	3.75	19.15	
90	13	3.25	17.57	



Minimum Tank Size	0.00 m ³
-------------------	---------------------

Outlet Orifice Design	
Approximate head above outlet	1 m water
Max allowable outflow	0.01802 m ³ /s
Discharge Velocity	4.43 m/s
Approx Pipe area	4068.228 mm ²
Approx Pipe Diameter	71.97 mm



Job No: 230224
 Date : 13/10/23
 Design: MDA
 Page:

37 MAIN NORTH ROAD, SMITHFIELD

POST-DEVELOPMENT - I IN 100 YEAR ARI STORM EVENT

TOTAL SITE AREA = 2030 m²

Roof Area 886 m²

Roof Pitch 11.2 degrees

Run-Off Coefficient 1

Pervious Area 537

Run-Off Coefficient 0.25

(PLAY AREAS AND LANDSCAPING)

Pavement Area 607

Run-Off Coefficient 0.9

(CARPARK)

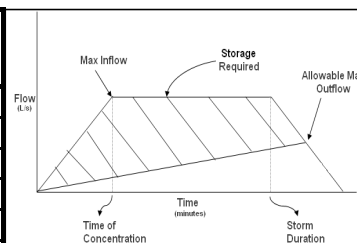
Storm Design Recurrence Interval 100 years

Time of Concentration 10.0 minutes

Max Allowable Outflow 18.0 L/s

Based on (AR&R 2019)

Duration (Minutes)	Intensity (mm/h)	Inflow (L/s)	Inflow Volume (m ³)	Max Storage (m ³)
5	186	86.08	25.82	17.71
10	136	62.94	37.76	26.95
15	110	50.91	45.82	32.30
20	94	43.50	52.20	35.98
25	82	37.95	56.92	38.00
30	73	33.78	60.81	39.19
35	66.5	30.77	64.63	40.30
40	61	28.23	67.75	40.72
50	53	24.53	73.58	41.15
55	49	22.68	74.83	39.69
60	45	20.82	74.97	37.13
65	41	18.97	74.00	33.45
70	37	17.12	71.92	28.67
75	33	15.27	68.72	22.77
80	29	13.42	64.42	15.76
85	25	11.57	59.00	7.65
90	21	9.72	52.48	-1.58

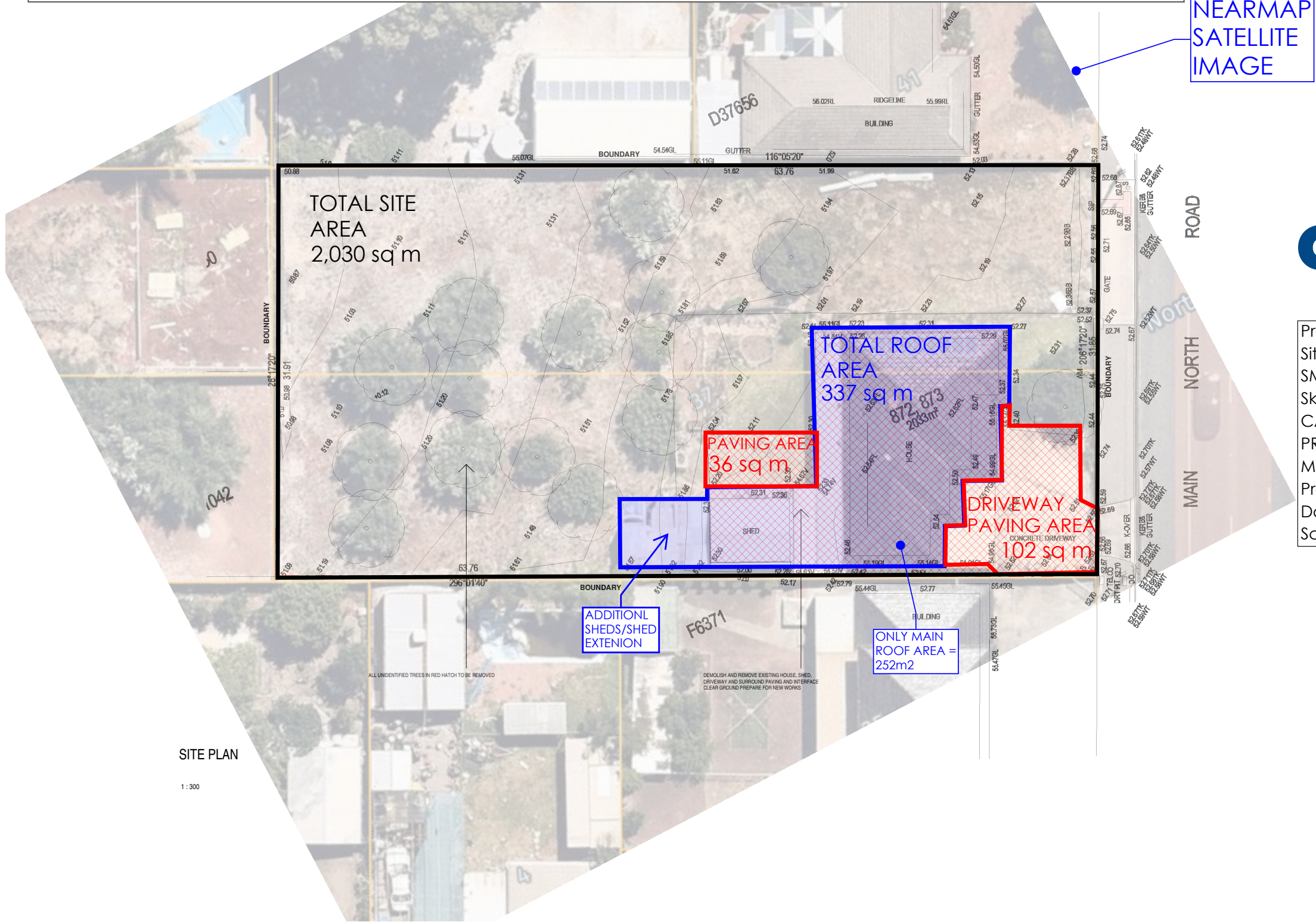


Minimum Tank Size	41.15 m ³
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Outlet Orifice Design	
Approximate head above outlet	1 m water
Max allowable outflow	0.01802 m ³ /s
Discharge Velocity	4.43 m/s
Approx Pipe area	4068.228 mm ²
Approx Pipe Diameter	71.97 mm

PRE-DEVELOPMENT SITE AREAS:
TOTAL ROOF = 337m² (MEASURED FROM SURVEY)
TOTAL PAVEMENT = 138m² (MEASURED FROM SURVEY)
TOTAL PERVIOUS = 1555m² (CALCULATED BY SUBTRACTING ROOF AND PAVING AREAS FROM TOTAL SITE AREA)

TOTAL SITE AREA = 2050m² (MEASURED 2030m² FROM SURVEY)



DA ISSUE		
ISSUED FOR DEVELOPMENT APPROVAL		
Rev	Amendment	Date
1	DA SET DRAFT	07/07/2023
2	DA SET	03/08/2023



Project: SMITHFIELD CCC
Site: 37 MAIN NORTH ROAD,
SMITHFIELD
Sketch Title: STORMWATER
CALCULATIONS -
PRE-DEVELOPMENT SITE AREA
MEASUREMENTS
Project Ref: 230224
Date: 13/10/2023
Scale: 1:200 @ A3

**BROWN
FALCONER**

28 Chesser Street, Adelaide, South Australia 5000
Telephone : 08 8203 5800 Facsimile : 08 8223 2440
ABN 65 007 848 586 brownfalconer.com.au

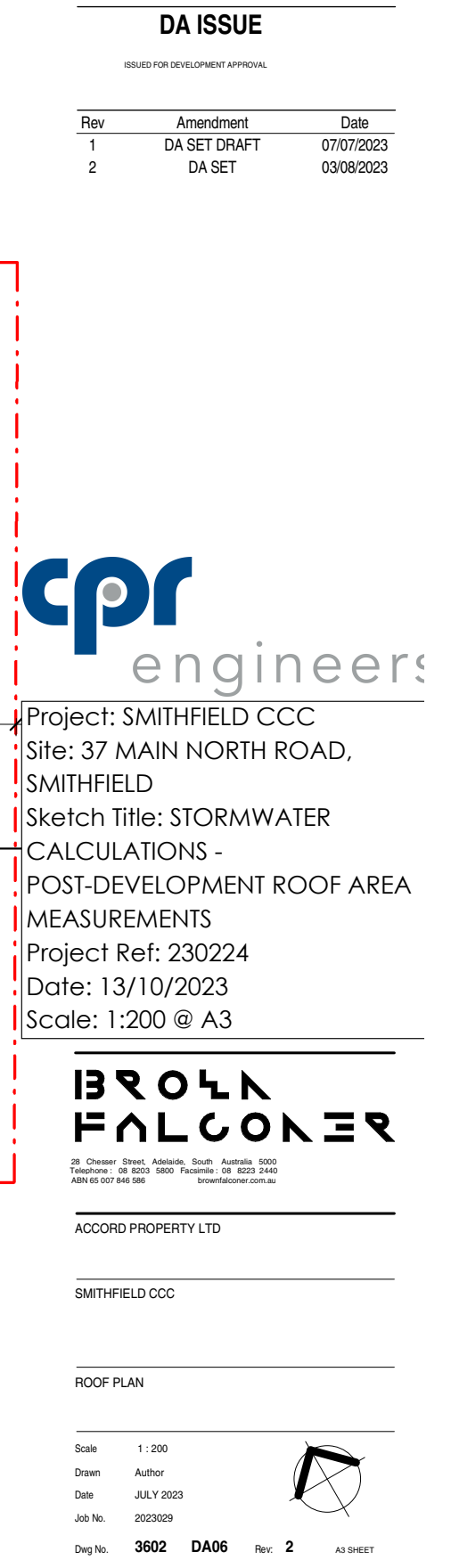
ACCORD PROPERTY LTD

SMITHFIELD CCC

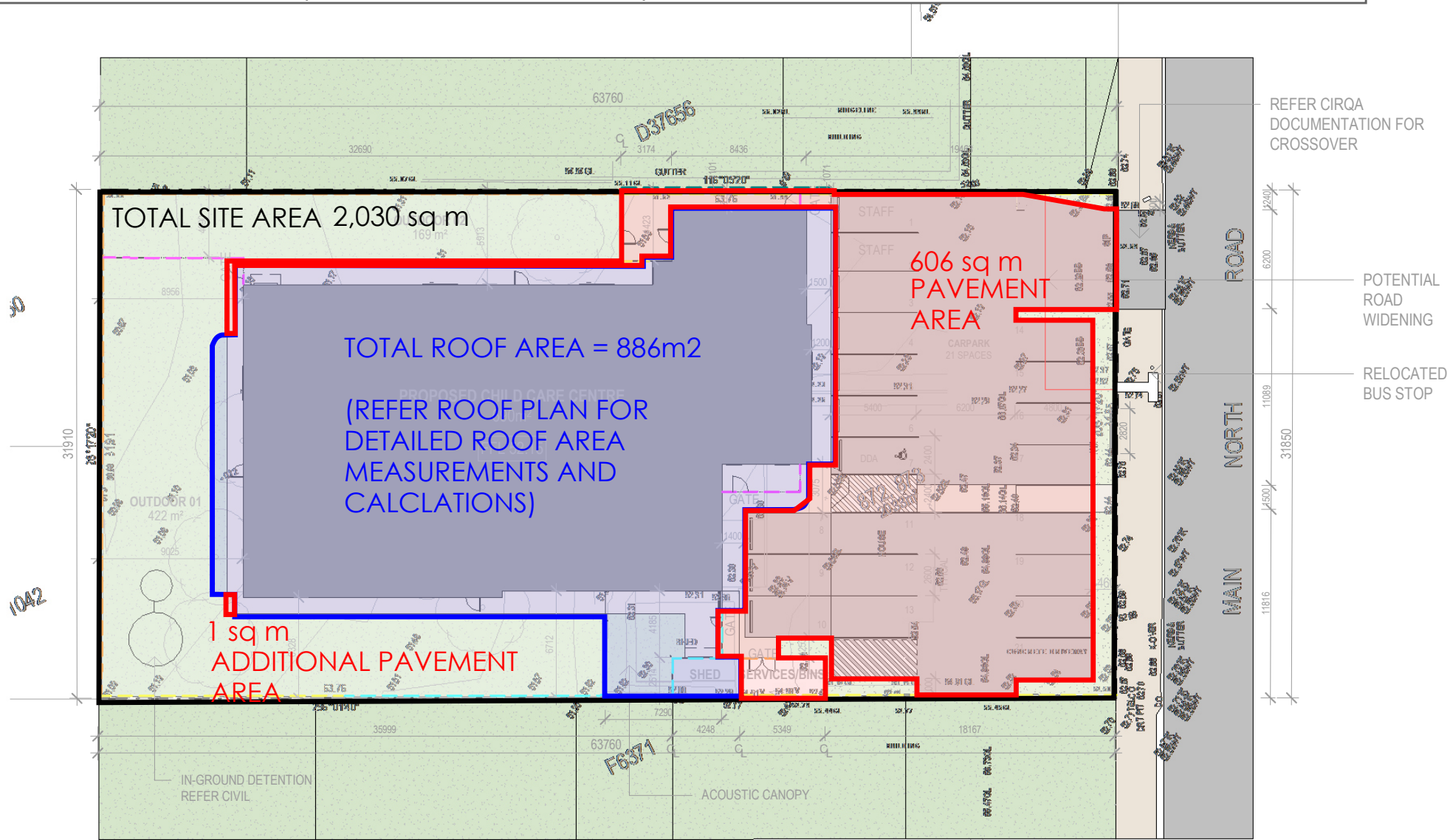
EXISTING CONDITIONS

Scale 1 : 300
Drawn Author
Date JULY 2023
Job No. 2023029
Dwg No. 3602 DA02 Rev: 2 A3 SHEET





PRE-DEVELOPMENT SITE AREAS:
TOTAL ROOF = 886m² (MEASURED FROM ARCH ROOF PLAN)
TOTAL PAVEMENT = 607m² (MEASURED FROM ARCH. SITE PLAN WITH ROOF PLAN OVERLAID)
TOTAL PERVIOUS = 837m² (CALCULATED BY SUBTRACTING ROOF AND PAVING AREAS FROM TOTAL SITE AREA)
TOTAL SITE AREA = 2030m² (MEASURED FROM SURVEY)



SITE PLAN

1 : 300

NUMBER OF PLACES

84

SITE AREA
SITE AREA PER PLACE

2026m²
24.1m²

BUILDING AREA
BUILDING AREA PER PLACE

690m²
8.2m²

OUTDOOR PLAY AREA
NUMBER OF CARPARKS

591m²
21

NOTE:
MEASURED AS 2030m²
FROM SURVEY
(CONSERVATIVE)

DA ISSUE

ISSUED FOR DEVELOPMENT APPROVAL

Rev	Amendment	Date
1	DA SET DRAFT	07/07/2023
2	DA SET	03/08/2023

FENCES

- FT01 FENCE TYPE 01
COLORBOND METAL 'GOOD NEIGHBOUR' TYPE FENCE
ON TOP OF RETAINING WALL AS REQUIRED
1800 MINIMUM HEIGHT
COLOUR: MONUMENT
- FT02 FENCE TYPE 02
COLORBOND METAL 'GOOD NEIGHBOUR' TYPE FENCE
ON TOP OF RETAINING WALL AS REQUIRED
2100 MINIMUM HEIGHT
COLOUR: MONUMENT
- FT03 FENCE TYPE 03



NOTE:
FENCES
INCLUDING
CANOPY

Project: SMITHFIELD CCC
Site: 37 MAIN NORTH ROAD,
SMITHFIELD
Sketch Title: STORMWATER
CALCULATIONS -
POST-DEVELOPMENT SITE AREA
MEASUREMENTS
Project Ref: 230224
Date: 13/10/2023
Scale: 1:200 @ A3

BROWN
FALCONER

28 Chesser Street, Adelaide, South Australia 5000
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ABN 65 007 846 586 brownfalconer.com.au

ACCORD PROPERTY LTD

SMITHFIELD CCC

SITE PLAN

Scale 1 : 300
Drawn Author
Date JULY 2023
Job No. 2023029



Dwg No. 3602 DA04 Rev: 2 A3 SHEET

Details of Representations

Application Summary

Application ID	23024476
Proposal	Child care facility with ancillary acoustic boundary fencing, retaining wall and associated advertisement
Location	37 MAIN NORTH RD SMITHFIELD SA 5114

Representations

Representor 1 - frank grillo

Name	frank grillo
Address	35 main north rd SMITHFIELD SA, 5114 Australia
Submission Date	21/09/2023 12:40 PM
Submission Source	Online
Late Submission	No
Would you like to talk to your representation at the decision-making hearing for this development?	Yes
My position is	I oppose the development

Reasons

1/ not consistent with the the current area,all the homes between kirk and queen st smithfield which consist of 5 house on the main north rd and all the homes behind the development are family homes this will sit between the homes on the main north rd off which mine is one im number 35 which is next door 2/traffic -this is the main rd so any more traffic is ridiculous i say that as since they built the on the run service station at the start of these homes it a life threatening situation pulling out of your home and driving in to your home as you get cut off and almost in a accident on a dialy basis with cars comming out of on the run this new development will only inflame this situation 3/noise -there would be a huge amount of noise from the children doesnt matter how high these so called walls are this would be like 50 dogs barking all day if you know the council laws they would do some thing about that amount of noise i will also supply a medical certificate i suffer from a proven medical condition that is caused by screaming kids which causes heightened anxiety and stress this been proved by medical studies 4-value- i have been told by a professional my value will drop by app 30 percent i built my home 40 years ago and am now retired.this development will have a huge impact on the quality of my lifestyle frank grillo

Attached Documents



UNLOCK

YOUR VISION

REF 01475-002

Memo - Response to Request for Further Information & Representation

To: Danni Biar – City of Playford

From: James Rhodes – Ekistics Planning and Design

Date: 20 October 2023

Applicant: Development Holdings Pty Ltd

Application ID: 23022869

Proposed Development: Child care facility with ancillary acoustic boundary fencing, retaining wall and associated advertisement

Subject Land: 37 Main North Road, Smithfield

Dear Danni,

We write in response to the Council Request for Further Information ('RFI') received on 26 September 2023 and the one (1) representation receiving during public notification. Our responses are provided below on behalf of the applicant, to address the matters raised in the RFI. Our responses are to be read in conjunction with the original Planning Statement prepared by Ekistics, dated 18 August 2023.

Our response is supported by the following documentation:

- **RFI Appendix 1** – Architectural Drawings including revised perspectives; and
- **RFI Appendix 2** – Revised Stormwater Management Plan, prepared by CPR Engineers.

We note the perspectives have been revised to correctly reflect tree planting locations and the northernmost staff car parking space. The plans otherwise remain unaltered.

1.1. Response to RFI

As confirmed via email dated 10 October 2023, Council advised that the traffic matter raised in the RFI (point 1) has been resolved.

Council also requested an updated stormwater management plan which reflects two matters. The first being to reflect Council's predevelopment stormwater calculations. CPR Engineers have reviewed their figures and remain confident their calculations are accurate for the 10% AEP pre-development discharge and the post-development 1% AEP discharge. To



UNLOCK

YOUR VISION

REF 01475-002

provide further clarification, the revised stormwater management plan includes site and roof plan mark-ups outlining the measurements used to inform pre-development and post-development calculations.

While stormwater management at the planning phase is always conceptual (prior to detailed design), CPR have updated their measurements to be as accurate as possible. The revised stormwater management plan reflects these minor measurement updates, which only results in a 0.1L/s increase in the pre-development (i.e. maximum allowable) discharge rate (to 18.0L/s). Subsequently, through more accurate measurements, greater stormwater discharge is permissible and the required volume of stormwater detention is reduced. On this basis, the proposal continues to effectively manage stormwater flows in accordance with Design in Urban Areas PO 42.3.

The second requested update is to identify “the site levels of the adjacent land and the future ground level within the outdoor play area”. CPR have provided the below response:

Please refer to the amended Preliminary Stormwater Management Plan (sheet 230224-C01-D in the attached report). We have done what we can to show the existing site levels outside the boundary as requested. Based on the survey levels and contours, it appears that there is a 1:60 fall from East to West, which is in agreement with the SA Property and Planning Atlas' contour map. In an effort to provide as much information as possible, the site survey has been overlaid in the Preliminary Stormwater Management Plan in a way that makes it easier to read. In addition, the site levels outside of the site boundary have been highlighted in yellow, and more have been added along with an indication of the direction of fall across the adjacent sites.

In short, additional spot levels have been nominated on the plan; including spot levels outside the site, as well as pavement and landscape levels of the outdoor play area.

1.2. Response to Representation

The application was subject to public notification between 21 September 2023 and 12 October 2023. We understand that only one representation was received, with the representor, Frank Grillo, opposing the development and wishing to be heard in support of his representation. Mr Grillo resides on neighbouring land at 35 Main North Road. Mr Grillo raised concern with the land use in a residential area, traffic generation, noise of children, and the value of his property.

In response, we highlight the General Neighbourhood Zone expressly contemplates the development of a child care facility within the zone and that the development has been designed to appropriately manage the interface with surrounding residential uses to ensure residential character and amenity is maintained.

The proposal adopts the recommended acoustic mitigation measures by qualified acoustic engineers, Sonus, to ensure that noise generated by children playing outdoors is will not exceed 50dB(A) at the closest sensitive receivers (i.e. residential allotments, including the representors property), as specified within the *Guidelines for Community Noise* published by the *World Health Organisation*. This noise level generated from the proposal is well below the maximum noise level measured under the existing condition on the site, which measures at 67 dB(A) in correlation with the typical vehicle passbys along Main North Road.



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YOUR VISION
REF 01475-002

Specialist traffic engineers, CIRQA, have confirmed that the additional vehicle movements will result in a “*minimal impact on the adjacent road network*”. In addition, the application was referred to the Department for Infrastructure and Transport (DIT) in relation to traffic, access and stormwater arrangements. We highlight that DIT raised no objection to the application and directed that seven conditions be imposed on a potential Planning Consent.

Lastly, we note that the perceived impact on ‘property value’ is not relevant to planning assessment and therefore has not been addressed as part of this response.

In our opinion, the proposed development remains aligned with the relevant provisions of the Planning and Design Code and accordingly, warrants Planning Consent, subject to reasonable and relevant Conditions.

2. Conclusion

We are confident the above responses will assist in your planning assessment and consideration of the key issues.

We note that the representor has expressed a desire to be heard before the Council Assessment Panel in support of his submission. Accordingly, we respectfully request the opportunity to also make a personal deputation to the Council Assessment Panel to address matters raised by representors, as well as answer any questions of panel members.

Subject to Council's consideration of our response to the representations, we respectfully request that the matter be presented the Council Assessment Panel meeting scheduled for 16 November 2023.

Please contact me on (08) 7231 0286 should you have any further queries in relation to this development application.

Yours Sincerely,

A handwritten signature in blue ink, appearing to read 'J Rhodes'.

James Rhodes
Planning Consultant



APPENDIX 1

Revised Architectural Plans

Brown Falconer



APPENDIX 2

Revised Stormwater Management Plan ***CPR Engineers***

Referral Snapshot

Development Application number:

23024476

Consent:

Planning Consent

Relevant authority:

City of Playford

Consent type for distribution:

Planning

Referral body:

Commissioner of Highways

Response type:

Schedule 9 (3)(7) Development Affecting Transport Routes and Corridors

Referral type:

Direction

Response date:

26 Oct 2023

Advice:

With comments, conditions and/or notes

Condition 1

All access to the development shall be gained in accordance with the Site Plan produced by Brown Falconer, drawing no. 3602 DA04, Revision 2, dated 03/08/2023; except where varied by conditions below.

Condition 2

Clear sightlines, as shown in Figure 3.3 'Minimum Sight Lines for Pedestrian Safety' in *AS/NZS 2890.1:2004*, shall be provided at the property line to ensure adequate visibility between vehicles leaving the site and pedestrians on the adjacent footpath. This will require the access to be located further to the south and all vegetation either side of the access being less than 1m in height.

Condition 3

All vehicles shall enter and exit the site in a forward direction. All on-site vehicle manoeuvring areas shall remain clear of any impediments.

Condition 4

The redundant crossover on Main North Road shall be closed and reinstated to Council's kerb and gutter standards at the applicant's expense prior to operation of the development.

Condition 5

The bus stop (including tactiles) adjacent the subject site shall be relocated to the satisfaction of DIT/SAPTA prior to the operation of the development with all costs borne by the applicant. The applicant shall contact Mr Wayne Stewart, SAPTA, Senior Project Officer on ph. 7133 2560, mobile 0401 713 311, or email. Wayne.stewart@sa.gov.au to progress this.

Condition 6

Any infrastructure within the road reserve that is demolished, altered, removed or damaged during the construction of the project shall be reinstated to the satisfaction of the relevant asset owner, with all costs being borne by the applicant.

Condition 7

Stormwater run-off shall be collected on-site and discharged without impacting the safety and integrity of the adjacent road network. Any alterations to the road drainage infrastructure required to facilitate this shall be at the applicant's cost.

Advisory Note 1

The Metropolitan Adelaide Road Widening Plan shows a possible requirement for a strip of land up to 4.5 metres in width from the Main North Road frontage of this site for future upgrading of the Main North Road/Curtis Road/Craigmore Road intersection. The consent of the Commissioner of Highways under the Metropolitan Adelaide Road Widening Plan Act is required to all building works on or within 6.0 metres of the possible requirement. As the built form does not encroach within the above areas, consent is not required in this instance.

Advisory Note 2

This property abuts a section of Main North Road that was proclaimed as controlled access road on 12 December 1960 pursuant to Part 2A of the Highways Act 1926. Departmental records show that there is a permitted means of access by which persons and vehicles may directly enter or leave the controlled access road from/to this site. Upon construction of the new access,

the applicant will need to contact the Department via dit.landusecoordination@sa.gov.au to arrange for the existing permit records to be updated.

STAFF REPORTS

MATTERS TO BE CONSIDERED BY THE COMMITTEE AND REFERRED TO COUNCIL

***Matters which cannot be
delegated to a Committee or Staff***

9.1.1 COUNCIL ASSESSMENT PANEL - TERMS OF REFERENCE

Responsible Executive Manager : Adam Squires

Report Author : Adam Squires

Delegated Authority : Matters which cannot be delegated to a Committee or Staff

Attachments : 1. Council Assessment Panel - Current (with tracked changes)
2. Council Assessment Panel - Terms of Reference (Proposed)

PURPOSE

For Council to endorse the reviewed Council Assessment Panel Terms of Reference (Attachment 2).

STAFF RECOMMENDATION

1. The Council Assessment Panel endorses the Council Assessment Panel Terms of Reference (Attachment 2).
2. The Council Assessment Panel authorises the Council Assessment Panel Assessment Manager to make further minor amendments that do not alter the intent of the Terms of Reference.

EXECUTIVE SUMMARY

The Council Assessment Panel is a relevant authority for making decisions on certain development applications assessed against the provisions of the Planning and Design Code and Council's Development Plan (for applications lodged prior to 19 March 2021).

At the Ordinary Council meeting on 29 November 2022, Council endorsed the Terms of Reference for the Council Assessment Panel (Resolution 5210).

In accordance with the current Council Assessment Panel (CAP) Terms of Reference, a review of the Terms of Reference for CAP will be undertaken annually with any amendments to be presented to Council for endorsement.

1. BACKGROUND

In accordance with the Council Assessment Panel (CAP) Terms of Reference (Attachment 1), a review of the Terms of Reference for CAP will be undertaken annually with any amendments to be presented to Council for endorsement.

The Terms of Reference have been reviewed and the proposed Terms of Reference (Attachment 2) are provided to Council for their endorsement.

2. RELEVANCE TO STRATEGIC PLAN

Decision-making filter – We will ensure that we meet our legislative requirements and legal obligations.

The Council Assessment Panel was appointed as a relevant authority under Section 82 and 83 of the *Planning, Development and Infrastructure Act 2016*. This decision will ensure the continuance of effective and efficient decision making.

3. PUBLIC CONSULTATION

There is no requirement to consult with the community on this matter.

4. DISCUSSION

- 4.1 The City of Playford CAP has been established pursuant to Section 83 of the *Planning, Development and Infrastructure Act 2016* by resolution of the Playford Council on 22 August 2017.
- 4.2 The role of CAP is:
- To act as delegate of the Council in accordance with the requirements of the *Planning, Development and Infrastructure Act 2016*.
 - As it thinks fit, to provide advice and reports to the Council on trends, issues and other matters relating to planning or development that have become apparent or arisen through its assessment of applications under the *Planning, Development and Infrastructure Act 2016*; and
 - To perform other functions (other than functions involving the formulation of policy) assigned by the Council.
- 4.3 At the CAP meeting held on 21 September 2022, the Panel endorsed their revised Operating Procedures in line with the Local Government Association of South Australia's model meeting procedures, to better reflect legislative change and vary the date of meetings to allow for the ongoing attendance of panel members (CAP510). The Council Assessment Panel Terms of Reference were amended to better align with the operating procedures and were endorsed by Council on 29 November 2022 (Resolution 5210). The meeting schedule is determined by the CAP in accordance with their CAP Operating Procedure.
- 4.4 Following consideration of the Council Assessment Panel Terms of Reference, it is intended that the Operating Procedures will be tabled for review at the December CAP meeting.
- 4.5 The amended Council Assessment Panel Terms of Reference (Attachment 2) is provided for the consideration of the Assessment Panel and will then be tabled with Council for consideration. The Terms of Reference are based on the existing Terms of Reference with some minor changes recommended.
- 4.6 A summary of the key changes to the Terms of Reference are outlined below:
- 4.6.1 Section 4 – The Role of the CAP has been amended to remove the requirement for the Panel to review their performance annually in line with Council's financial calendar year. There is no legislative requirement for an annual report to be prepared and provided to the Minister for Planning.
- 4.6.2 Section 13 – Review of Terms of Reference has been amended to align with the review of Council's Statutory and Non-Statutory Committees which occur every four years following a Local Government General Election.

5. OPTIONS

Recommendation

1. The Council Assessment Panel endorses the Council Assessment Panel Terms of Reference (Attachment 2).
2. The Council Assessment Panel authorises the Council Assessment Panel Assessment Manager to make further minor amendments that do not alter the intent of the Terms of Reference.

Option 2

1. Council endorses the Council Assessment Panel Terms of Reference (Attachment 2) with the following amendments:
 - _____
 - _____
 - _____
2. The Council Assessment Panel Assessment Manager is authorised to make further minor amendments that do not alter the intent of the Terms of Reference.



COUNCIL ASSESSMENT PANEL TERMS OF REFERENCE

1. Background

- 1.1 The City of Playford Council Assessment Panel (CAP) was appointed as a relevant authority under Section 82 and 83 of the *Planning, Development and Infrastructure Act 2016* by resolution of the City of Playford Council on 22 August 2017.

2. Approval and Change History

Version	Approval Date	Approved By	Review By	Change
1	22 August 2017	Ordinary Council	Senior Manager – Development Services	New Legislation
2	28 November 2017	Ordinary Council	Senior Manager – Development Services	Updates Required
2.1	25 May 2021	Ordinary Council	CAP Assessment Manager	Insurance and Remuneration Clauses added
3	29 November 2022	Ordinary Council	CAP Assessment Manager	Terms of Reference updated to reflect the Local Government of South Australia's modelling for Panel terms of reference
4	28 November 2023	Ordinary Council	CAP Assessment Manager	Annual Review

3. Definitions

The following definitions apply:

CAP means the City of Playford Council Assessment Panel

The Act means the *Planning, Development and Infrastructure Act (2016)*

The Regulations means the *Planning, Development and Infrastructure (General) Regulations (2017)*

Development Plan means the City of Playford Development Plan

Staff includes Council staff, contractors, volunteers and all others who perform work on behalf of Council

4. The Role of the CAP

- 4.1 The role of the CAP is:

- 4.1.1 to act as delegate of the Council in accordance with the requirements of the Act;

- 4.1.2 as it thinks fit, to provide advice and reports to the Council on trends, issues and other matters relating to planning or development that have become apparent or arisen through its assessment of applications under the Act; and
- 4.1.3 to perform other functions (other than functions involving the formulation of policy) assigned to it by Council.

5. The Role of Council Administration

- 5.1 The Council will provide an Assessment Manager for the CAP. The role of the Assessment Manager is to:
 - 5.1.1 act as a relevant authority as provided under the Act;
 - 5.1.2 be responsible for managing the staff and operations of the CAP; and
 - 5.1.3 provide advice to the CAP as appropriate
- 5.2 Council administration may provide advice and further clarification of issues during the meeting upon request of a CAP Member through the Presiding Member.
- 5.3 Council staff are not Members of the CAP and do not have voting rights. Their role is to provide professional advice to assist the CAP to make decisions based on the provisions of the City of Playford's Development Plan.
- 5.4 The Council is responsible for any costs, expenses or liabilities arising in relation to the activities of the CAP:
 - 5.4.1 Whilst acting in their capacity as an Independent Member, for any honest act or omission, Members will be provided indemnity for civil liability claims under the Local Government Association Mutual Liability Scheme.
 - 5.4.2 Whilst acting in their capacity as an Independent Member, personal accident coverage will be provided to Independent Members. Note that under the *Private Health Act 2007* and the rules attached to that Act, the personal accident policy may not cover medical expenses and Independent Members are responsible for their own medical expenses.

Deleted: <#>The CAP will review its performance annually in line with Council's financial calendar year.¶
 <#>The Annual Report may include information identifying:¶
 <#>the number of meetings held;¶
 <#>the attendance record of Members;¶
 <#>the number and nature of application for Development Plan Consent that were considered (including the number of confidential items considered);¶
 <#>advice in respect of any trends, issue or other matters that have become apparent or arisen through the CAP's assessment applications for Development Plan Consent; and¶
 <#>the number of decisions of the CAP that were appealed to the Environment, Resources and Development Court.¶
 <#>The Annual Report will be forwarded to Members of the Council, CAP Members and the Minister for Planning for information.¶

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6. Membership of CAP

Appointment of Members

- 6.1 The CAP will consist of 5 members to be appointed by the Council, comprising:
 - 6.1.1 one Member of the Council (Council Member)
 - 6.1.2 four Independent Members, not being Members of the Council or State Parliament
- 6.2 The Council may determine that the CAP will be constituted by a different number of members for different classes of development, in which case the relevant details will be specified by the Council.

- 6.3** When appointing CAP Members, the Council must have regard to the following:
- 6.3.1 the candidate's knowledge of the operation and requirements of the Act and, during transition to the Act, the *Development Act 1993*;
 - 6.3.2 in relation to Independent Members, the candidate's qualifications or experience in a field that is relevant to the activities of the CAP;
 - 6.3.3 in relation to the Council Member, the candidate's experience in local government;
 - 6.3.4 that a balance of qualifications and experience among CAP Members is desirable
 - 6.3.5 that gender diversity among CAP Members is desirable; and
 - 6.3.6 such other matters as the Council considers relevant.

Appointment of Deputy Members

- 6.4** The Council may appoint at least one Deputy Member to the CAP for the purpose of filling in for a CAP Member who is unable to attend a CAP meeting or part of a CAP meeting.
- 6.5** Subject to clause 6.6, a Deputy Member must not be a Member of the Council or State Parliament.
- 6.6** Where a Deputy Member appointed for the Council Member is also a member of the Council, that person may not act as a deputy for any other CAP Member (whereas a Deputy Member who is not a member of the Council may act as a deputy for any CAP Member).
- 6.7** Where more than one Deputy Member is appointed, the Council must specify the circumstances in which each Deputy Member, ~~(any one or more of them)~~ will be invited to attend a CAP meeting.
- 6.8** In appointing a Deputy Member, the Council must have regard to the matters in clause 6.3, as well as to the qualifications and experience of the CAP Member or CAP Members to whom the candidate will be a deputy.
- 6.9** Unless the context otherwise requests, a reference to a CAP Member in this document includes a Deputy Member.

Expression of Interest

- 6.10** Subject to clause 6.11, the Council will call for expressions of interest for appointment of CAP Members.
- 6.11** At the expiration of a term of appointment, an existing member is eligible for reappointment.

Presiding Member and Acting Presiding Member

- 6.12** The Council will appoint an Independent Member to be the Presiding Member of the CAP for such term and on such conditions as determined by the Council.

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6.13 The Presiding Member will preside at any CAP meeting at which he or she is present.

6.14 In the event that the Presiding Member is not present at a meeting (or part thereof), an Acting Presiding Member will be appointed by those CAP Members who are present at the meeting.

6.15 A Presiding Member is eligible to be reappointed as the Presiding Member at the expiry of his or her term of office as Presiding Member.

6.16 In the event that the Presiding Member resigns or is removed from office, the Council will appoint an Independent Member to be the Presiding Member for such term and on such conditions as determined by the Council.

Term of Appointment

6.17 Subject to clause 9, Independent Members will be appointed for a term of up to 2 years and on any such other conditions as determined by the Council.

6.18 Subject to clause 9, the Council Member will be appointed for a term of up to 2 years and on any such other conditions as determined by the Council.

6.19 Deputy Members will be appointed for a term of up to 2 years and on any such other conditions as determined by the Council.

6.20 A CAP Member is eligible for reappointment for a further term, or further terms, upon the expiry of his or her current term.

6.21 A CAP Member whose term of office has expired may nevertheless continue to act as a Member until the vacancy is filled or for a period of six months from the expiry of the Member's term of office, whichever occurs first.

7. Vacancy in Membership

7.1 In the event of a vacancy arising in the office of a CAP Member, the Council may appoint a person to be a CAP Member for the balance of the original CAP Member's term of office as soon as is reasonably practicable.

7.2 The CAP Member appointed to fill a vacancy may be a Deputy Member in which case that person will automatically cease to be a Deputy Member.

7.3 In appointing a CAP Member pursuant to clause 7.1, the Council may have regard to the matters in clause 6.3 and 6.8 as the case requires.

7.4 A vacancy in the membership of the CAP will not invalidate any decisions of the CAP, provided a quorum is maintained during meetings.

7.4.1 A quorum at a meeting of the CAP is a number ascertained by dividing the total number of Members of the CAP by 2, ignoring any fraction resulting from the division, and adding 1.

8. Conditions of Appointment

8.1 Independent Members will receive a sitting fee for each meeting, the fee (exclusive of GST) is prescribed by Council.

8.2 Independent Members who are required to attend an Ordinary Council meeting, other Committee meetings or mandatory training are entitled to 50% of the sitting fee.

8.3 The Presiding Member is entitled to 125% of the sitting fee.

Sitting fee	50 %	125 %
\$ 450	\$ 225	\$ 563

8.4 Under Section 83(1)(c) of the Act:

8.4.1 CAP Members, excluding a Member who is a Member or former Member of the Council, must be accredited professionals – planning level 2 under the Act; and

8.4.2 CAP Members who are Members or former Members of the Council must have sufficient experience in local government to satisfy the Council that they are appropriately qualified to act as a Member of the CAP.

8.5 Independent Members may be offered to undertake training relevant to their role on the Committee. This may include mandatory training required of Council Members and Council and Committee Meeting Procedure training.

9. Removal from Office

9.1 A CAP Member will automatically lose office where:

9.1.1 the CAP Member has become bankrupt or has applied to take the benefit of a law for the relief of insolvent debtors;

9.1.2 the CAP Member has been convicted of an indictable offence punishable by imprisonment;

9.1.3 in the case of a Council Member, the Member ceases to be a member of the Council.

9.2 Subject to Clause 9.4, the Council may by resolution remove a CAP Member from office where, in the opinion of the Council, the behaviour of the CAP Member amounts to:

9.2.1 a breach of a condition of his or her appointment as a CAP Member;

9.2.2 misconduct;

9.2.3 a breach of any legislative obligation or duty of a CAP Member;

9.2.4 neglect of duty in attending to role and responsibilities as a CAP Member;

9.2.5 a failure to carry out satisfactorily the duties of his or her office;

9.2.6 a breach of fiduciary duty that arises by virtue of his or her office;

9.2.7 inability to carry out satisfactorily the duties of his or her office.

- 9.2.8 except in relation to Deputy Members, a failure without reasonable excuse to attend three consecutive CAP meetings without the CAP previously having resolved to grant a leave of absence to the CAP Member;
 - 9.2.9 in relation to a Deputy Member, a failure without reasonable excuse on three consecutive occasions to attend a meeting of the CAP when requested to do so; or
 - 9.2.10 for any other reason the Council considers appropriate.
- 9.3** The removal of the CAP Member pursuant to clause 9.2 will take effect upon the Council passing a resolution to remove the CAP Member from office (unless the Council resolves otherwise), and such resolution will be confirmed in writing to the CAP Member within 7 days of being passed.
- 9.4** Prior to resolving to remove a CAP Member from office pursuant to clause 9.2, the Council must:
- 9.4.1 give written notice to the CAP Member of:
 - 9.4.1.1 its intention to remove the CAP Member from office pursuant to clause 9.2; and
 - 9.4.1.2 the alleged behaviour of the CAP Member falling within clause 9.2.2 or reason the Council considers it appropriate to remove the CAP Member, not less than 7 days before the meeting of the Council at which the matter is to be considered;
 - 9.4.2 give the CAP Member an opportunity to make submissions to the Council on its intention to remove the CAP Member from office either orally at the Council meeting at which the matter is to be considered, or in writing by such date as the Council reasonably determines; and
 - 9.4.3 have due regard to the CAP Member's submission in determining whether to remove the CAP Member from office.

10. Disclosure of Interests

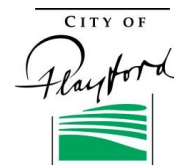
- 10.1** A Member of the CAP who has a direct or indirect personal or pecuniary interest in a matter before the CAP (other than an indirect interest that exists in common with a substantial class or persons):
- 10.1.1 must, as soon as he or she becomes aware of his or her interest, disclose the nature and extent of the interest to the CAP;
 - 10.1.2 must not take part in any hearings conducted by the CAP, or in any deliberations or decision of the CAP, on the matter and must be absent from the meeting when any deliberations are taking place or a decision is being made.
- 10.2** A Member of the CAP will be taken to have an interest in a matter if an associate of the Member has an interest in the matter.

- 10.3 Independent Members must provide a return in accordance with Schedule 1 of the *Planning, Development and Infrastructure Act 1996*.
11. **Meetings, Agendas and Minutes**
- 11.1 Meetings of the City of Playford Council Assessment Panel are determined by the CAP inline with the Council Assessment Panel Operating Procedure. Changes to the scheduling of CAP meetings will be determined by the Assessment Manager in consultation with the Presiding Member.
- 11.2 Members of the public are able to access agendas and minutes of meetings of the CAP on Council's website at www.playford.sa.gov.au.
- 11.3 The CAP may, before it releases a copy of any minutes, exclude from the minutes information about any matter dealt with on a confidential basis.
12. **Review of CAP Decisions**
- 12.1 CAP Decisions are final and shall not be referred to Council for further consideration.
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13. **Review of Terms of Reference**
- 13.1 A review of the Terms of Reference for CAP will be undertaken ~~every four years in line with the Local Government General Elections~~ with any amendments to be presented to Council for endorsement.
14. **Supporting Documentation**
- 14.1 The following documents support this Terms of Reference:
- Schedule 3 (Codes of conduct and professional standards) of the *Planning, Development and Infrastructure Act 2016*
15. **Document Control**
- 15.1 Approved by Ordinary Council Resolution ~~TBA~~ on ~~28 November 2023~~.
- 15.2 Before using a printed copy, please verify it is the current document.

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COUNCIL ASSESSMENT PANEL TERMS OF REFERENCE

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- 6.17 Subject to clause 9, Independent Members will be appointed for a term of up to 2 years and on any such other conditions as determined by the Council.
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- 6.19 Deputy Members will be appointed for a term of up to 2 years and on any such other conditions as determined by the Council.
- 6.20 A CAP Member is eligible for reappointment for a further term, or further terms, upon the expiry of his or her current term.
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8.3 The Presiding Member is entitled to 125% of the sitting fee.

Sitting fee	50 %	125 %
\$ 450	\$ 225	\$ 563

8.4 Under Section 83(1)(c) of the Act:

8.4.1 CAP Members, excluding a Member who is a Member or former Member of the Council, must be accredited professionals – planning level 2 under the Act; and

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9.2.3 a breach of any legislative obligation or duty of a CAP Member;

9.2.4 neglect of duty in attending to role and responsibilities as a CAP Member;

9.2.5 a failure to carry out satisfactorily the duties of his or her office;

9.2.6 a breach of fiduciary duty that arises by virtue of his or her office;

9.2.7 inability to carry out satisfactorily the duties of his or her officer.

- 9.2.8 except in relation to Deputy Members, a failure without reasonable excuse to attend three consecutive CAP meetings without the CAP previously having resolved to grant a leave of absence to the CAP Member;
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- 9.4.1 give written notice to the CAP Member of:
 - 9.4.1.1 its intention to remove the CAP Member from office pursuant to clause 9.2; and
 - 9.4.1.2 the alleged behaviour of the CAP Member falling within clause 9.2.2 or reason the Council considers it appropriate to remove the CAP Member, not less than 7 days before the meeting of the Council at which the matter is to be considered;
 - 9.4.2 give the CAP Member an opportunity to make submissions to the Council on its intention to remove the CAP Member from office either orally at the Council meeting at which the matter is to be considered, or in writing by such date as the Council reasonably determines; and
 - 9.4.3 have due regard to the CAP Member's submission in determining whether to remove the CAP Member from office.

10. Disclosure of Interests

- 10.1** A Member of the CAP who has a direct or indirect personal or pecuniary interest in a matter before the CAP (other than an indirect interest that exists in common with a substantial class or persons):
- 10.1.1 must, as soon as he or she becomes aware of his or her interest, disclose the nature and extent of the interest to the CAP;
 - 10.1.2 must not take part in any hearings conducted by the CAP, or in any deliberations or decision of the CAP, on the matter and must be absent from the meeting when any deliberations are taking place or a decision is being made.
- 10.2** A Member of the CAP will be taken to have an interest in a matter if an associate of the Member has an interest in the matter.

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11. Meetings, Agendas and Minutes

- 11.1 Meetings of the City of Playford Council Assessment Panel are determined by the CAP inline with the Council Assessment Panel Operating Procedure. Changes to the scheduling of CAP meetings will be determined by the Assessment Manager in consultation with the Presiding Member.
- 11.2 Members of the public are able to access agendas and minutes of meetings of the CAP on Council's website at www.playford.sa.gov.au.
- 11.3 The CAP may, before it releases a copy of any minutes, exclude from the minutes information about any matter dealt with on a confidential basis.

12. Review of CAP Decisions

- 12.1 CAP Decisions are final and shall not be referred to Council for further consideration.
- 12.2 In circumstances prescribed by the Act, an applicant or third party who is dissatisfied with the CAP's decision may appeal to the judicial system of the State.

13. Review of Terms of Reference

- 13.1 A review of the Terms of Reference for CAP will be undertaken every four years in line with the Local Government General Elections with any amendments to be presented to Council for endorsement.

14. Supporting Documentation

- 14.1 The following documents support this Terms of Reference:
- Schedule 3 (Codes of conduct and professional standards) of the *Planning, Development and Infrastructure Act 2016*

15. Document Control

- 15.1 Approved by Ordinary Council Resolution TBA on 28 November 2023.
- 15.2 Before using a printed copy, please verify it is the current document.