



NOTICE

of

COUNCIL ASSESSMENT PANEL MEETING

Pursuant to the provisions of Section 82 of the Planning, Development and Infrastructure Act 2016

TO BE HELD IN

**COUNCIL CHAMBERS
PLAYFORD CIVIC CENTRE
10 PLAYFORD BOULEVARD, ELIZABETH**

MEMBERS MAY PARTICIPATE BY ELECTRONIC MEANS

ON

THURSDAY, 18 DECEMBER 2025 AT 6:00 PM

THIS MEETING WILL ALSO BE VIEWABLE AT
<https://www.youtube.com/user/CityOfPlayford>

**MATT DINEEN
SENIOR MANAGER DEVELOPMENT SERVICES**

Issue Date: Thursday, 11 December 2025

MEMBERSHIP

MR GEOFF PARSONS – PRESIDING MEMBER

Mr Aaron Curtis

Ms Cherie Gill (Deputy)

Mr Paul Mickan

Ms Misty Norris

Ms Tanya Smiljanic (Deputy)

Mr Adam Squires

**City of Playford
Council Assessment Panel Meeting**

AGENDA
THURSDAY, 18 DECEMBER 2025 AT 6:00 PM

ACKNOWLEDGEMENT OF COUNTRY

We would like to acknowledge that this land we meet on today is the traditional land of the Kaurna people, and that we respect their spiritual relationship with their country. The City of Playford would also like to pay respects to Elders past, present and emerging.

1 ATTENDANCE RECORD

- 1.1 Present
- 1.2 Apologies
- 1.3 Not Present

2 CONFIRMATION OF MINUTES

RECOMMENDATION

The Minutes of the Council Assessment Panel Meeting held 20 November 2025 be confirmed as a true and accurate record of proceedings.

3 APPLICATIONS WITHDRAWN

4 DECLARATIONS OF INTEREST

5 APPLICATIONS FOR CONSIDERATION – PERSONS WISHING TO BE HEARD

- 5.1 25030642 - Lot 101 Ashfield Road, Elizabeth - Variation to DA 23022101 - Amendments to the two-storey building encompassing a mix of indoor recreation facilities, decrease in building height by 3.1 metres, removal of basement car parking, revised car parking layout, amended landscaping, advertisement and revised tree damaging activity in the form of the removal of regulated and significant trees (Attachments)6

**Representors
to be heard:** Kate Isterling
William Isterling
Conrad William Isterling

Applicant: Pelligra Group C/- Future Urban

6 APPLICATIONS FOR CONSIDERATION – NO PERSONS TO BE HEARD

Nil

7 APPLICATIONS FOR CONSIDERATION - CATEGORY 1

Nil

8 OUTSTANDING MATTERS – APPEALS AND DEFERRED ITEMS

Nil

9 OTHER BUSINESS

9.1 STAFF REPORTS

Nil

10 CONFIDENTIAL MATTERS

Nil

11 POLICY DISCUSSION FORUM

Nil

12 CLOSURE

APPLICATIONS FOR CONSIDERATION

APPLICATIONS FOR CONSIDERATION – PERSONS WISHING TO BE HEARD

5.1 25030642 - LOT 101 ASHFIELD ROAD, ELIZABETH - VARIATION TO DA 23022101 - AMENDMENTS TO THE TWO-STOREY BUILDING ENCOMPASSING A MIX OF INDOOR RECREATION FACILITIES, DECREASE IN BUILDING HEIGHT BY 3.1 METRES, REMOVAL OF BASEMENT CAR PARKING, REVISED CAR PARKING LAYOUT, AMENDED LANDSCAPING, ADVERTISEMENT AND REVISED TREE DAMAGING ACTIVITY IN THE FORM OF THE REMOVAL OF REGULATED AND SIGNIFICANT TREES

Author: Mahmoud Hasaneen

Proposal: Variation to DA 23022101 – Amendments to the two-storey building encompassing a mix of indoor recreation facilities, decrease in building height by 3.1 metres, removal of basement car parking, revised car parking layout, amended landscaping, advertisement and revised tree damaging activity in the form of the removal regulated and significant trees

Development Number: 25030642

Date of Lodgement: 30 September 2025

Owner: City of Playford

Applicant: Pelligra Group C/- Future Urban

Location: Lot 101 Ashfield Road, Elizabeth SA 5112

Zone: Urban Activity Centre Zone / General Neighbourhood Zone

Classification: Code Assessed – Performance Assessed

Public Notification Category: Yes

Representation Received: Yes

Request for Additional Information Made? Yes

Recommendation: To Grant Planning Consent – Subject to Conditions

Attachments:

1 Download	Planning Statement
2 Download	Architectural & Landscaping Plans
3 Download	Traffic Report & Waste Management Plan
4 Download	Stormwater Management Report
5 Download	Arboricultural Impact Assessment Report
6 Download	Environment Noise Assessment Report
7 Download	Combined Representations Received
8 Download	Response to Representations
9 Download	Council Assessment Panel Report 18 April 2024
10 Download	Council Assessment Panel Minutes 18 April 2024

1. The Subject Land

The subject land, commonly referenced as Lot 101 Ashfield Road, Elizabeth is a single allotment as per below:

- Allotment 101 Deposited 133867 and contained within Certificate of Title Volume 6305 Folio 58.

The subject land is regular in shape with a primary street frontage of approximately 144 metres to Ashfield Road, Elizabeth and secondary street frontages of approximately 75.5 metres to Philip Highway to the north and approximately 135 metres to Main North Road to the east. The site area of the subject land is 11,298m² in size.

The subject land contains established vegetation and trees located throughout the site without any physical structures located on the land. No Land Management Agreements or encumbrances are registered on the Certificate of Title. However, a high voltage underground cable traverses the land (identified within South Australia Property and Planning Atlas (SAPPA)) which is to be relocated as part of the development.

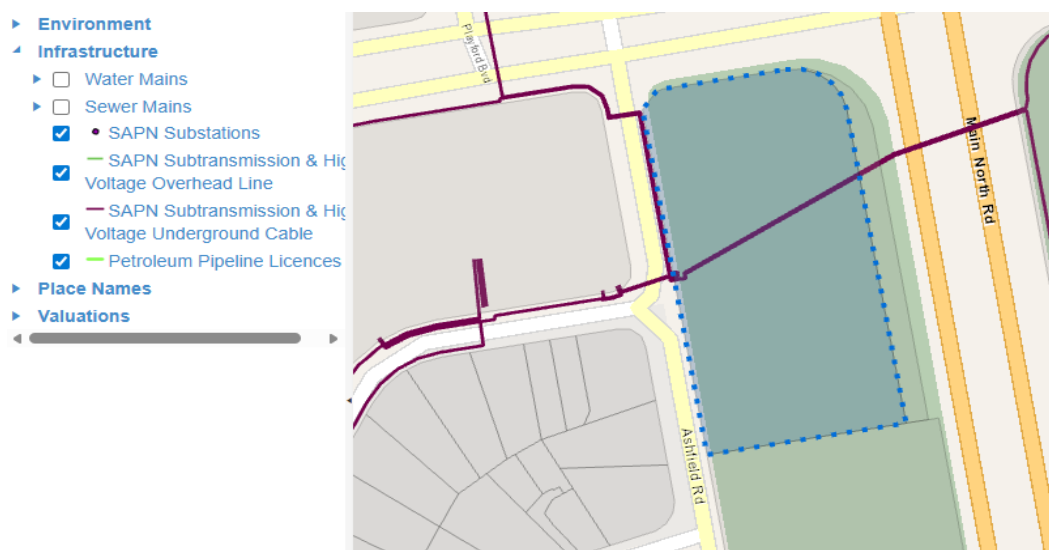


Figure 1: SAPPA screenshot with high voltage underground cable traversing the subject land

The northern portion of the land is within the Urban Activity Centre Zone, and the southern portion of the land is within the General Neighbourhood Zone of the Planning and Design Code (the Code).

2. The Locality

The locality accommodates a range of retail and commercial land uses, established residential development and passive recreational areas.

To the northwest of the locality a mix of retail and commercial activities, including bulky goods outlets, supermarkets and other speciality shops of the Elizabeth Shopping Centre. To the north of the subject land are Council civic buildings. Together, these form a part of the wider Elizabeth City Centre.

Established residential areas primarily containing single storey detached dwellings on traditional residential allotments are located to the southwest and to the east of the locality.

Fremont Park is located to the northeastern corner of the locality that provides passive recreational activities for the wider community.

The intersection of Philip Highway and Main North Road is a prominent feature to the northern portion of the locality. Main North Road and Philip Highway are both classified as State maintained roads and are managed by the Department for Infrastructure and Transport (DIT).

2.1 Locality Plans



Figure 2: Subject site outlined in yellow, and locality shown in blue line



Figure 3: Street view of subject land and surrounding context (looking south from Philip Highway)



Figure 4: Street view of subject land and surrounding context (looking west from Main North Road)



Figure 5: Street view of subject land and surrounding context (looking north from Ashfield Road)

2.2 Zoning

The subject land is located within the Urban Activity Centre Zone and the General Neighbourhood Zone as identified in the Code. The following Overlays and Technical and Numerical Variations (TNVs) also apply:

Overlays:

- Advertising Near Signalised Intersections
- Building Near Airfields
- Defence Aviation Area (All structures over 15 metres)
- Future Road Widening
- Hazards (Flooding - General)
- Major Urban Transport Routes
- Prescribed Wells Area
- Regulated and Significant tree
- Stormwater Management
- Traffic Generating Development
- Urban Tree Canopy.

TNVs

- Concept Plan – 81 - Edinburgh Defence Airfield Lighting Constraints

2.3 Zone Map

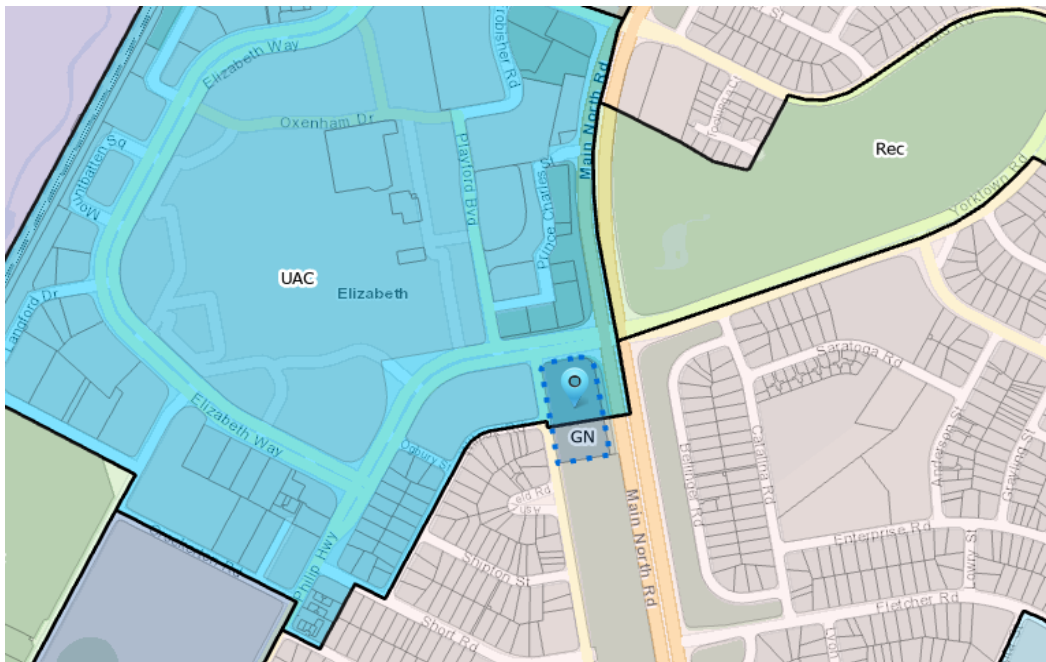


Figure 6: Subject site within the Urban Activity Centre Zone and General Neighbourhood Zone, with proximity to the Recreation Zone

3. Background

This application seeks to vary Development Application No. 23022101, which involved a proposal for a mixed-use development encompassing an indoor recreation facility, retail outlet, café, tree damaging activity, with associated parking and landscaping at Lot 1259 Ashfield Road, Elizabeth. The below figure shows the subject site in relation to the subject land for DA 23022101.



Figure 7: Showing the subject site and subject land from DA 23022101

The subject site is now legally identified as Lot 101 Ashfield Road, Elizabeth. This designation occurred through Land Division Application 292/D030/14, which resulted in the creation of Lot 101.

An extract from the approved land division (Figure 8) is provided below, illustrating the formation of Lot 101.

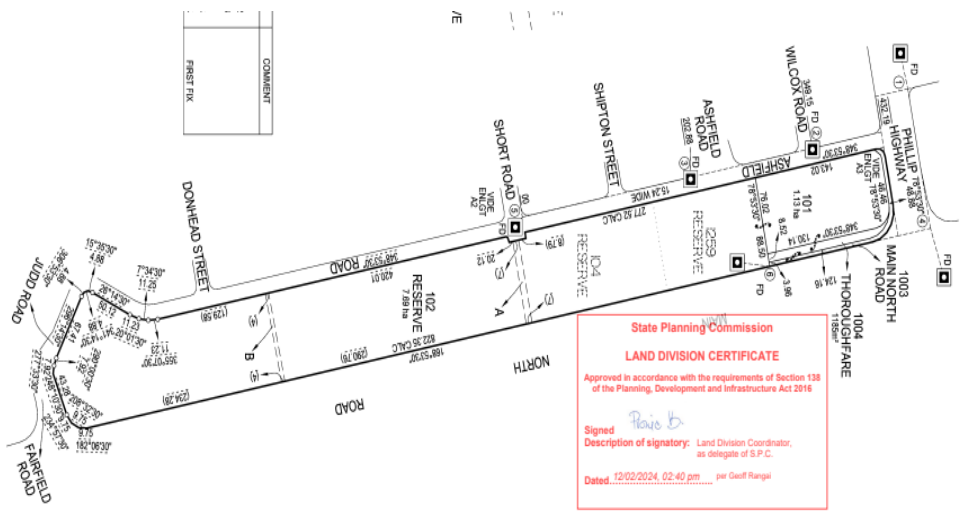


Figure 8: Extract of DA 292/D030/14 can creation of Lot 101

It should be noted that the subject site of the previous application remains the same extent as the current variation application, and this is now wholly reflective of the created allotment of Lot 101 Ashfield Road, Elizabeth.

The City of Playford Council Assessment Panel (CAP) at its meeting on 18 April 2024 resolved (Resolution CAP552) to grant planning consent for a two-level (plus basement) mixed-use building incorporating indoor recreation facilities, shops, basement and ground level car parking, landscaping and tree damaging activity in the form of the removal of 13 regulated trees and three (3) significant trees subject to a number of

conditions (DA 23022101 - see Attachment 10: Council Assessment Panel Minutes 18 April 2024).

Council staff have considered this variation request and resolved that the variation is not minor and is to be assessed as a variation to the original application.

4. The Proposal

Future Urban on behalf of Pelligra Group seeks Planning Consent for the variation to DA 23022101 which incorporates amendments to the two-storey building encompassing a mix of indoor recreation facilities, decrease in building height by 3.1 metres, removal of basement car parking, revised car parking layout, amended landscaping, advertisement and revised tree damaging activity in the form of regulated and significant trees.

The key elements of the two-storey building include a mix of indoor recreation facilities as follows:

- 3,307m² Ice Arena (identified as Ice Rink within ground floor plan)
- 2,535m² Gymnasium (identified as Revo Fitness within ground floor plan)
- 1,500m² for the purposes of indoor recreation facility, with the tenancy yet to be confirmed (identified as Tenancy 2 within ground floor plan).

The key changes (as part of this proposal) from the original application (23022101) are as follows:

- Removal of basement car parking, basketball courts, rock climbing gym, children's play centre (Funtopia), shop/restaurant and forecourt dining area
- Building height decreased from 16.2 metres to 13.1 metres
- Revised car parking layout and provision
- Increased building setback from Ashfield Road, Elizabeth
- Updated landscaping plan including further internal landscaping within car parking area
- Inclusion of an advertisement associated with the development
- Simplified and scaled-back architectural design
- Revised tree damaging activity.

Overall, the original development has been significantly scaled back. Resulting in a more consistent design and a refined elevation that presents well to the Ashfield Road streetscape.

A copy of the proposal plans and supporting documentation are contained in attachments 2 - 6.

5. Procedural Matters

5.1 Classification

Section 128 of the *Planning, Development & Infrastructure Act 2016* (the Act) establishes that a person may seek the variation of a development authorisation previously given under the Act including by seeking the variation of a condition imposed with respect to the development authorisation.

A variation application may only be made if the relevant authorisation is still operative and cannot seek to extend the period for which the authorisation remains operative.

As the development was granted Planning Consent on 18 April 2024 (Resolution CAP552), the authorisation is operative until 18 April 2026 (2 years). Until this time, the Applicant is entitled to seek approval of a variation request.

The variation proposal comprising the previously listed changes (see Section 4 of this report) from the original proposal, have been assessed using the Performance Assessment pathway in accordance with Section 106 of the Act.

The previously listed elements (see Section 4 of this report) captured as part of the proposed development are not classified as an Accepted, Deemed-to-Satisfy or Restricted development within the relevant Tables of the Zone. The proposed development is, therefore, a Code Assessed – Performance Assessed development pursuant to Section 105(b) and Section 107 of the Act, requiring an on-merit assessment against the relevant provisions of the Code.

5.2 Public Notification

All classes of performance assessed developments require public notification unless, pursuant to Section 107(6) of the Act, the class of development is excluded from notification by the Code in Table 5 – Procedural Matters (PM) – Notification of the relevant Zone(s).

Table 5 of the Zone(s) identifies classes of development that are exempt from public notification. In this instance, the proposed development is not a form of development listed in Column A of Table 5 within the General Neighbourhood Zone. Additionally, within the Urban Activity Centre Zone the site is adjacent to land used for residential purposes within a neighbourhood-type Zone. Accordingly, the proposal does not constitute an excluded form of development under Table 5 of the Urban Activity Centre Zone.

Furthermore, in the opinion of the relevant authority, it is considered that the proposed development is not minor in nature. As such, public notification is required.

Public notification commenced on 31 October 2025 and concluded on 20 November 2025.

Eight (8) representations were received during the public notification period. It must be noted that one (1) representation was received in-person over the Council counter and one (1) representation was received as a verbal objection to the Council Assessing Officer. It is noted that two (2) representations were received after the closure of the public notification period. These have been included as part of this report and addressed through the Applicant's response.

As per Section 9.1 of the Council Assessment Panel Operating Procedures, it is at the discretion of the Assessment Manager to exclude a representation received out of time. In this instance, the Assessment Manager has reviewed the two (2) late representations and determined that they will be accepted as part of the representations for this proposal.

Additionally, the Assessment Manager has agreed to allow the Representors to be heard at the meeting, noting that both have indicated they wish to be heard through one of the other Representors (Mr Conrad William Isterling) already nominated to be heard.

A copy of the representations are included within Attachment 7.

All Representatives were in opposition to the proposal, with three (3) Representatives requesting to be heard before the Panel. The Representatives are listed below:

Representor received (Submission Source)		Summary of Issues Raised	Position	Wish to be heard
1	<p>Fran E 12 Wilcox Road, ELIZABETH SA 5112</p> <p>Online</p>	<ul style="list-style-type: none"> Objects to the number of indigenous trees proposed to be removed, stating that the Council should not approve the application given the loss of crucial tree canopy that is needed for cooling, shade and habitat in the area. Also stated, that the trees could be retained if the carpark were located beneath the building and that an Ice arena is not necessary infrastructure, and the development should be refused due to the negative environmental impacts associated with the tree removal. 	Oppose	No
2	<p>Conrad William Isterling 60 Ashfield Road, ELIZABETH SA 5112</p> <p>Counter</p>	<ul style="list-style-type: none"> Objects to the development due to the increased air and sound pollution in the surrounding environment. Residential roads are inadequate to catering for commercial developments and that this would not result in an appropriate planning outcome. The development will not meet community needs. Noting that the nearby gym located within 80 metres from Philip Highway is underused. The removal of trees is eliminating a buffer to the residential properties from Main North Road and exposing residents to further noise impacts. The representation has also raised concerns about the declining health of the area due to the increased traffic generating from the development. 	Oppose	Yes
3	<p>Yeshaiah Varona 15 Broadwater Place, BLAKEVIEW SA 5114</p> <p>Online</p>	<ul style="list-style-type: none"> Objects to the development on the basis that an ice-skating arena should not be built in Elizabeth, as it does not cater to the socio-economic profile of the community and will become an expensive, underutilised facility. 	Oppose	No

		<ul style="list-style-type: none"> Questions the changes from the original development application noting that the removal of the basketball court, rock climbing, children's play area reduces social benefit to the community. 		
4	<p>Beverly Rudnick 17 Fletcher Road, ELIZABETH EAST SA 5112</p> <p>Online</p>	<ul style="list-style-type: none"> Objects to the development due to the removal of mature trees, the degradation of prominent features at the main entrance to the Elizabeth City Centre, and the loss of an opportunity to establish the area as open space connected to Fremont Park. The representation also raises concerns that the area may become rundown and that increased traffic congestion will increase pollution to streetscape and health impacts on nearby residents. 	Oppose	No
5	<p>Troy Davill 9 Whitehead Crossing, GREENWITH SA 5125</p> <p>Online</p>	<ul style="list-style-type: none"> Concerned that the cost of the proposed development will result in high ticket prices that would not be affordable to residents. Also stated that the reduction in scope to the original proposal is not appropriate and questions why the developer is removing critical elements. 	Oppose	No
6	<p>Ronald Peter Stirling 29 Wilcox Road, ELIZABETH SA 5112</p> <p>Verbal (Over phone)</p>	<ul style="list-style-type: none"> Objects to the development due to the existing trees stopping air emissions to nearby residential properties and the increased health risks to residents due to the loss of environment. Also is concerned with the level of traffic that would be utilising residential roads (i.e., Ashfield Road) and believes that the development is not appropriate to the zoning, specifically as the loss of trees/habitat and the importance of open space to the health of nearby residents. 	Oppose	No
7	<p>Kate Isterling 8 Shipton Street, ELIZABETH SA 5112</p> <p>Email (Late submission)</p>	<ul style="list-style-type: none"> Wishing to present views on the proposed development of the tree's windbreak of lot 101 Ashfield Road, Elizabeth that is located three minutes' walk from her house. 	Oppose	Yes
8	<p>William Isterling 8 Shipton Street, ELIZABETH SA</p>	<ul style="list-style-type: none"> Wishing to present views on the proposed development of the tree's windbreak of lot 101 Ashfield Road, 	Oppose	Yes

	5112 Email (Late submission)	Elizabeth that is located three minutes' walk from his house.		
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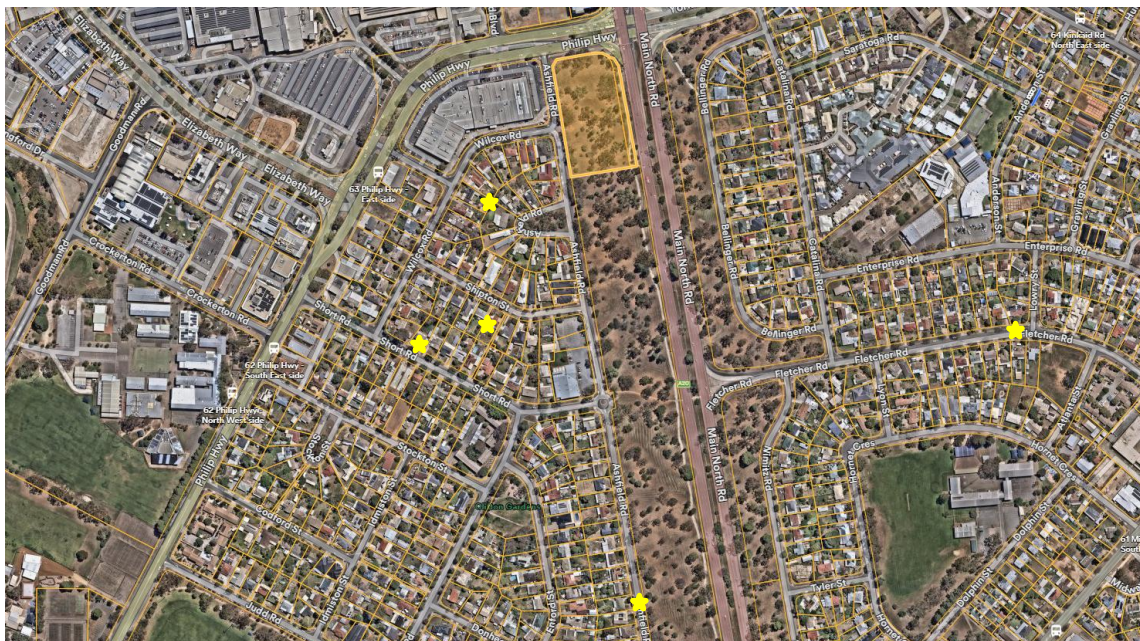


Figure 9: Received representations shown highlighted in yellow stars. Troy Davill's and Yeshaiah Varona's representations have not been highlighted in the above map as they appear outside of the locality.

The Applicant has been afforded the opportunity to respond to the matters raised by the Representors. The Applicant's response provides additional response on the following matters:

- Land use
- Tree removal and loss of open space
- Air pollution
- Traffic congestion.

A copy of the Applicant's response is contained within Attachment 8.

It must be noted that issues identified by the Representors are also addressed further in the planning assessment section of this report (see Section 7 in this report).

It is noted that an assessment of the application is limited to that of the relevant policies and provisions within the Code and that a number of aspects raised as part of the public notification process do not relate to matters to be considered within the planning assessment of the application. For this reason, matters raised relating to oversupply of indoor recreation facilities, ticket prices and Council decision making processes (i.e. not declaring land as open space) are not addressed as part of this report.

Moreover, as this form of development required public notification, the development is of a form which the Act identifies CAP as the relevant authority for the purpose of decision making.

5.3 Statutory Referrals

Pursuant to Schedule 9 of the *Planning, Development and Infrastructure Regulations 2017* (the Regulations), specifically Schedule 9 (3)(4) Future Road Widening and Corridors and Schedule 9 (3)(21) Advertising Near Signalised Intersections, a statutory referral was issued to the Commissioner of Highways for the two (2) associated overlays for this development application.

This information has been reviewed by staff from the Commissioner of Highways, who have offered support for the proposed development with conditions (contained with Section 11 Recommendation of this report).

5.4 Internal Referrals

An internal referral to Council's Engineers was undertaken to review traffic management, car parking, access and stormwater management.

- Council's Traffic Engineer formed the view that the proposed development is acceptable in terms of parking and traffic, noting the reduced car parking rate. Council's traffic engineer supports the lower level of off-street car parking due to the proximity to alternative public parking options and public transport as reflected within PO 5.1 of the Transport, Access and Parking Module of the Code.
- Council's Land Development Engineer formed the view that although the proposed development adequately addresses the required stormwater requirements in accordance with the original proposal, further information/investigation is required regarding the stormwater impact on the future public realm. Specifically, how the stormwater storage within the subject site discharges at a limited rate to ensure no impacts are generated as a result of the development to both council's stormwater infrastructure and DIT's road asset (identified as Main North Road). A reserved matter is proposed that requests further information on this matter prior to Building Consent should CAP support the proposal.
- The application was further referred to Council's Arboriculturalist, who formed the view that the removal of the one (1) regulated tree and 15 significant trees is appropriate given that the same trees are being retained on boundary with Ashfield Road, Elizabeth and Philip Highway. However, the assessing officer in consultation with Council's Tree Services Team has deemed the need for a reserved matter pertaining to an updated arborist report. The updated report will include the following:
 - A tree protection plan and tree sensitive methods for the trees to be retained by minimising any impact on the encroachment within the associated Tree Protection Zones (TPZs).
 - Refine the scope of the trees assessed (including further discussion regarding exempt tree species) and the final number of significant and regulated trees impacted by the development.
- The application was further referred to Council's Landscaping Team, who formed the view that the level of internal landscaping is appropriate.

Moreover, additional planting will be implemented to include street trees along Ashfield Road, Elizabeth consistent with the City of Playford Urban Design Guidelines. This planting will be undertaken by the City of Playford in conjunction with the Developer to ensure it aligns with the overall public realm upgrades.

6. Key Issues

Consistent with the original application, the following matters are considered pertinent in reaching a recommendation for the proposal:

- Whether the proposal is an appropriate form of development in the Urban Activity Centre Zone and the General Neighbourhood Zone
- Whether the proposal will create any adverse visual or amenity impacts or conflicts between adjoining land uses within the locality
- Whether the development will create adverse traffic impacts on the existing road network in the locality or generate any adverse noise or air pollution.

7. Planning Assessment

The subject land is located within the Urban Activity Centre Zone and the General Neighbourhood Zone (the Zones).

7.1 Urban Activity Centre Zone – Desired Outcome and Land Use

As per the Desired Outcome (DO) 1, the Zone seeks *“a cohesive and legible environment supporting a broad spectrum of regional level business, shopping, entertainment and recreational facilities that can cater for large crowds, smaller social gatherings and events over extended hour”*.

Urban Activity Centre Zone Performance Outcome (PO) 1.1 states:

Shops, offices, entertainment, health, education and recreation related uses and other businesses that provide a comprehensive range of goods and services to the region.

This PO is informed by the associated Designated Performance Feature (DPF) 1.1 which seeks that:

Development comprises of one (1) or more of the following:

(n) Indoor recreation facility

The proposed variation to the original application has not changed the primary land use as “Indoor Recreation Facility” and is consistent with the desired outcomes of the Zone to provide a range of recreation facilities and shops which accommodates large groups of people, while in proximity to public transport. This is in accordance with the desired outcomes of the Zone and the land use is consistent with the list of envisaged land uses in DPF 1.1 of the Urban Activity Centre Zone.

The proposed variation maintains the indoor recreation facilities, which complements the nearby activity centre function of the Elizabeth City Centre. As the centre is identified as a regional hub of activity within Council’s retail hierarchy and identified as a regional centre with the Greater Adelaide Regional Plan (GARP). The expansion of the land use to the subject land is logical and rationale with the services envisaged within the associated Urban Activity Centre Zone.

Consistent with the original proposal, it is considered that the proposal’s range of recreational facilities (i.e. Revo Fitness and Ice Arena) is appropriate for its location that supports the key performance outcomes of the Urban Activity Centre Zone.

7.2 Urban Activity Centre Zone – Built Form and Character

The proposed variation is still maintaining the original two-storey building with a decrease to the overall building height by 3.1 metres. Therefore, the scaled back design at a prominent location at the corner of Main North Road, Philip Highway and Ashfield

Road, still maintains integration and connection to the southern edge of the Elizabeth City Centre.

The connection of the built form is also reinforced through the upgrade of the public realm through a high level of landscaping/trees, bike track and a shared use path.

The building is also providing visual interest to all road frontages through a mix of materials and articulation.

Along with architectural elements, the increased setback to Ashfield Road, Elizabeth, provides visual interest to the building while maintaining separation between the land use and the residential properties within the locality.

Urban Activity Centre Zone PO 2.6 states:

Buildings and facilities are sited and designed to provide an active frontage to the public realm, contributing to streetscapes and spaces that encourage social interaction.

This proposal is consistent with PO 2.6 of the Zone by providing a positive impact to the streetscapes of the surrounding roads.

While the variation to the building design is scaled-back to a simplified form, the development is still maintaining distinctive elements at an appropriate scale for the associated land use and location.

7.3 Urban Activity Centre Zone – Building Height

The indoor recreation facility building originally had an approved overall height of 16.2 metres, which as part of this variation has been decreased to 13.1 metres. The variation to the building height is consistent with the varying heights of the bulky goods outlet and Elizabeth City Centre within the locality and wider of the subject land.

It must be noted that the Urban Activity Centre Zone does not provide a maximum or minimum building height, however, the Zone seeks medium-rise development with increased high-rise development at the centre of the Zone. As expressed within the Code, medium-rise is defined as “*in relation to development, means 3 to 6 building levels*”. It is acknowledged that the building is two-storey. However, the overall height is consistent with the existing height profile of the Urban Activity Centre Zone being between 13 metres to 32 metres high.

7.4 Urban Activity Centre Zone – Advertisements

The proposed variation encompasses signage as part of the proposal, specifically for the Revo Fitness tenancy. The signage strategy includes three advertisement signs affixed to the eastern elevation facing Ashfield Road, Elizabeth for each tenancy, along with a second affixed advertisement sign associated with the gym facing Main North Road.

Urban Activity Centre Zone PO 5.1 states:

Advertisements are sited and designed to achieve an overall consistency and appearance along individual street frontages.

Consistent with PO 5.1, the signs are sited and designed to achieve overall consistency and appearance along each street frontage. Although the signs are illuminated, they do not flash or rotate and it is considered that no light spill impacts are generated to adjoining residential properties.

7.5 Urban Activity Centre Zone – Vehicle Parking

The proposed variation to the development has removed the previous basement parking and maintained the ground level parking accessed through Ashfield Road, Elizabeth.

Urban Activity Centre Zone PO 6.1 states:

Vehicle parking areas are sited and designed to minimise visual dominance when viewed from the public realm.

The proposal, in accordance with PO 6.1, the parking areas are sited and designed to minimise visual dominance when viewed from the street and adjoining residential properties. As expanded on in Section 9.2 of this report, the development is providing 179 off-street car parking spaces and 24 bicycle spaces for the use of patrons to the indoor recreation facilities with any overflow parking to be serviced by nearby parking facilities open to the general public.

7.6 Concept Plan 81 - Edinburgh Defence Airfield Lighting Constraints

The proposal is located within the 'Controlled Light Installation Area', of Concept Plan 81 – Edinburgh Defence Airfield Lighting Constraints and is located within the Civil Aviation Safety Authority's 6 km radius from a (Defence) airfield.

Consistent with the original proposal, the variation does not cause any adverse upward light that would adversely impact on the operation of the airfield.

It must be noted that as a result of the variation decreasing the building height to 13.1 metres, the Applicant would not be required to seek clearance/approval from the Department of Defence due to the development being below 15 metres in height.

7.7 General Neighbourhood Zone – Desired Outcome and Land Use

As per the Desired Outcome (DO) 1, the General Neighbourhood Zone seeks *"low and medium density housing that supports a range of needs of lifestyles but does anticipate employment and community service uses that contribute to making the neighbourhood a convenient place to live without comprising residential amenity"*.

General Neighbourhood Zone PO 1.1 seeks:

Predominantly residential development with complementary non-residential uses that support an active, convenient, and walkable neighbourhood.

Furthermore, General Neighbourhood Zone PO 1.2 seeks:

Non-residential development located and designed to improve community accessibility to services, primarily in the form of:

- a) small scale commercial uses such as offices, shops and consulting rooms*
- b) community services such as educational facilities, community centres, places of worship, child care facilities and other health and welfare services*
- c) services and facilities ancillary to the function or operation of supported accommodation or retirement facilities*
- d) open space and recreation facilities.*

An indoor recreation facility is not listed in General Neighbourhood Zone DPF 1.1 as one of the land uses that would be most likely to satisfy PO 1.1. However, as per Part 1 of

the Code, a DPF is only one way to satisfy a PO and the fact that it is not a use that is listed in the DPF is not necessarily fatal to the application.

PO 1.1 and PO 1.2 speaks to considering and allowing complementary non-residential uses that support an active, convenient and walkable neighbourhood and provide services/facilities to the community. As stated previously, the land use has not changed from the original application and still maintains sufficient design and setbacks which complements the existing residential character of the Zone.

Additionally, PO 1.3 of the General Neighbourhood Zone seeks for:

Non-residential development sited and designed to complement the residential character and amenity of the neighbourhood.

As per PO 1.3, the proposed design changes should ensure that they are complementary to the residential character and amenity of the associated General Neighbourhood Zone. Although, the amended design compared to the original design has created a minimalistic design approach, the elements associated with precast concrete and metal cladding are consistent with the existing adjacent non-residential uses. The design changes are appropriate given that the built form incorporates a larger setback from Ashfield Road to provide greater separation and minimise impacts on the adjacent residential land. The changes are also supported due to the public realm upgrade of Philip Highway, Main North Road and Ashfield Road including a high degree of mature street trees and shrubs to soften the building's presence on the subject land.

It must be noted that, while the Ice Arena is located within the General Neighbourhood Zone (which incorporates the element of indoor recreation facility) that is not uncommon within the Zone. It is acknowledged that the building's scale and size is greater than generally experienced within the Zone. The bulk of the building is maintained through the Urban Activity Centre Zone, and therefore, it is considered that the building's design and setbacks appropriately limits potential negative impacts on the residential amenity of the General Neighbourhood Zone, specifically through the increased separation to Ashfield Road, Elizabeth.

7.8 General Neighbourhood Zone – Building Height and Setbacks

The proposed variation to the original application still maintains the low-rise development (two-storey) complemented with the generally single storey nature of the Zone. However, two-storey development is evident in the wider locality due to infill type development and is consistent with the changing density levels surrounding the Urban Activity Centre Zone.

The siting of the building being set back from Ashfield Road has changed from the original 16.6 metres to 36.49 metres, therefore, consistent with the Zone's PO 4.1 and PO 5.1,

General Neighbourhood Zone PO 4.1 states:

Buildings contribute to a low-rise suburban character.

As expressed within the Code, low-rise means *"in relation to development, means up to and including 2 building levels"*.

General Neighbourhood Zone PO 5.1 states

Buildings are setback from primary street boundaries to contribute to the existing/emerging pattern of street setbacks in the streetscape.

It is considered that the further setbacks and spacing provided around the site assists to limit the visual impact of the development to the adjacent residential streetscape.

7.9 General Neighbourhood Zone – Advertisements

As per PO 12.1, advertisements within the General Neighbourhood Zone are only envisaged where it identifies the associated business activity and does not detract from the residential character of the locality.

The only sign within the Zone is the Ice Arena identification sign above the main entry door, although this sign is illuminated, it is small scale in nature and does not impact the streetscape of Ashfield Road.

8. Planning Assessment – Overlay

8.1 Advertising Near Signalised Intersections

The architectural drawings show an illuminated sign for Revo Fitness visible from Main North Road. Given the separation of the sign from the existing signalised intersection, it is considered to not cause unreasonable distraction to road users.

It is also noted that the Commissioner of Highways have accepted the location of the signs with the following conditions to minimise any risk to road users:

- The illuminated signage shall be permitted to use LED lighting for internal illumination of a light box only
- The illuminated signage shall be limited to a low level of illumination so as to minimise distraction to motorists ($\leq 150\text{cd/m}^2$)
- The signage shall not contain any element that flashes, scrolls, moves or changes, or imitates a traffic control device.

8.2 Regulated and Significant tree

The Regulated and Significant tree Overlay seeks the conservation of regulated and significant trees to provide aesthetic and environmental benefits and mitigate tree loss.

DO 1 of the Regulated and Significant tree Overlay seeks the:

Conservation of regulated and significant trees to provide aesthetic and environmental benefits and mitigate tree loss.

Furthermore, within the Regulated and Significant tree Overlay PO 1.1 and PO 1.2 states:

PO 1.1

Regulated trees are retained where they:

- a) make an important visual contribution to local character and amenity*
- b) are indigenous to the local area and listed under the National Parks and Wildlife Act 1972 as a rare or endangered native species*

and / or

- c) *provide an important habitat for native fauna.*

PO 1.2

Significant trees are retained where they:

- a) *make an important contribution to the character or amenity of the local area*
- b) *are indigenous to the local area and are listed under the National Parks and Wildlife Act 1972 as a rare or endangered native species*
- c) *represent an important habitat for native fauna*
- d) *are part of a wildlife corridor of a remnant area of native vegetation*
- e) *are important to the maintenance of biodiversity in the local environment and / or*
- f) *form a notable visual element to the landscape of the local area.*

Consistent with the original application, the Applicant engaged Arborman Tree Solutions to undertake an assessment of existing trees at the proposed site of the development.

As part of the variation, of the 23 trees assessed, one (1) is identified as a regulated tree, and 15 are identified as significant trees. Seven (7) trees were determined to be exempt from the Regulations.

It must be noted that the original application, had 26 trees assessed, four (4) were identified as Significant trees, and 20 were identified as regulated trees. One (1) tree was determined to be exempt from the Regulation and another tree as unregulated.

The variation application includes the same trees to be removed. It must be noted that the original application was assessed under the previous Regulations reflecting a regulated tree as having >2 metres trunk circumference and a significant tree as having >3 metres trunk circumference. However, the variation application, lodged after the change to the Regulations which came into effect on 16 May 2024, has been assessed under the current Regulations reflecting a regulated tree as having >1 metres trunk circumference and a significant tree as having >2 metres trunk circumference.

Therefore, notwithstanding this legislative change, the same required trees have been retained in both applications.

It must be noted that the submitted demolition site plan shows a large number of trees on site, whereas the provided planning statement and arboricultural impact assessment report assess a smaller number of trees.

Therefore, Council staff have undertaken a site visit and have reassessed the trees to understand the additional regulated and/or significant trees that will be required to form part of the amended arboricultural impact assessment due to previously explained legislative changes. As per Figure 10 (page 24) shows the additional 16 regulated trees (highlighted in red), 16 exempt trees (highlighted in blue) and 24 non-present/existing shrubs (highlighted in yellow).

Council's assessment has deemed the final tree removal strategy to incorporate 17 regulated and 15 significant trees.

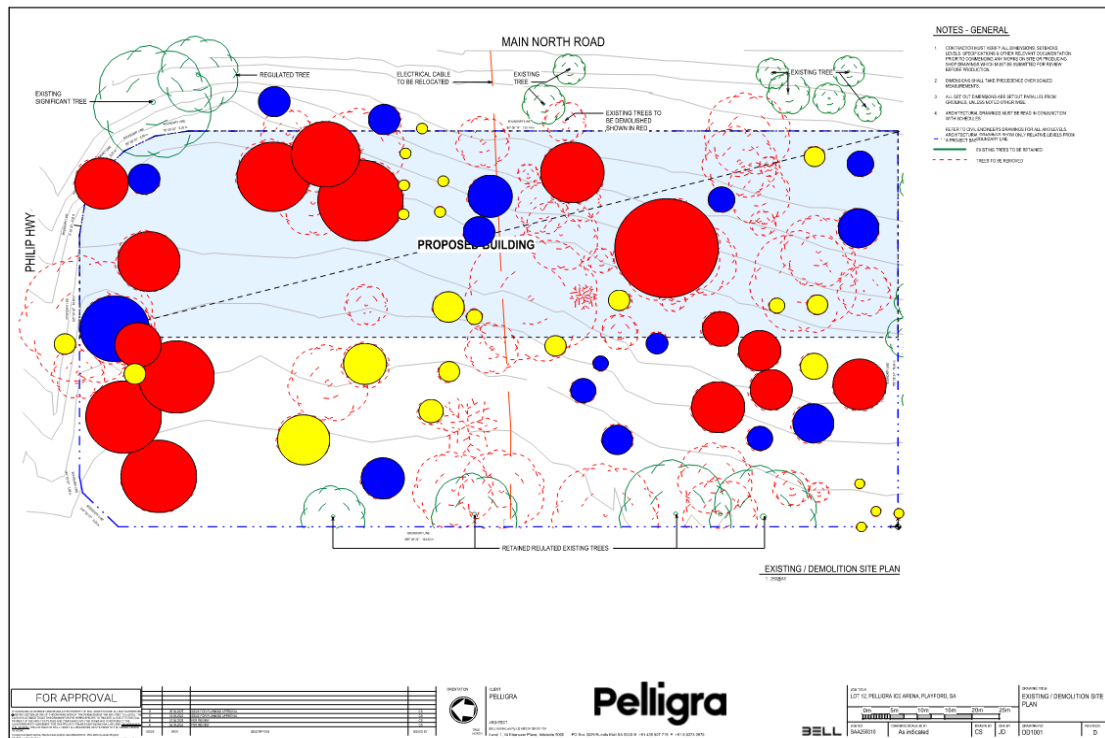


Figure 10: Council staff markup of additional regulated and exempt trees, and shrubs proposed for removal

Therefore, a reserved matter will be included to request an amended arborist report to reflect the legislative change to be reflective of the final number of additional trees deemed to be regulated or significant. This is also critical to ensure accurate conditioning of the payment into the Urban Tree Fund/replacement tree strategies.

It also must be noted that the reason for the reduction of the number of trees assessed from the original application having 26 trees assessed, and the variation application having 23 trees assessed, is that previously identified Trees 1, 2 and 3 will not be impacted by the proposed development.

Consistent with the original application and as reflected in figure 11, the Applicant has retained the four (4) significant trees (identified as trees 4, 5, 6 & 7) along Ashfield Road and two (2) significant trees (identified as trees 10 & 11) along Main North Road as part of the development. The variation has also resulted in the retention of one (1) significant tree along Philip Highway.



Figure 11: Significant trees to be retained as part of development

Council's Arboriculturalist has assessed the report and associated works and is satisfied with the level of tree retention and removal strategy associated with the development. Noting, the specialised construction and impact to the TPZ's of the retained trees requires further information including a tree protection plan and amended report to be provided by the Applicant prior to any works commencing. This is to ensure that the tree sensitive methods associated with the retention of trees are acceptable and does not jeopardise the long-term health and viability of the trees to be maintained.

As stated by the Representations, it is acknowledged that the existing trees within the subject land creates a positive amenity to the area, which forms part of the 'Wind Breaks' along the length of Main North Road. As clarified within the original application, this amenity value is considered to derive largely from the cumulative volume of the trees rather than any singular distinct tree contributing more or less than other trees along this portion of the road.

With regard to the significant and regulated trees which are proposed to be removed, an assessment of the amenity value of these trees has been undertaken, along with consideration as to how the reasonable development of the land in a manner envisaged by the zoning could be facilitated. Regulated and Significant tree Overlay PO 1.4 states:

A tree-damaging activity in connection with other development satisfies all the following:

- a) it accommodates the reasonable development of land in accordance with the relevant Zone or subZone where such development might not otherwise be possible*
- b) in the case of a significant tree, all reasonable development options and design solutions have been considered to prevent substantial tree-damaging activity occurring.*

The subject land can be reasonably anticipated to experience a higher order of use and site coverage due to the Urban Activity Centre Zoning and the anticipated suite of land uses within this zone.

As identified in the above sections, the proposed land use is appropriate for the Zone, and weighting of this is taken when considering the development of the land. It must be noted that the disbursement of the trees across the site does not allow for practical full retention while allowing reasonable development.

Consistent with the original application, regulated and significant trees (where possible), have been kept along the site boundaries to provide the highest possible level of visibility to the public along site boundaries. As stated earlier, this retention strategy is also reinforced by increased urban tree canopy through the Applicant's upgrade of the public realm inclusive of street trees along Main North Road and Philip Highway and the 9 replacement trees (specified as Dwarf Apple trees in concept landscaping plan) within the carparking area. This is also reinforced through the upgrade of the public realm by City of Playford in conjunction with the Developer to Ashfield Road to provide additional street trees. It must be noted that the internal trees within the proposed carparking area are unable to be retained due to their disbursement within the layout and the inability to retain trees within kerb uplifts due to waste vehicle movements/clearances.

To compensate for the removal of these regulated and significant trees a combination of payment into Council's Urban Tree Fund along with replacement trees has been proposed. This fund ensures that contributions are allocated for future tree planting initiatives. To reflect this position, two conditions of approval which reinforces this requirement will be included within the recommendation should CAP determine to grant Planning Consent for the proposal (see Section 11 Recommendation).

Therefore, the proposed decrease to the removal strategy from the original application is not considered to result in a level of harm to the immediate and wider amenity while still allowing reasonable development consistent with DO 1, PO 1.1, PO 1.2 and PO 1.4 of the Regulated and Significant tree Overlay.

8.3 Stormwater Management

Consistent with the original application, the variation proposal includes a stormwater management report and associated plans prepared by FMG Engineering. The intent of the stormwater management report and associated plans is to demonstrate the suitability of the development and to ensure appropriate management of stormwater, while not impacting adjoining land and the public infrastructure.

The proposed variation has shown a finished floor level of 34.00mm Australian Height Datum (AHD) to maintain at least 300mm above the height of a 1% annual exceedance probability (AEP) flood event and mitigate any flooding impact to the development.

The stormwater report also speaks to the current conditions of the site being inundated during a 100-year storm event and the development maintaining a peak flood conveyance of 1.1m³/s. The flood conveyance is to ensure that the pre-existing conditions are no worse as a result of the development.

Stormwater detail has been reviewed by Council's Land Development Officer who is comfortable with the built form and the stormwater strategy. However, a reserved matter is required to ensure that the stormwater quality is investigated, modelling is provided, safe runoff route and upgrade to the discharge point is allowed. It also must be noted that the amended documentation will need to speak to additional detention storage within the land, stormwater's system impact on retained trees and how the upgrade of the public realm will not result in increased flow on the road gutter at the left-turn slip lane at the Main North Road/Philip Highway Signalised Intersection.

As part of the flood mitigation strategy to the development, the amended report to be provided through a reserved matter prior to Building Consent will ensure that an internal stormwater pit within the carparking area will be created to ensure that stormwater flows are discharged to the public stormwater infrastructure at a limited discharge rate.

8.4 Traffic Generating Development

Consistent with the original proposal, the variation does not propose vehicle access to/from a State maintained road, nor would it have an impact on the performance of State maintained roads in the vicinity given the existing road network in which Ashfield Road is under the care and control of the City of Playford.

It must be noted that the Commissioner of Highways has reviewed the proposed traffic rates and documentation details within the application. They have provided support subject to conditions to ensure the road network does not underperform and maintains a high level of service to the generated traffic and wider vehicle movements.

9. General Development Policies

9.1 Design in Urban Areas

The variation seeks to alter the external appearance of the previously approved built form, inclusive of removal of articulation in building height and materials. Noting these changes, the variation still seeks to provide variance in materiality through the use of aluminium cladding to the Ashfield Road frontage and tilt up panel to the balance. The variation further seeks to provide design considerations to the Main North Road façade breaking this precast panelling up with feature LED strip lighting and accentuating the corner treatment at the intersection of Main North Road and Philip Highway.

The design remains contextual to the commercial development within the locality and provides for additional set back considerations to the residential interface. Coupled with the planned works to improve the surrounding realm, it is considered that the variation to the external design still meets the relative Design in Urban Areas policy provisions.

9.2 Interface Between Land Uses

The Interface between Land Uses General Development section of the Code contains a suite of provisions that seek to mitigate adverse impacts on or from neighbouring land uses. These policies are considered applicable to the proposed development.

The subject land is within and adjoins the General Neighbourhood Zone where a number of sensitive receivers are located in the form of residential properties to the west of the subject site. There is also land on the opposite side of Main North Road and to the immediate south of the subject site (which is currently vacant) that is located within the General Neighbourhood Zone.

An Environmental Noise Assessment Report undertaken by Sonus for the variation to the development based their assessment on the level of traffic including noise from car parking, waste collection, operating hours, music and mechanical plant operations.

The report concluded that the proposal, subject to several noise attenuation measures, can be supported to ensure that it will not cause unreasonable interference or detrimentally affect the amenity of the locality and ensures consistency with the intent of the Code. Subject to the conditions and recommendations contained within the report, it is considered that the development would not result in an unreasonable level of noise generation.

The key issues identified for consideration are illustrated under the following headings:

Noise

As stated within the report, the proposed development will generate noise levels due to the 24/7 operation of the gymnasium and minor music levels from both the Ice Arena and the gymnasium. Subject to the noise model and the operation of the indoor recreation facilities, several treatments are required to the external materials to minimise noise including providing a dedicated acoustically treated internal space for any group fitness classes.

In accordance with PO 4.1, 4.3 and 4.6 of the Interface between Land Uses Module states:

PO 4.1

Development that emits noise (other than music) does not unreasonably impact the amenity of sensitive receivers (or lawfully approved sensitive receivers).

PO 4.3

Fixed plant and equipment in the form of pumps and/or filtration systems for a swimming pool or spa are positioned and/or housed to not cause unreasonable noise nuisance to adjacent sensitive receivers (or lawfully approved sensitive receivers).

PO 4.6

Development incorporating music achieves suitable acoustic amenity when measured at the boundary of an adjacent sensitive receiver (or lawfully approved sensitive receiver) or Zone primarily intended to accommodate sensitive receivers.

It must be noted that a condition of approval consistent with the recommendations contained within the sonus environmental noise assessment has been included in the recommendation should CAP determine to grant Planning Consent for the proposal.

Also, the previous condition relating to mechanical plant, has been reflected within these variations as a reserved matter to ensure that a mechanical services plan is provided which confirms the type and size of mechanical equipment system, typically occurring prior to Council issuing Development Approval.

Hours of Operation

Consistent with the original application, the ice arena is operational between 6am to 10pm, while the gymnasium will operate 24 hours per day, 7 days per week. The proposed hours of operation are generally consistent with other similar indoor recreation facilities which typically accommodate busy periods during the morning and evening on weekdays and busy periods during the day on weekends. Comparable hours of operation are evident within the wider Elizabeth City Centre, with a 24/7 gym currently operating in the group of shops located within the building to the west of the site. It must also be noted that the 24-7 gym has moved from being located within the General Neighbourhood Zone component of the site to be located within the Urban Activity Centre Zone, therefore, further reinforcing the appropriate location of the gym being further located away from residential properties.

Furthermore, the proposed variation does not seek to change the hours of operation, and the acoustic treatments required for the operation of a 24-hour access gym will be replicated within the conditions of approval.

Lighting

As stated within the planning statement, outdoor lighting is yet to be determined, however, consistent with all developments, lighting and illumination shall be designed to conform with Australian Standards AS 4282 – 1997 'Control of the obtrusive effects of outdoor lighting'.

In accordance with PO 6.1 of the Interface between Land Uses Module, '*external lighting is positioned and designed to not cause unreasonable light spill impact on adjacent sensitive receivers (or lawfully approved sensitive receivers)*', the lighting for the purposes of security and use of car park at nighttime, will be directed downwards into the subject site and not causing light spill into adjoining residential properties.

As per the original application, a condition of approval which reinforces this requirement will be included within the recommendation should CAP determine to grant Planning Consent for the proposal.

9.3 Traffic Impact, Access and Car Parking

Consistent with the original proposal, the Applicant engaged CIRQA to provide design and traffic/waste management assessment advice for the proposed development. Specifically preparing a traffic assessment report and waste management plan.

In accordance with PO 5.1 of the Transport, Access and Parking Module, it states:

Sufficient on-site vehicle parking and specifically marked accessible car parking places are provided to meet the needs of the development or land use having regard to factors that may support a reduced on-site rate such as:

- a) availability of on-street car parking*
- b) shared use of other parking areas*
- c) in relation to a mixed-use development, where the hours of operation of commercial activities complement the residential use of the site, the provision of vehicle parking may be shared*
- d) the adaptive reuse of a State or Local Heritage Place.*

This PO is informed by the associated DPF 5.1 which seeks

development provides a number of car parking spaces on-site at a rate no less than the amount calculated using one of the following, whichever is relevant:

- a) Transport, Access and Parking Table 2 - Off-Street Vehicle Parking Requirements in Designated Areas if the development is a class of development listed in Table 2 and the site is in a Designated Area*
- b) Transport, Access and Parking Table 1 - General Off-Street Car Parking Requirements where (a) does not apply*
- c) if located in an area where a lawfully established carparking fund operates, the number of spaces calculated under (a) or (b) less the number of spaces offset by contribution to the fund.*

As the subject land is located within a 'Designated Area', the car parking rates under Table 2 – Off Street Car Parking Requirements in Designated Areas is applicable to the development. Therefore, the DPF expectation is that approximately 221 car parking spaces are associated with the development.

The proposed variation includes a parking provision rate of 2.44 spaces per 100m² of total floor area with dedicated minibus parking to provide alternative transport solution for ride sharing. It must be noted that the original application included a parking provision of 2.22 spaces per 100m² of total floor area. Therefore, the variation includes 179 off-street parking spaces (within the ground level) as opposed to the original 236 off-street parking spaces (contained within the original basement and ground level layouts).

The shortfall in car parking spaces is appropriate given the mixed-use nature of the development and the Code contemplating lower parking provisions based on the type of development and associated land use.

In addition, as reflected within the traffic report, the shortfall in off-street parking spaces is also justified due to the development's differing peak periods, accessibility to nearby public transport options (i.e. bus stops in proximity of the subject land) including a short walk to the Elizabeth Train Station. Also, in accordance with PO 6.3 of the Transport, Access and Parking Module seeks '*vehicle parking areas are designed to provide opportunity for integration and shared-use of adjacent car parking areas to reduce the total extent of vehicle parking areas and access points*'. The proposal has shared parking between the Elizabeth City Centre and subject site is available for use including nearby alternative parking opportunities such as the Windsor Car Park for use of patrons to the development, if deemed required.

As the development relies on alternative public car parking options (i.e., the Windsor Car park) the pedestrian movement from/to the proposed development must be appropriate. Pedestrians would be using the safe crossing points at the Philip Highway/Main North Road through the signalised intersection to ensure that no pedestrians utilise illegal crossing across Philip Highway. This safe movement through the signalised intersection crossing points are controlled through pedestrian operated signals to ensure no conflicts between vehicles and cyclists/pedestrians occur through the busy intersection.

Therefore, consistent with the original application, the proposed development supports the integrated transport system by providing an adequate number of parking spaces in accordance with *Table 2 – Designated Areas*, while being in proximity to the high public transit opportunities associated with the Elizabeth City Centre and the Council's activity hub.

Transport, Access and Parking Module PO 9.1 states:

The provision of adequately sized on-site bicycle parking facilities encourages cycling as an active transport mode.

This PO is informed by the associated DPF 9.1 seeks

Areas and/or fixtures are provided for the parking and storage of bicycles at a rate not less than the amount calculated using Transport, Access and Parking Table 3 - Off Street Bicycle Parking Requirements.

The proposed variation, as opposed to the original application, has provided 24 bicycle parking spaces. In accordance with Table 3 – Off-Street Bicycle Parking Requirements, for indoor recreation facility, 1 bicycle space per 4 employees plus 1 space per 200m² of gross leasable floor area for visitors. Although the exact number of staff is not known, the traffic report has applied 30 staff members resulting in 44 spaces required. Given that 24 bicycle spaces have been provided, based on the varying peak period of the land uses, the rate provided is appropriate against PO 9.1 of the Transport, Access and Parking Module. It is also noted that the provided 1.8 metre high acoustic fence on the southern car park boundary prevents vehicle movement into the balance windbreak.

Separate from the parking assessment, CIRQA have provided a waste management plan to show that the waste storage and management areas are appropriately screened from public view, in accordance with PO 1.5 and PO 11.2 in the Design in Urban Areas Module.

PO 1.5

The negative visual impact of outdoor storage, waste management, loading and service areas is minimised by integrating them into the building design and screening them from public view (such as fencing, landscaping and built form), taking into account the form of development contemplated in the relevant Zone.

PO 11.2

Communal waste storage and collection areas are located, enclosed and designed to be screened from view from the public domain, open space and dwellings.

The waste storage and management areas are screened through fencing and landscaping. The waste storage area is also positioned to allow a waste vehicle to enter and exit the site in a forward motion without impacting on patrons of the development and private vehicles, consistent with PO 11.4 of the Design in Urban Areas Module.

Council's Traffic Engineer did not raise any concerns with the proposed variation, specifically, the justification provided for the decreased number of car parking spaces and utilising nearby public transport services and alternative parking options such as the Elizabeth City Centre and the Windsor car park.

10. Conclusion

The proposed variation is an appropriate development within the Urban Activity Centre Zone and General Neighbourhood Zone, the Overlays which apply to the site, and of the various sections of the General Development Policies of the Code. In summary:

- The variation results in a decrease to the overall building height to ensure less visual impact to nearby residential properties and reduce the visual prominence of the development within the wider locality.
- The variation is appropriate when considering the changes associated with the removal of internal elements to ensure the primary land use of an indoor recreation facility is the focus of the design.
- Although the architectural design has been simplified, the variation ensures an increased setback from Ashfield Road to reduce impact on the associated General Neighbourhood Zone and adjacent residential uses.
- The variation pertains scaling back the original proposal and the inclusion of required advertisement, further landscaping and a reduced number of trees being removed, therefore, reducing the impacts of the previously approved development.

On balance, the proposed variation to the original development is not considered to be 'seriously at variance' with the relevant provisions of the Code. The proposal warrants Planning Consent subject to the conditions listed below.

11. Recommendation

1. DETERMINES that the variation is not seriously at variance with the provisions of the Planning and Design Code.
2. GRANTS Planning Consent to the application by Pelligra Group C/- Future Urban for the Variation to DA 23022101 – Amendments to the two-storey building encompassing a mix of indoor recreation facilities, decrease in building height by 3.1 metres, removal of basement car parking, revised car parking layout, amended landscaping, advertisement and revised tree damaging activity in the form of the removal of regulated and significant trees, subject to the following reserved matters and conditions:

Reserved Matter(s)

Pursuant to Section 102(3) of the *Planning, Development and Infrastructure Act 2016* (the PDI Act), the following matters shall be reserved for further assessment to the satisfaction of the Council Assessment Panel, and sub-delegated to the Assessment Manager for determination:

1. The Applicant is to submit a mechanical services plan (or similar) that confirms the type and size of mechanical equipment system to be installed within the facility prior to Council issuing Development Approval.
2. The Applicant is to submit an updated landscaping concept plan which shows the retained trees along Philip Highway and Main North Road prior to Council issuing Development Approval.
3. Prior to lodgement of Building Consent, the Applicant is to submit an amended arboricultural impact assessment report stating the final number of retained and removed regulated and significant trees consistent with the Regulations definitions of significant and regulated trees effective from 16 May 2024. The report must be inclusive of a tree protection plan and tree protection specifications which demonstrates the proposed tree sensitive methods and specialised construction methods as stated within the provided Arboricultural Impact Assessment Report prepared by Arborman Tree Solutions dated 15 October 2025.
4. Prior to lodgement of Building Consent, the Applicant shall submit a detailed Civil and Stormwater Management Plan detailing stormwater quantity, and quality measures prepared in consultation with the City of Playford and Department for Infrastructure and Transport to provide specific detail in relation to the following:
 - A detailed Stormwater Management Plan will include:
 - Existing ground and floor levels and proposed finished floor levels and site/bench levels, including in relation to the top of any kerb level, showing the height and location of any earthworks or retaining walls.
 - Retained trees reflected within the arboricultural impact assessment report.
 - Detention storage volume increased to cater for the 2,220m² carparking area with the currently modelled 210 cu.m.
 - Proposed stormwater treatment measures to cater for the increased car park surface area.

- Method of stormwater disposal, all discharge locations with appropriate scour protection and associated discharge rates.
- Demonstrate that a safe major storm overland flow path can be conveyed to the discharge location to ensure that adjacent properties are protected from any risk of flood inundation.
- In lieu of discharging major storm pre-development flow direct into the existing single side entry pit, Council requirements are as follows:
 - Development pipe discharge into a double side entry pit.
 - Bubble-up pit relocated to within the development site.
 - Provide a safe major storm runoff route via two proposed driveway crossovers.
 - Pipe discharge into SEP controlled to pre-development flow rates in consultation with the City of Playford.
- DRAINS model (in .drn file format) will include:
 - Pipe discharge to side entry pit.
 - Major storm overland spill runoff via driveway crossovers.
 - Increased detention storage regarding the new 5,860 roof and 5,439 surface post-development catchment properties.
- MUSIC model (in .msf file format) will include:
 - Water quality treatment reduction targets of the typical urban average annual load to meet EPA and WSUD Guidelines for the Greater Adelaide Region as follows:
 - Total Suspended Solids (TSS) 80%.
 - Total Phosphorus (TP) 60%.
 - Total Nitrogen (TN) 45%.
 - Retention of litter greater than 50 mm for flows up to a 3-month Average Recurrence Interval (ARI) peak flow.
 - No visible oils for flows up to a 3-month ARI peak flow.
 - The treatment flow shall be based on a 3-month to 1-year ARI peak discharge rate from the local catchment. This is to ensure that 98% of all annual rainfall and daily runoff events from the local catchment will receive treatment to the best practice standards.
- A detailed Civil Plan of the Public Realm Upgrade will include:
 - Stormwater management strategy for the public realm upgrade to remove any stormwater flow on the road gutter at the left-turn slip lane of Philip Highway and Main North Road as part of the proposed reconstruction of the Main North Road kerb.

- The Applicant is to notify DIT to inspect major stormwater runoff and impact on road users due to the diversion of stormwater flow from Philip Highway to Main North Road.
- Stormwater runoff at trapped low point required to be managed appropriately and to show the integration of the stormwater system with the public infrastructure to reduce any impact to pedestrians/cyclists utilising 3m shared use path and bike track.
- Existing and proposed RL levels to be reflected accurately.
- Retained trees reflected within the arboricultural impact assessment report.
- Disability Discrimination Act (DDA) compliant footpath connection between internal footpath to the Philip Highway and Ashfield Road external footpath.
- Extent of public realm such as 3 metres shared use path and bike track, verge and street trees with the integration of the kerbed stormwater system.

Conditions

Council Conditions:

1. The development must be undertaken, completed and maintained in accordance with the plan(s) and information detailed in this Application except where varied by any condition(s) listed below.

Reason: To ensure that the development is constructed and operated in accordance with the plans and details provided.

2. All recommendations and noise attenuation measures contained within the Environmental Noise Assessment Report by Sonus Document Reference S7578C5 dated September 2025, shall be implemented prior to occupation of the site and complied with at all times.

Specifically, the following noise attenuation measures (consistent with page 8 and 14) are to be implemented:

- Specific roof/ceiling construction of the gym.
- Specific glazing for the gym façade.
- Constructing an airlock for the gym entry.
- Facing the ventilation for the ice arena plant room louvres in a specific direction.
- Specific locations for the roof-mounted mechanical plant.
- Construction of rooftop mechanical plant screens using acoustic louvres (or a solid barrier) with specific height requirements.
- Specific performance requirements for the acoustic louvres.
- Installation of acoustic absorption to the underside of the plant room soffit; and

- Construction of a 1.8 metres high boundary made of 0.35mm BMT sheet steel (Colorbond or similar) to the southern carpark boundary.
3. Operating hours of the facility (Monday to Sunday) herein approved as follows:
- 6am to 10pm for the ice arena;
 - 7am to 10pm for retail/commercial tenancy; and
 - 24 hours for the gymnasium.

4. Deliveries and service hours herein approved are as follows:

Rubbish collection and deliveries:

- Between 9am and 7pm on Sundays or Public Holidays
- Between 7am and 7pm on any other day.

5. In lieu of planting 69 replacement trees, \$35,604 (@ \$516 per tree) must be paid into Council's Urban Tree Fund within one month of the tree(s) being removed.

Reason: To ensure compliance with the legislative requirement for the planting of replacement trees, pursuant to Section 127(4) of the *Planning, Development and Infrastructure Act 2016*.

6. Replacement trees must be planted within 12 months of completion of the development at the following rates:

- if the development relates to a regulated tree—2 trees to replace a regulated tree; or
- if the development relates to a significant tree—3 trees to replace a significant tree.

Nine (9) replacement trees must be planted within the carparking area for the required removal of trees.

7. All loading and unloading of goods and merchandise shall be carried out upon the subject land and no loading of any goods or merchandise shall be permitted to be carried out in the street in conjunction with the consent herein granted.
8. Landscaping shown on the plans herein approved shall be established to the reasonable satisfaction of Council prior to the operation of the development and shall be maintained and nurtured at all times with any diseased or dying plants being replaced within the first planting season following removal.
9. The operator of the facility is to maintain a Traffic Event Management Plan that will be provided to Council (5 business days) prior to any major event (ice hockey or of a similar scale) where traffic and pedestrian movements are required to be managed.
10. All waste and rubbish shall be stored in covered containers prior to removal and shall be screened from public view.
11. Any lights on the subject land must be directed and screened so that overspill of light into the nearby properties is avoided and motorists are not distracted.

12. All vehicle car parks, driveways and vehicle entry and manoeuvring areas shall be designed and constructed in accordance with the relevant Australian Standards and be constructed, drained and paved with bitumen, concrete or paving bricks in accordance with sound engineering practice and appropriately line marked to the reasonable satisfaction of Council prior to the occupation or use of the development.
13. Car parking areas, driveways and vehicle manoeuvring areas shall be maintained at all times to the reasonable satisfaction of the relevant authority.
14. All stormwater drainage shall discharge so that it does not flow or discharge onto the land of adjoining owners or, in the opinion of the relevant authority, detrimentally affect structures on this site, any adjoining land or public road.

Department of Infrastructure and Transport Conditions:

15. Access to the site shall be gained via Ashfield Road in accordance with the site plan produced by BELL, Job No. BAA250010, Drawing No. DD2011, Rev. G, Dated 29.09.2025.
16. All stormwater run-off shall be collected on-site and discharged without impacting the safety and integrity of the adjacent road network (including the adjacent embankments). A final stormwater management plan shall be developed in conjunction with DIT and in accordance with DIT Master Specification RD-DK-D1 Road Drainage Design and other relevant guidelines. All drainage infrastructure is to be to the satisfaction of Council and DIT.
17. The illuminated signage shall be permitted to use LED lighting for internal illumination of a light box only.
18. The illuminated signage shall be limited to a low level of illumination so as to minimise distraction to motorists ($\leq 150\text{cd/m}^2$).
19. The signage shall not contain any element that flashes, scrolls, moves or changes, or imitates a traffic control device.

Advisory Notes

1. The Metropolitan Adelaide Road Widening Plan shows a possible requirement for a strip of land up to 4.5 metres in width from the Main North Road and Philip Highway frontage of this site for future upgrading of Main North Road and Philip Highway intersection. The consent of the Commissioner of Highways under the *Metropolitan Adelaide Road Widening Plan Act 1972* is required to all building works on or within 6.0 metres of the possible requirement. As the development encroaches within the above area, the attached consent form and a copy of the approved plan/s must be provided to DIT (via dit.landusecoordination@sa.gov.au) for consent purposes.
2. The Applicant has a right of appeal against the conditions which have been imposed on this Planning Consent, Such an appeal must be lodged at the Environment, Resources and Development Court within two months from the day of receiving this notice or such longer time as the Court may allow. The Applicant is asked to contact the Court if wishing to appeal. The Court is located in the Sir Samuel Way Building, Victoria Square, Adelaide, (telephone number 8204 0289).
3. Any pruning, damage or removal of a Council tree by a person, other than the Council (or a person acting under some other statutory authority) may result in action taken under Section 221 of the *Local Government Act 1999*. Maximum penalty \$5,000.

4. The Applicant is reminded of its general environmental duty, as required by Section 25 of the *Environment Protection Act 1993*, to take all reasonable and practicable measures to ensure that the activities on the whole site, including during construction, do not pollute the environment in a way which causes or may cause environmental harm.

If during any site works, contamination is identified which poses actual or potential harm to the health or safety of human beings or the environment that is not trivial, taking into account the land use, or harm to water that is not trivial, the Applicant may need to remediate the contamination in accordance with EPA guidelines.

EPA information sheets, guidelines documents, codes of practice, technical bulletins etc can be accessed on the following web site: <http://www.epa.sa.gov.au>



September 30, 2025

Matt Dineen
Assessment Manager
City of Playford
Via: *The PlanSA Portal*

Dear Matt,

**RE: VARIATION TO APPLICATION 23022101 – LOT 101 ASHFIELD ROAD,
ELIZABETH**

We write on behalf of Pelligra Group (**Proponent**).

The proponent seeks planning consent from the City of Playford (**Council**) to vary the previous planning consent in relation to Application ID 23022101 at Lot 101 Ashfield Road, Elizabeth (**the site**). The variation application seeks to construct a two-storey building comprising a mix of indoor recreation facilities with associated ground level car parking, landscaping, advertisement and tree damaging activity.

Please find enclosed:

- a copy of the architectural plans prepared by BELL Architecture (**Appendix 1**);
- a copy of the landscape concept drawing (**Appendix 2**);
- a copy of the stormwater management plan and civil engineering drawings prepared by FMG Engineering (**Appendix 3**);
- a copy of the traffic and parking assessment prepared by CIRQA (**Appendix 4**);
- a copy of the waste management plan prepared by CIRQA (**Appendix 5**);
- a copy of the arborist report prepared by Arborman Tree Solutions (**Appendix 6**: to be provided once finalised); and
- a copy of the acoustic report prepared by Sonus (**Appendix 7**).

The purpose of this letter is to describe the site, its surroundings and the proposal, and to assess the proposal against relevant provisions of the Planning and Design Code (**the Code**).

Background

The site presents itself as a key landmark entrance to the Elizabeth City Centre which principally includes the Elizabeth Shopping Centre being a regional shopping centre serving the northern suburbs of Adelaide and the Playford Civic Centre which is home to the iconic Shedley Theatre, Playford Library, City of Playford Customer Care Centre and Council Chambers.

The subject site remains under the ownership of Council and prior to the application progressing to submission, significant consideration has been given to the proposed land use.

An application for this site has already appeared before the Council Assessment Panel, whereby planning consent was originally granted on August 18, 2024, for the following:

'Construction of a two-level (plus basement) mixed-use building incorporating indoor recreation facilities, shops, basement and ground level car parking, tree damaging activity in the form of the removal of 13 regulated and 3 significant trees and associated landscaping'

We confirm that the development authorisation is, and will remain, operative until August 18, 2026, thereby satisfying the requirements of Section 128(2)(a) of the Planning Development and Infrastructure Act 2016 (**the Act**).

As the extent of the variation application is not minor in nature, the application must be treated as a new application for development authorisation. The extent of development which is assessable under this application is limited to the extent of the proposed variation herein.

If the legislated process as per the PDI Act remains unclear, the correct approach was recently affirmed in *Canning and Laycock Pty Ltd v State Planning Commission* [2025] SAERDC 7 which referenced the following:

20. In Holds & Ors v The City of Port Adelaide Enfield & Ors [2011] (Holds) Kourakis J (as he was then) outlined that the first question to be addressed in determining a variation application was whether the proposal is a variation of the approved development or an application for approval of a different development.

22. The second step outlined in Holds is to identify the extent of the variation between the proposal and the approved development. These elements will comprise the extent of the proposed variation, and the development for which authorisation is sought for the purposes of s 128(2)(b) of the Act. The extent of the variation, and the development for which approval is sought, is outlined above. The Act is clear that elements of the development which remain unchanged by the variation proposal are not to be assessed, or rather re-assessed, within the variation application.

To assist Council's assessment, the following key changes to the proposal are noted:

- the overall development has been scaled back, with the removal of the following elements:
 - » Basement car parking;
 - » Basketball courts;
 - » Rock climbing gym;
 - » Funtopia playground; and
 - » Shop/restaurant and outdoor dining area;
- building height decreased from 16.2 metres to 13.1 metres;
- revised car parking layout and provision;
- increased building setback from Ashfield Road;
- updated landscaping plan;
- inclusion of advertisement; and
- simplified and scaled-back architectural design.

All supporting documentation submitted with the original application has been updated to reflect these changes.

Additionally, a land division application has been approved to realign the site boundary, ensuring it reflects the subject land of the current proposal.

The Site

The site is generally rectangular in shape, featuring an integrated corner cut-off at the intersection of Ashfield Road and Philip Highway, and a curved corner cut-off at the junction of Philip Highway and Main North Road. It is legally described as Allotment 101 in Deposited Plan D133867, within Certificate of Title Volume 6305 Folio 58.

The total site area is approximately 11,298 square metres, with a primary frontage of 142 metres to Ashfield Road, a 70-metre frontage to Philip Highway to the north, and a 150-metre frontage to Main North Road to the east.

The land is relatively flat and currently vacant, with no existing built structures. A number of established trees are scattered throughout the site. Due to its longstanding vacant status, there is currently no formal vehicular access to the property.

The Locality

An inspection of the subject site and its surrounding context confirms that the locality supports a diverse mix of land uses, including retail and commercial activities, established residential development, and passive open space areas.

Notably, the northern part of the locality features a concentration of retail and commercial uses, including bulky goods outlets, a supermarket, and a variety of specialty stores forming part of the Elizabeth Shopping Centre. This area also includes key civic buildings associated with the City of Playford Civic Centre.

To the east and west of the site, the surrounding residential areas are predominantly low-rise in character, comprising detached and semi-detached dwellings. These areas fall within the General Neighbourhood Zone and are consistent with the intended residential scale and built form of that Zone.

A key feature of the locality is the intersection of Philip Highway and Main North Road, located to the north of the site. Both roads are state-maintained and represent major transport corridors within the broader region.

Zoning

The site is located within the Urban Activity Centre (UAC) Zone and General Neighbourhood (GN) Zone and captured by the following Overlays:

- Advertising Near Signalised Intersections;
- Building Near Airfields;
- Defence Aviation Area – All structures over 15 metres;
- Future Road Widening;
- Hazards (Flooding - General);
- Major Urban Transport Routes;
- Prescribed Wells Area;
- Regulated and Significant Tree;

- Stormwater Management;
- Traffic Generating Development;
- Urban Tree Canopy.

It is also subject to the following concept plan:

- Concept Plan 81 - Edinburgh Defence Airfield Lighting Constraints

The Proposal

The proposed variation involves the construction of a two-storey building comprising a mix of indoor recreation facilities with associated ground level car parking, landscaping, advertisement and tree damaging activity.

The proposal is summarised below and depicted across the compendium of design drawings enclosed within this statement.

Land Use

The development proposes:

- A two-storey building comprising a mix of indoor recreation facilities, including:
 - » a 3,307 square metre ice arena;
 - » a 2,535 square metre gym; and
 - » a 1,500 square metre tenancy (for the purposes of an indoor recreation facility) which is yet to be confirmed.

The proposed land use is consistent with the land use definition for 'indoor recreation facility' in Part 7 of the Code.

Built Form and External Materials

The proposal features a single building set back approximately 36.0 metres from Ashfield Road, positioned along the north, east, and west boundaries. The building is designed to a high architectural standard, with the following key features:

- it will frame and enhance the corner presentation of the site with striking vertical lines, created through express joints, varied colours, and feature LED strip lighting, which will break up the precast concrete construction;
- the west elevation will be finished in profiled metal cladding, with projecting awnings, expansive glazing, and a verandah canopy to define entry points and create a pedestrian-friendly atmosphere; and
- the building will be constructed using durable, long-lasting materials and features a large footprint, providing flexibility for a range of future uses.

Vehicle Access and Parking

A copy of the Traffic and Parking Report is contained in **Appendix 4**.

As previously approved, vehicle access to the site will be provided via two entry points on Ashfield Road, each at least 6.0 metres wide to accommodate two-way traffic, including commercial vehicles up to 10.5 meters in length for waste collection and deliveries.

The site will feature a total of 179 at-grade parking spaces. Additionally, parallel bays will be available for mini-buses and service/delivery vehicles, though access to these bays will be infrequent, typically when adjacent roller doors are not in use.

All car parking spaces are designed to comply with the requirements of AS/NZS 2980.1:2004, *Parking Facilities Part 1: Off-street car parking*, as well as AS/NZS 2890.1:2002 *Parking Facilities Part 6: Off-street parking for people with disabilities*.

The proposal also includes 22 bicycle parking spaces, with end-of-trip facilities anticipated within the tenancies.

Civil and Stormwater

The application is supported by a Stormwater Management Plan (**SMP**) and accompanying civil drawings prepared by FMG Engineering (refer to **Appendix 3**). The stormwater system has been designed in accordance with the City of Playford's *Land Division Guideline – Appendix A: Stormwater Requirements*, as well as specific guidance provided by Council in relation to this development.

While the supporting SMP provides the full technical detail, the key features of the proposed stormwater system are summarised below:

- A 210m³ on-site detention tank is proposed to ensure post-development peak discharge during both major and minor storm events does not exceed the pre-development peak flow rates for equivalent events;
- A new 150mm high barrier kerb is proposed along Main North Road to safely divert existing overland floodwaters within the road reserve; and
- The internal stormwater network has been designed to manage peak discharge flow rates consistent with pre-development conditions.

The SMP confirms that the site can be developed as proposed without adversely impacting downstream properties or drainage infrastructure and that Council's stormwater management requirements can be met.

To mitigate potential flood risks, the development also incorporates the following design responses:

- A finished floor level (**FFL**) set 300mm above the designated flood level, ensuring appropriate site grading, boundary levels, and vehicle access are maintained; and
- The main building is proposed at a level of 34.00m AHD. While this may be refined during detailed design, a minimum freeboard of 150mm above the 1% AEP internal ponding level, and 300mm above the 1% AEP external flood level, will be maintained.

Waste Management

A copy of the Waste Management Plan is contained in **Appendix 5**.

Expected waste generation rates are as follows:

- General waste – 2,603 litres;
- Combined recycling (glass, cans and plastics etc.) – 1,504 litres;
- Cardboard recycling – 603 litres; and
- Green organics – 1,549 litres.

It is proposed that each tenant will manage waste independently, and in particular:

- an at-grade, secured and ventilated bin storage area located within the at-grade car park, sized to accommodate the requisite number of bins for one or two collections per week depending on the waste stream;
- waste will be transported to the bin store by staff or cleaning contractors;
- a refuse collection vehicle can store within the 'break' (aisle) in the central rows of parking without impacting general car park circulation, exiting the site in a forward motion; and
- any additional waste streams (i.e. confidential documents/shredding, printer cartridges, light globes, etc.) would be undertaken within the individual tenancies and collection would be organised by the tenant and undertaken by private contractors, as and when required.

Landscaping

The landscape concept plan is provided at **Appendix 2**.

The landscape plan features a diverse range of vegetation, including the retention of four existing trees along the southern boundary and the planting of nine new trees within garden beds distributed throughout the car parking area. Additionally, a mix of ground covers and low-level plantings are proposed within the dedicated garden beds to enhance visual amenity and contribute to the site's overall landscape quality.

Regulated and Significant Trees

A copy of the Arboricultural Impact Assessment Report is contained in **Appendix 6**. A summary of trees that exist on the site of the development, their status and what is proposed is outlined in Table 1 below.

Table 1 Summary of regulated and significant trees

ID	Species	Legislative Status	Development Impact
4	<i>Schinus areira</i>	Exempt	Retained
5	<i>Eucalyptus leucoxylon</i>	Regulated	Retained
6	<i>Eucalyptus cladocalyx</i>	Regulated	Retained
7	<i>Eucalyptus cladocalyx</i>	Regulated	Retained
8	<i>Eucalyptus cladocalyx</i>	Regulated	Removed
9	<i>Eucalyptus occidentalis</i>	Significant	Removed
10	<i>Eucalyptus cladocalyx</i>	Regulated	Retained
11	<i>Eucalyptus cladocalyx</i>	Regulated	Retained
12	<i>Corymbia citriodora</i>	Regulated	Removed
13	<i>Corymbia citriodora</i>	Regulated	Removed
14	<i>Tamarix aphylla</i>	Exempt	Removed
15	<i>Tamarix aphylla</i>	Exempt	Removed
16	<i>Tamarix aphylla</i>	Exempt	Removed
17	<i>Eucalyptus cladocalyx</i>	Regulated	Removed
18	<i>Tamarix aphylla</i>	Exempt	Removed
19	<i>Tamarix aphylla</i>	Exempt	Removed

20	<i>Tamarix aphylla</i>	Exempt	Removed
21	<i>Eucalyptus cladocalyx</i>	Regulated	Removed
22	<i>Eucalyptus cladocalyx</i>	Significant	Removed
23	<i>Eucalyptus cladocalyx</i>	Regulated	Removed
24	<i>Eucalyptus cladocalyx</i>	Unregulated	Removed
25	<i>Eucalyptus cladocalyx</i>	Regulated	Removed
26	<i>Eucalyptus cladocalyx</i>	Regulated	Removed

In total:

- 8 regulated trees are proposed to be removed; and
- 2 significant tree is proposed to be removed.

Highlighting that, following the land division approval, Trees 1, 2, and 3 now fall outside the development site and will not be impacted by the proposed development. Additionally, the six *Tamarix aphylla* trees are classified as Declared Weed Species under the *Natural Resources Management Act 2004* and are therefore exempt from protection requirements at this location.

Advertisements

Three advertisement signs are proposed to be affixed to the eastern elevation facing Ashfield Road for each tenancy, along with a second advertisement sign associated with the gym facing Main North Road. The signs are illuminated in nature and will not flash or rotate.

No other signage is proposed as part of this development application.

Procedural Matters

At the time of submission, the relevant version of the Planning and Design Code (**Code**) was consolidated on September 25, 2025 (Version 2025.18).

Due to ongoing amendments, the version of the Code used to prepare this statement may not be the relevant version at the time of lodgement of the application. To the extent of any inconsistency, the version of the Code at the time of lodgement will be relevant for the processing and assessment of the application.

Verification

For the purposes of Regulation 31(1)(a), (b) and (c) of the *Planning, Development and Infrastructure Regulations 2017* (**Regulations**), the following applies:

Table 2 Verification snapshot

Verification Matter	Comment
Nature of Development	Construction of a two-storey building comprising a mix of indoor recreation facilities with associated ground level car parking, landscaping, advertisement and tree damaging activity.
Elements	<ul style="list-style-type: none">• Indoor recreation facility

	<ul style="list-style-type: none"> • Tree damaging activity • Advertisement
Category of Development	Performance Assessed
Relevant Authority	Council Assessment Panel at the City of Playford

Relevant Authority

Pursuant to Regulation 22(a)(ii) of the Regulations, the Council's Assessment Manager may act as the relevant authority where a proposed development is exempt from the requirements in Section 107(3) of the Planning, Development and Infrastructure Act 2016 (Act) to give notice of the application to which it relates.

Regardless of the public notification triggers (discussed below), it is understood that the Council Assessment Panel is the relevant authority for the purpose of decision making.

Public Notification

Pursuant to Section 107(6) of the *Planning, Development and Infrastructure Act 2016*, notification will be required given that an indoor recreation facility is not listed in Table 5 of the General Neighbourhood Zone and the site is adjacent land used for residential purposes in a neighbourhood-type zone, thus not meeting the requirements of Table 5 of the Urban Activity Zone.

Referrals

The site is subject to various overlays and other legislative requirements that could require a referral to a body prescribed by the Regulations. These are summarised below:

Table 3 Referral Snapshot

Referral trigger	Referral required?	Comments
Advertising Near Signalised Intersections	Yes	The signage is proposed to be illuminated. Accordingly, a referral is required.
Future Road Widening	Yes	A portion of the development will be located within the 6 metre Consent Area. Accordingly, a referral is required.
Major Urban Transport Routes	No	The proposal crossover is setback 25 metres from a State Maintained Road. Accordingly, a referral is not required.
Prescribed Wells Area	No	The site is connected to mains water and does not require the taking of water allocated under the <i>Landscape South Australia Act 2019</i> . Accordingly, a referral is not required.

Traffic Generating Development	No	The proposed development will not exceed a commercial gross floor area of 10,000 square meters or more. Accordingly, a referral is not required.
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The Merits

We have concluded from our assessment that the proposed development satisfies the vast majority of the relevant policies of the Code and is, therefore, worthy of consent. Our detailed assessment is set out below.

Land Use

The proposed land use is expressly envisaged in Designated Performance Feature (DPF) 1.1 of the Urban Activity Centre (UAC) Zone and will contribute to a "broad spectrum of regional-level business, shopping, entertainment, and recreational facilities that can cater for large crowds, smaller social gatherings, and events over extended hours," in line with Desired Outcome (DO) 1 of the UAC Zone.

Accordingly, the proposal is considered an entirely appropriate land use within the UAC Zone. The proposed use can operate over extended hours, reinforcing the role of the centre as a key focus for social activity.

The remaining 39 percent of the site is situated within the General Neighbourhood (GN) Zone and primarily seeks to accommodate low and medium density residential development that supports a variety of needs and lifestyles, located within convenient proximity to services and facilities, as outlined in DO 1 of the GN Zone.

While the proposed land use is not explicitly listed as envisaged development under GN DPF 1.1, the corresponding PO allows for complementary non-residential uses that support an active, convenient, and walkable neighbourhood. In this context, the proposal is considered to be a suitable form of development that aligns with the intent of GN PO 1.1.

It is also worth noting that the proposed land use has previously received planning approval, albeit in a broader form. The current proposal is now solely for an indoor recreation facility. As such, a level of planning acceptance has already been established, particularly as the revised proposal does not materially alter the previously approved land use or the underlying planning rationale that supported it.

While some ancillary land uses have been removed due to the scaled-back nature of the variation, the core use as an indoor recreation facility remains consistent with the original approval.

Built Form and Character

Built form and character provisions are largely underpinned by the UAC Zone (See PO 2.1, 2.2, 2.3, 2.4, 2.5 and 2.6). The proposed development is considered to align with the design and appearance expectations expressed, namely:

- the proposal presents a visually interesting built form outcome to the public realm, through the following design elements:
 - » the two-storey building presents itself as the gateway to the Elizabeth City Centre, activating a vacant parcel of land which presents prime opportunity for development within the activity centre;

- » incorporating a variety of textures and durable materials to add sufficient visual interest and articulation to the adjoining roads, specifically its frontage to Main North Road and Philip Highway;
- » Inclusion of feature LED strip lights within the façade to further deepen the visual interest and activate surrounding road frontages; and
- » providing architectural projections, awnings, openings and tall windows to create modulation and distinguishable entry points facing Ashfield Road.
- the proposed design is reflective of existing retail, commercial and bulky good outlet buildings within the locality;
- the building comprises large tenancy footprints that are capable of being adapted for a range of shopping, entertainment and recreation facility activities in the future;
- the design presents an inclusive, pedestrian friendly design as:
 - » the main entry areas are appropriately sited at the front of the building to provide direct access from the car parking areas;
 - » the proposal seeks to integrate with the southern edge of the Elizabeth City Centre by providing recreational activities that support the function of the activity centre as place to meet people and experience a range of goods and services;
 - » safe, convenient and equitable access is provided to disabled users through provision of a disabled space located directly adjacent individual tenancies;
 - » vehicle access points and car parking area are coordinated and consolidated to enable the shared use of parking spaces; and
 - » the open at-grade carpark promotes visual permeability of the site whilst optimising security and safety both internally and externally from the public realm.
- the proposal provides landscaped areas that contribute towards appropriate water sensitive urban design outcomes; and
- provision of landscaping garden beds that have been designed to accommodate a number of the existing trees and new tree plantings where possible and optimise soft landscaping opportunities throughout (as sought PO 3.2 of the Design module).

Accordingly, the built form and character provisions of the UAC Zone are considered satisfied.

Building Height

PO 3.1 of the UAC Zone envisages a building height consistent with the form expressed in any relevant Technical Numeric Variation (**TNV**) or is generally medium-rise development, with the highest intensity of built form at the centre of the zone, and lower scale at the peripheral zone interface.

Given that the subject site fronts Ashfield Road, PO 3.4 of the UAC Zone is also relevant and states:

PO 3.4 *Buildings on an allotment fronting a road that is not a State maintained road, and where land on the opposite side of the road is within a neighbourhood-type zone, provides an orderly transition to the built form scale envisaged in the adjacent zone to complement the streetscape character.*

Furthermore, building height is also managed through the interface height policies and building envelopes PO 3.2 and 3.3 in the UAC Zone.

With respect to the GN Zone, the following applies:

PO 4.1 Buildings contribute to a low-rise suburban character.

The proposed development has a total building height of 13.1 metres (which has decreased by 3.1 metres from the previous approval) and includes a mezzanine level, presenting as a two-storey built form. For the purposes of this assessment, the development is described as two storeys. The height is considered appropriate in the context of the site, with the following key points noted:

- There are no TNVs applicable under the UAC Zone;
- The building comprises two levels and therefore qualifies as 'low-rise' under Part 8 of the Code, satisfying PO 4.1 of the GN Zone;
- The building is well setback into the site and fronts both Main North Road and Philip Highway, two major arterial roads, creating a suitable urban context. The two-storey built form is appropriately scaled for this frontage, and the adjacent car parking area provides a buffer to the residential dwellings located to the west;
- The proposal offers an appropriate transition in built form and scale between the Elizabeth City Centre and the adjoining bulky goods developments to the north and west of the site; and
- The UAC Zone is designed to support higher-intensity built form, including an uplift in building height. The proposed development aligns more closely with the intended character and policy outcomes of the UAC Zone than those of the GN Zone. Accordingly, the provisions of the UAC Zone are considered more applicable in this context. Nevertheless, due consideration has been given to minimising potential impacts on sensitive receivers within the GN Zone, acknowledging that the site remains subject to the GN Zone provisions regardless of the proposed land use.

Further to the above, the proposal is located approximately 60 metres from the nearest residential dwellings to the west and from an allotment being used for residential purposes. This substantial separation ensures the development does not result in unreasonable impacts in terms of scale, visual massing, or overshadowing, and therefore does not offend PO 3.2 or PO 3.3 of the UAC Zone.

It is also noted that the southern side of the site adjoins a vacant allotment that is not currently used for residential purposes. As such, the interface height provisions for this elevation are not considered applicable in this context.

Additionally, the building height is not considered to pose a hazard to the operation of Defence Aviation Areas and is adequately separated from runways and other take-off and landing facilities in accordance with the expectations of the Building Near Airfields and Defence Aviation Area Overlays.

Setbacks

The UAC Zone is intentionally silent on specific setback requirements, as these are instead addressed through interface height policies and building envelope controls. In this context, and as outlined above, the siting and setbacks of the proposed development are considered to satisfy the intent of the UAC Zone.

While PO 5.1, 6.1, 7.1, 7.2, 8.1, and 9.1 of the GN Zone refer to setback requirements, these provisions primarily relate to residential development outcomes. As such, they are not considered highly relevant to a commercial development of this nature. Nonetheless, the proposed separation from residential properties to the west ensures that the development will not adversely impact the suburban streetscape character or the existing residential context in that locality. Thus, achieving the underlying intent of the aforementioned setback provisions while also improving upon the setbacks and separation from residential development as previously approved.

Advertisement

The UAC Zone outlines the following objectives:

***PO 5.1** Advertisements are sited and designed to achieve an overall consistency and appearance along individual street frontages.*

The GN Zone states:

***PO 12.1** Advertisements identify the associated business activity, and do not detract from the residential character of the locality.*

It is important to note that most of the advertisement policy is detailed in the Advertisements Module of the Code. Additionally, the development is subject to a referral to the Commissioner of Highways due to the signs' illumination and proximity to a signalised intersection.

The four proposed signs will:

- reflect a consistent and cohesive appearance along street frontages, particularly in relation to the site's integration with the broader UAC and shopping centre precinct (in line with PO 5.1 of the UAC Zone);
- appropriately identify the associated business activity without detracting from the residential character of the area (in line with PO 12.1 of the GN Zone);
- be affixed to the primary façade and rear of the building, integrated into the building design, and positioned below the roofline to maximise visibility without protruding (in accordance with PO 1.1 of the Advertisement Section of the Code);
- be fully contained within the allotment boundaries (in accordance with PO 1.3 of the Advertisement Section of the Code);
- be of a size and scale that is appropriate to the building and site, as well as to the retail and commercial character of the locality (in accordance with PO 1.5 of the Advertisement Section of the Code);
- ensure the sign area is well below the recommended maximum of 15% of the building facade, avoiding visual clutter and excessive branding (in accordance with PO 2.3 of the Advertisement Section of the Code);
- be illuminated, but will not flash, move, or change displays or messages (in line with PO 5.2 and 5.6 of the Advertisement Section of the Code). The Proponent is amenable to a condition of approval controlling the maximum allowable brightness levels to ensure compliance with safety requirements from the Commissioner of Highways and relevant Code policies; and
- given the separation from adjoining sensitive receivers and the low illumination levels associated with the signage, light spill is expected to be minimal and can be effectively controlled through conditions of approval (in accordance with PO 4.1 of the Advertisement Section of the Code).

Traffic, Access and Car Parking

A Traffic and Parking report prepared by CIRQA is included in **Appendix 4**, and the key points are addressed below.

Traffic Generation

The proposed development is anticipated to have the forecasted traffic volumes:

- am weekday peak hour – 47 trips;
- pm weekday peak hour – 61 trips; and
- Saturday afternoon peak hour – 185 trips.

The revised forecasts represent an approximate 15% reduction in traffic generation compared to the previously approved development.

The original traffic and parking report, including detailed impact analysis and modelling using SIDRA intersection software, concluded that the traffic volumes associated with the earlier proposal could be adequately accommodated by the surrounding road network.

Given that the current proposal will generate less traffic, the associated impacts will be reduced. The forecasted (lower) traffic volumes are therefore expected to be readily accommodated by the adjacent road network.

Road Widening

The northern portion of the building falls within the 6-metre Consent Area under the Future Road Widening Overlay. Procedurally, the application will trigger a referral to the Commissioner of Highways. However, it is understood that this section of the site has already been severed under the recently approved land division to accommodate the future road widening requirements, ensuring efficient delivery of this infrastructure.

Car Parking

The site is identified as being located within a 'Designated Area', and the car parking rates under *Table 2 – Off-Street Car Parking Requirements in Designated Areas* is applicable. While it is acknowledged that a higher rate applies to the portion of the allotment located within the GN Zone, given that the majority of the site is within a 'Designated Zone' and that there would be negligible difference in accessibility considerations, it is considered appropriate that the development be assessed as being wholly within a 'Designated Area'.

On this basis, there would be a DPF expectation that approximately 221 parking spaces are associated with the development.

PO 5.1 of the Transport, Access and Parking module of the Code states:

PO 5.1 *Sufficient on-site vehicle parking and specifically marked accessible parking places are provided to meet the needs of the development or land use having regard to factors that may support a reduced on-site rate such as:*

- a) availability of on-street car parking*
- b) shared use of other parking areas*
- c) in relation to a mixed-use development, where the hours of operation of commercial activities complement the residential use of the site, the provision of vehicle parking may be shared*
- d) the adaptive reuse of a State or Local Heritage Place.*

In consideration of PO 5.1, the following points are noted:

- the Code allows for flexibility in parking provisions, permitting lower parking rates than the specified guidelines, based on development and land use factors;
- the temporal distribution of peak demand will vary across different components of the development. For example, gyms typically generate peak demand early in the morning and in the mid-to-late evening on weekdays, while the ice arena is likely to have peak demand on Saturdays. While the tenant for Tenancy 2 is not yet confirmed, other indoor recreational uses could lead to peak periods different from those of the gym and ice arena. As a result, the overall parking demand for the site is expected to be lower than a purely theoretical assessment;
- the site is located within a 10-minute walk from the Elizabeth Train Station, with frequent bus services available along Philp Highway, Playford Boulevard, and Yorktown Road;
- the site is also in close proximity to the Windsor Car Park, a purpose-built facility designed to accommodate surrounding developments, with available capacity to meet a portion of the site's parking demands; and
- Council has previously accepted a degree of reliance on off-street parking, and it is understood that this approach remains acceptable.

Notably, the previous proposal included a parking provision rate of 2.22 spaces per 100 square meters of total floor area. In comparison, the current proposal increases the provision to 2.44 spaces per 100 square meters, representing an overall improvement. This alone supports a more favourable parking outcome.

It is also worth highlighting the inclusion of dedicated minibus parking, which provides an alternative transport solution where ride-sharing may be required.

Bicycle Parking

The Code identifies the following rate:

- Indoor recreation facility – 1 space per 4 employees plus 1 space per 200 square metres of gross leasable floor area for visitors

At this stage, the exact number of staff for the facility is unknown. Therefore, an assumption of 30 staff members has been applied, which results in a total requirement of 44 bicycle parking spaces.

The proposal currently includes 22 bicycle parking spaces, the theoretical requirement based on the Code's rate is considered overly conservative, particularly when taking into account the varying peak demand periods for the different tenants.

As such, the provision of 22 on-site bicycle parking facilities is considered appropriate and will encourage cycling as an active transport mode, satisfying PO 9.1 of the Transport, Access and Parking Module.

Waste Management

The proposed waste management strategy, outlined in the report prepared by CIRQA within **Appendix 5**, effectively addresses the management of waste and refuse generated by the various waste streams.

Key points regarding waste management include:

- The different tenancies have been considered in terms of the nature of activities, expected waste generation, and frequency of collection, ensuring that each site is adequately serviced (see Design in Urban Areas, PO 11.1);

- Waste storage and management areas are appropriately screened from public view through the use of fencing and landscaping (see Design in Urban Areas, POs 1.5 and 11.2);
- The waste storage areas are designed to be well-ventilated and situated away from residential uses, promoting safety and hygiene (as per Design in Urban Areas, PO 11.3); and
- The waste storage area is positioned to allow a waste truck to enter and exit the site in a forward motion, ensuring smooth and efficient waste collection (consistent with Design in Urban Areas, PO 11.4).

Stormwater Management

The stormwater management plans prepared by FMG Engineering is contained within **Appendix 3**.

The Design in Urban Areas module encourages water sensitive design and seeks to mitigate stormwater discharge from the building and surface water impacts to be equivalent to, or better than its pre-developed state. In particular, PO 42.3 of the Design in Urban Areas section of the Code states:

PO 42.3 *Development includes stormwater management systems to mitigate peak flows and manage the rate and duration of stormwater discharges from the site to ensure that development does not increase peak flows in downstream systems.*

With respect to the management of stormwater, we wish to highlight that:

- post-development stormwater flows have been designed to not exceed the predevelopment flow, with a design incorporating an underground detention tank which will be located within the carpark;
- In accordance with Councils requirements, discharge from the site has been restricted from 1% AEP (100-year ARI) and 10% AEP storm event post-development peak flows to the equivalent pre-development peak flows;
- The proposed development will have an FFL of 34.00m AHD and all verges around the building will be adjusted to ensure the land does not grade towards the building. Noting that the elevated floor level will not result in significant additional earthworks; and
- WSUD has been considered via all new development catchment being controlled runoff, mitigating potential contaminants.

Tree Removal

The Regulated and Significant Tree Overlay seeks to conserve regulated and significant trees to provide aesthetic and environmental benefits and mitigate tree loss.

With respect to the proposed tree-damaging activity in the form of removal of a total of eight regulated trees and two significant tree, we make the following assertions having regard to PO 1.4 of the Overlay:

- it is in connection with, and accommodates the reasonable development of land in accordance with the outcomes sought by the UAC and GN Zone;
- given the built form and intensity of land uses sought by the UAC Zone, the outcomes would otherwise not be possible; and
- due to the disbursement of trees across the site, it is not considered that all trees could be practically retained while providing for the reasonable development of the site. It is noted that where possible, trees along the site boundaries are to be retained, which provides for the highest level of visibility to the public along site boundaries; and
- with respect to the significant trees, it is further noted that:

- » all reasonable development options and design solutions been considered to prevent substantial tree-damaging activity occurring, however not feasible due to the location of sealed surfaces and the car parking present constraints to the site and limiting incursions into the tree's Notional Root Zone were unachievable.

Noise

An environmental noise assessment has been prepared by Sonus (refer to **Appendix 7**), with the key findings summarised below.

Appropriate noise criteria have been established in accordance with the South Australian *Planning and Design Code*, the *Environment Protection (Commercial and Industrial) Policy 2023*, and the EPA *Guidelines for Assessing Music Noise from Indoor Venues*.

The assessment concludes that the proposed development will comply with the relevant noise criteria, provided the recommended acoustic treatments are implemented. These treatments include:

- use of specific roof and ceiling construction for the gym;
- installation of specified glazing to the gym façade;
- construction of an airlock at the gym entry;
- directional orientation of the ventilation louvres for the ice arena plant room;
- strategic placement of roof-mounted mechanical plant;
- installation of rooftop plant screens constructed from acoustic louvres (or a solid barrier) with defined height requirements;
- specification of minimum performance standards for acoustic louvres;
- installation of acoustic absorption material to the underside of the plant room soffit; and
- construction of a boundary fence with defined acoustic performance.

Subject to the implementation of these noise mitigation measures, the Sonus report concludes that the proposed development will not unreasonably impact the amenity of nearby sensitive receivers. As such, the development is considered to satisfy the relevant environmental noise provisions of the Code.

Lighting

The proposal is located within the Concept Plan 81 – Edinburgh Defence Airfield Lighting Constraints.

Outdoor lighting is yet to be determined; however, it is anticipated to be installed for security purposes and use of the carpark at nighttime for the gym. Any lighting is to be in accordance with *Australian Standard 4282 – 1997 'Control of the obtrusive effects of outdoor lighting'* to ensure there is no unreasonable light spill to adjoining properties or the amenity of the locality.

Similarly, by meeting the relevant Australian Standards for outdoor lighting, it will not pose a hazard to the aircraft operations of the nearby RAAF Base and will not cause any adverse upward light meeting PO 1.1. of the Building Near Airfields Overlay and expectations of Concept Plan 81.

Summary

If you have any queries or concerns regarding the proposed development, please do not hesitate to contact me.

Yours sincerely,



Laura Goulden
Consultant

ICE ARENA, PLAYFORD, SA

PLANNING DOCUMENTAION SEPTEMBER 2025

Pelligra

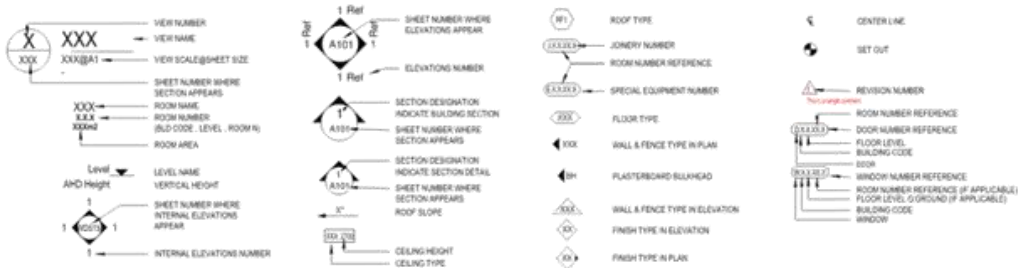


LOCATION PLAN

1 : 4000@A1

DRAWING REGISTER	
SHEET NO.	SHEET NAME
000001	COVER SHEET
000002	CONCEPT
000003	EXISTING / DEMOLITION SITE PLAN
000004	GROUND FLOOR PLAN
000005	LEVEL 1 FLOOR PLAN
000006	ROOF PLAN OVERALL
000007	ELEVATIONS
000008	SECTIONS

SYMBOL LEGEND



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DATE	DESCRIPTION	DESIGNED BY
24.09.2025	REVISION FOR PLANNING APPROVAL	JD
24.09.2025	REVISION FOR PLANNING APPROVAL	JD
27.09.2025	FOR REVIEW	CS
28.09.2025	FOR REVIEW	CS

ORIENTATION

CLIENT
PELLIGRA

Pelligra

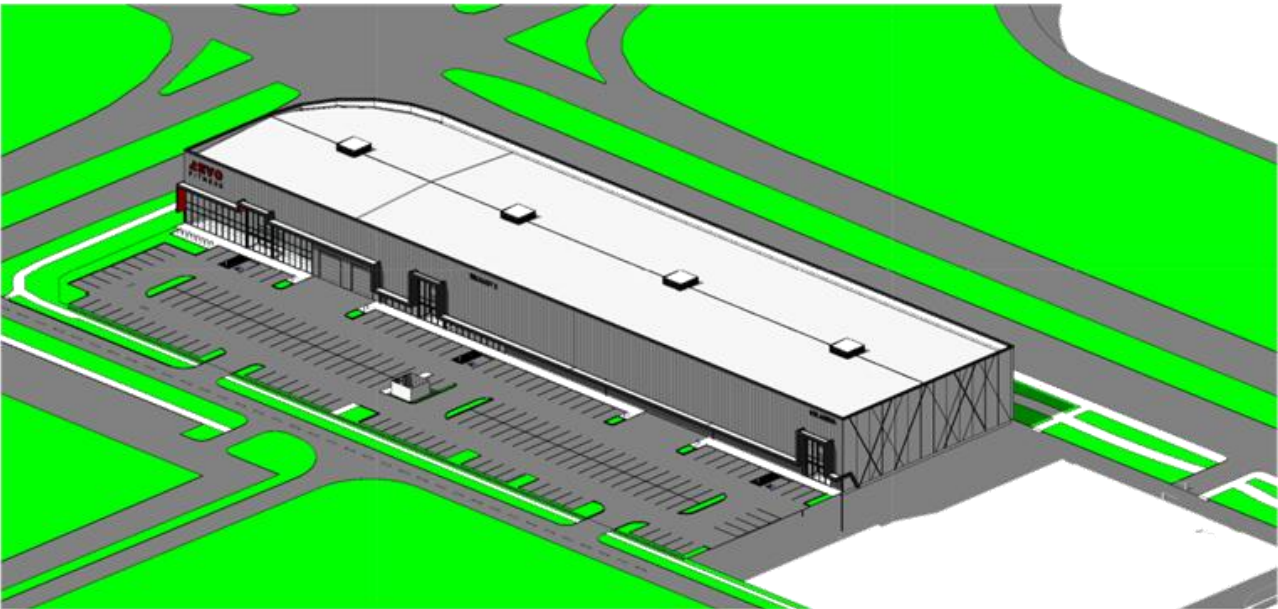
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BELL

JOB TITLE
LOT 12, PELLIGRA ICE ARENA, PLAYFORD, SA

DRAWING TITLE
COVER SHEET

JOB NO.	DRAWING SCALE @ A1	DRAWN BY	CHECK BY	DRAWING NO.	REVISION
BAA250010	As indicated	CS	JD	DD00001	D



SOUTH WEST 3D



NORTH EAST 3D

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NO.	DATE	DESCRIPTION	BY
1	24.09.2023	ISSUE FOR PLANNING APPROVAL	JD
2	24.09.2023	ISSUE FOR PLANNING APPROVAL	CS
3	27.09.2023	100% REVIEW	CS
4	28.09.2023	100% REVIEW	CS

DATE

28.09.2023

BY

JD

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DIAGRAMS

JOB NO

BAA250010

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CS

CHEK BY

JD

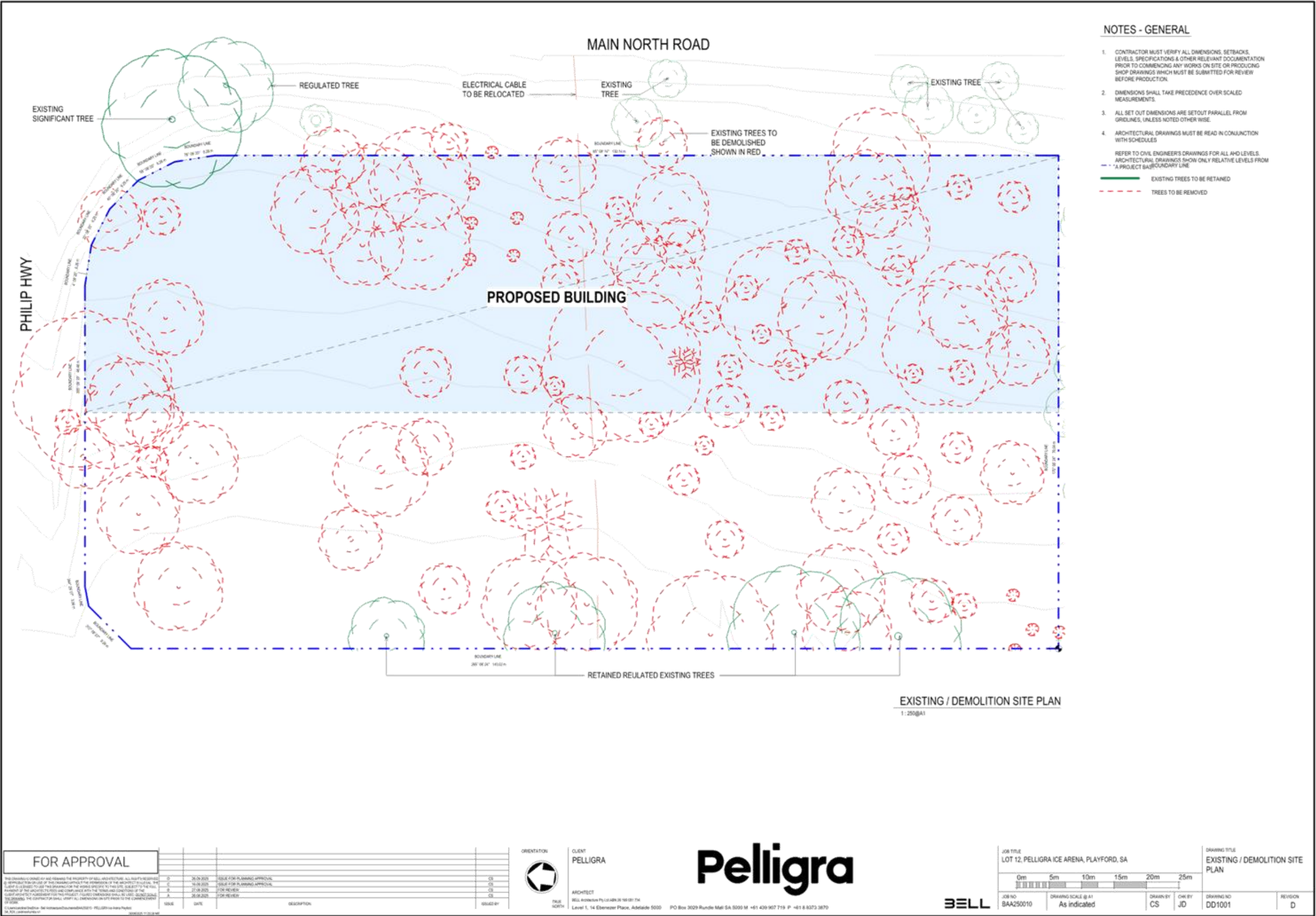
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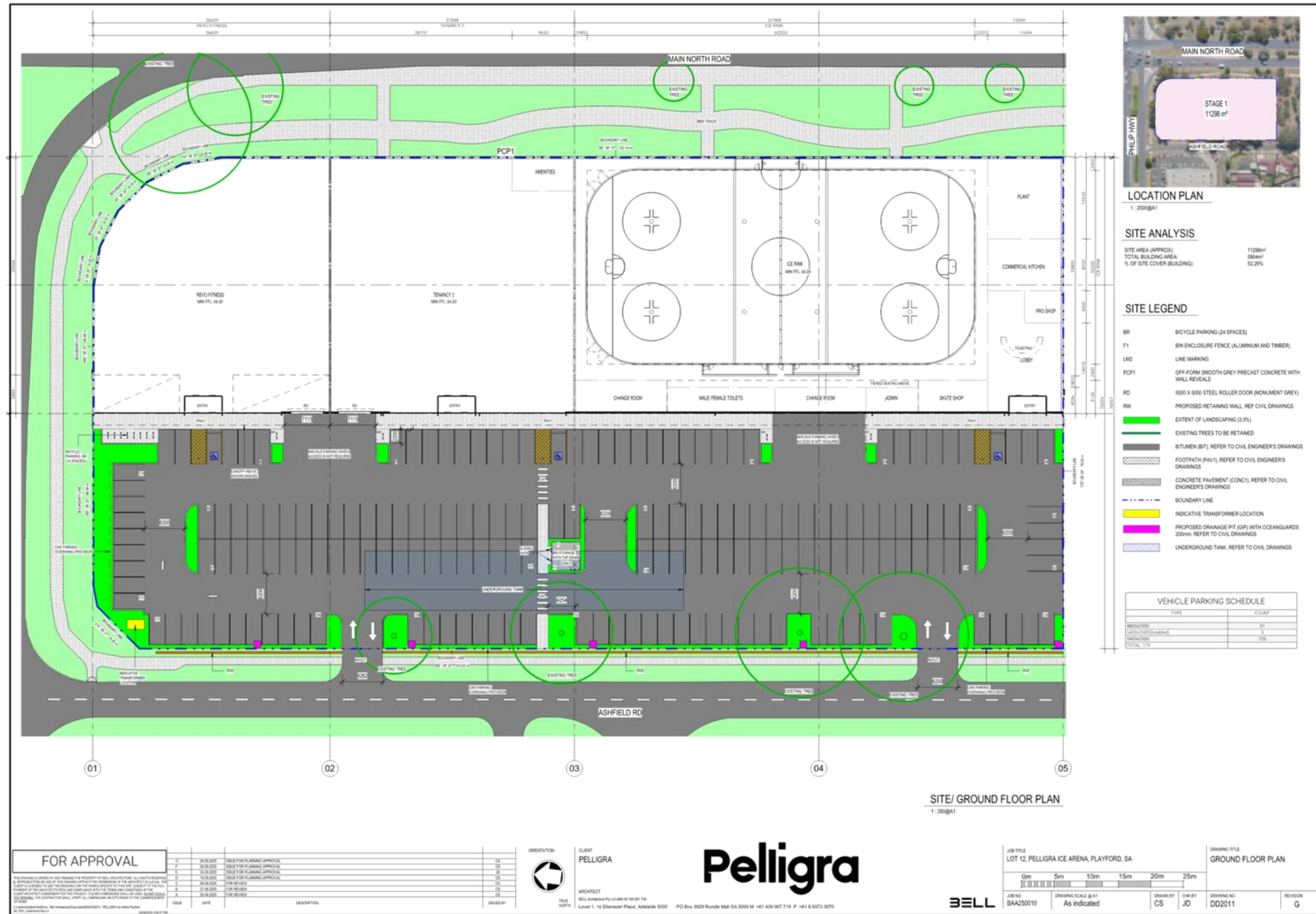
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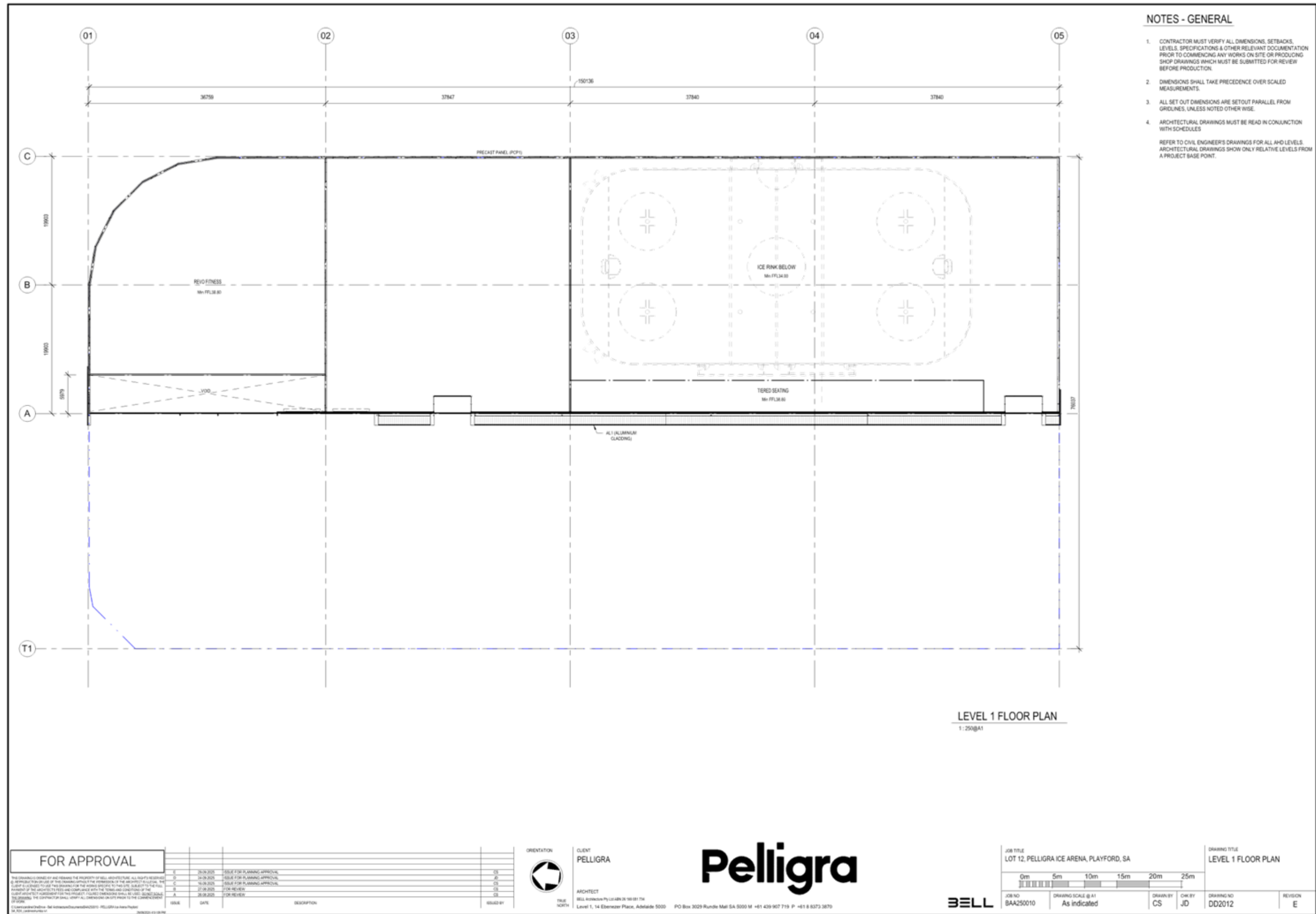
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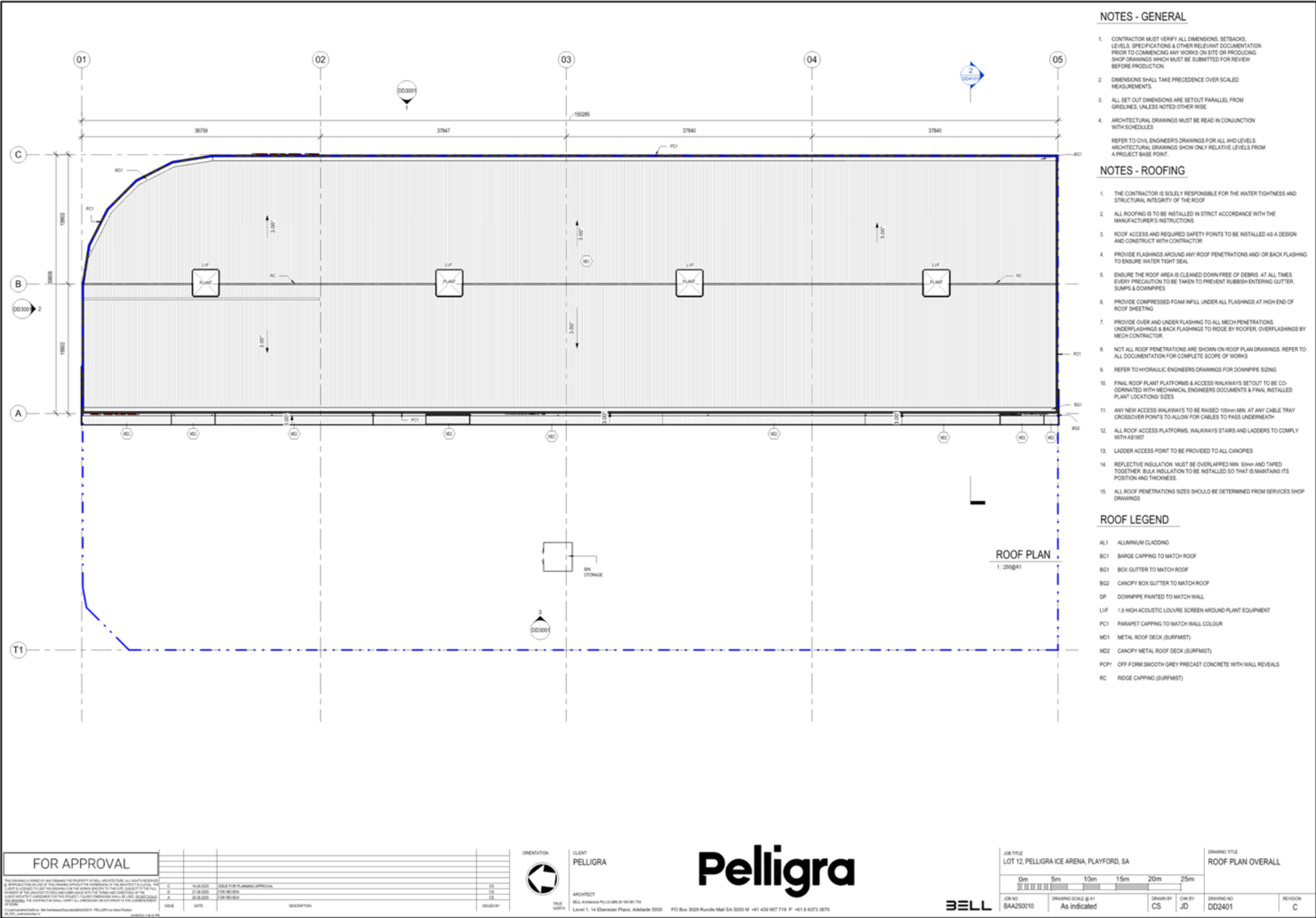
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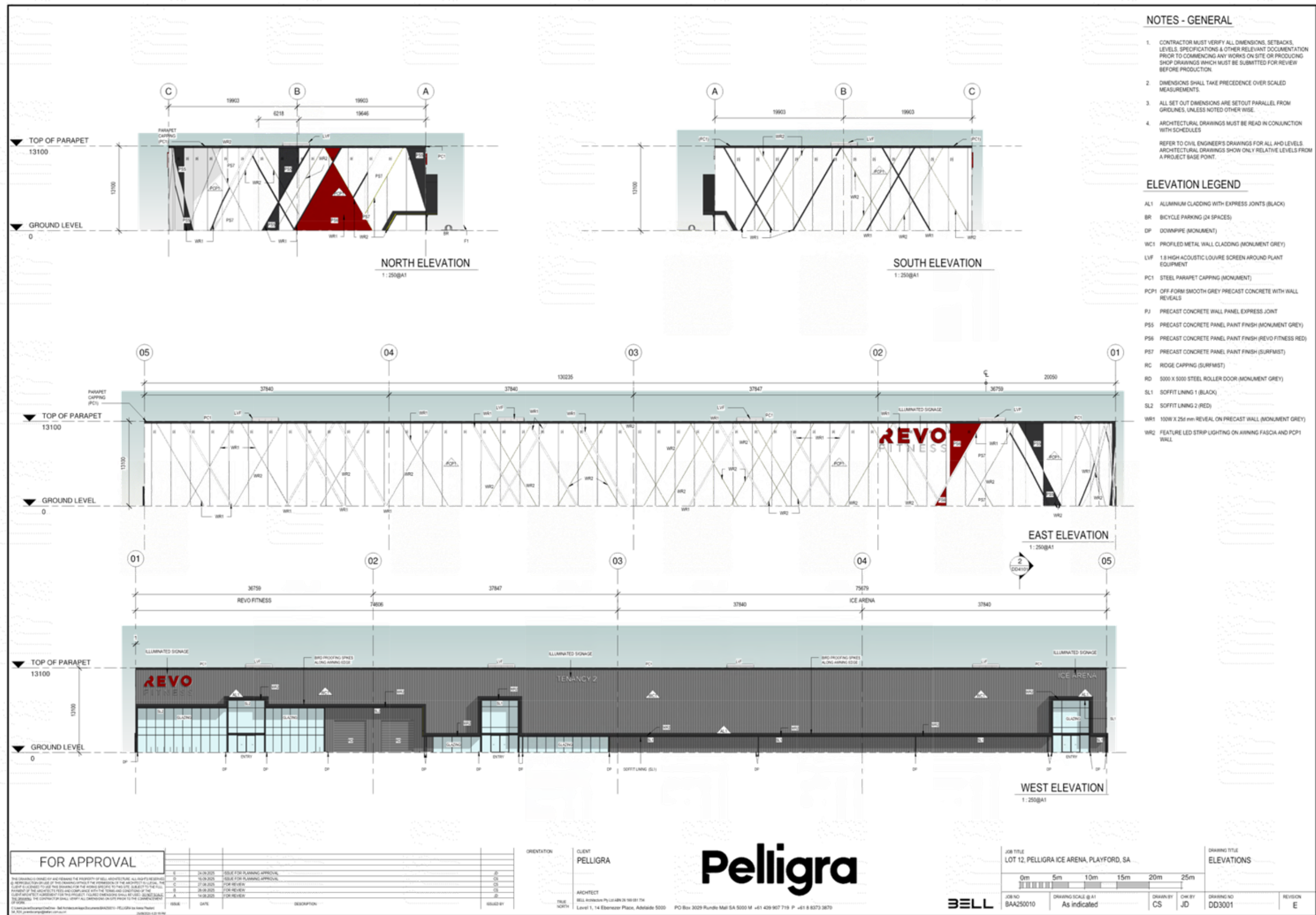
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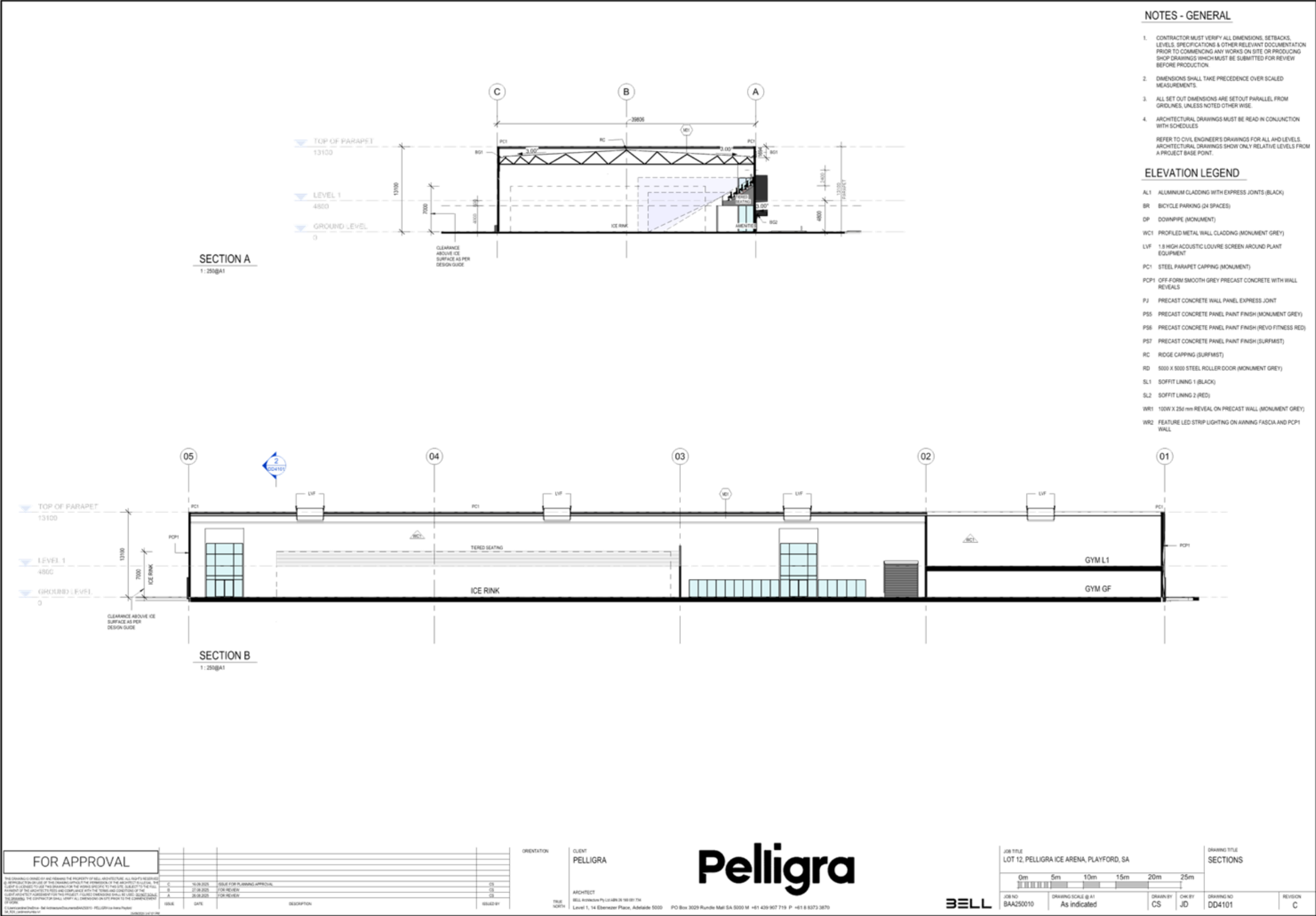


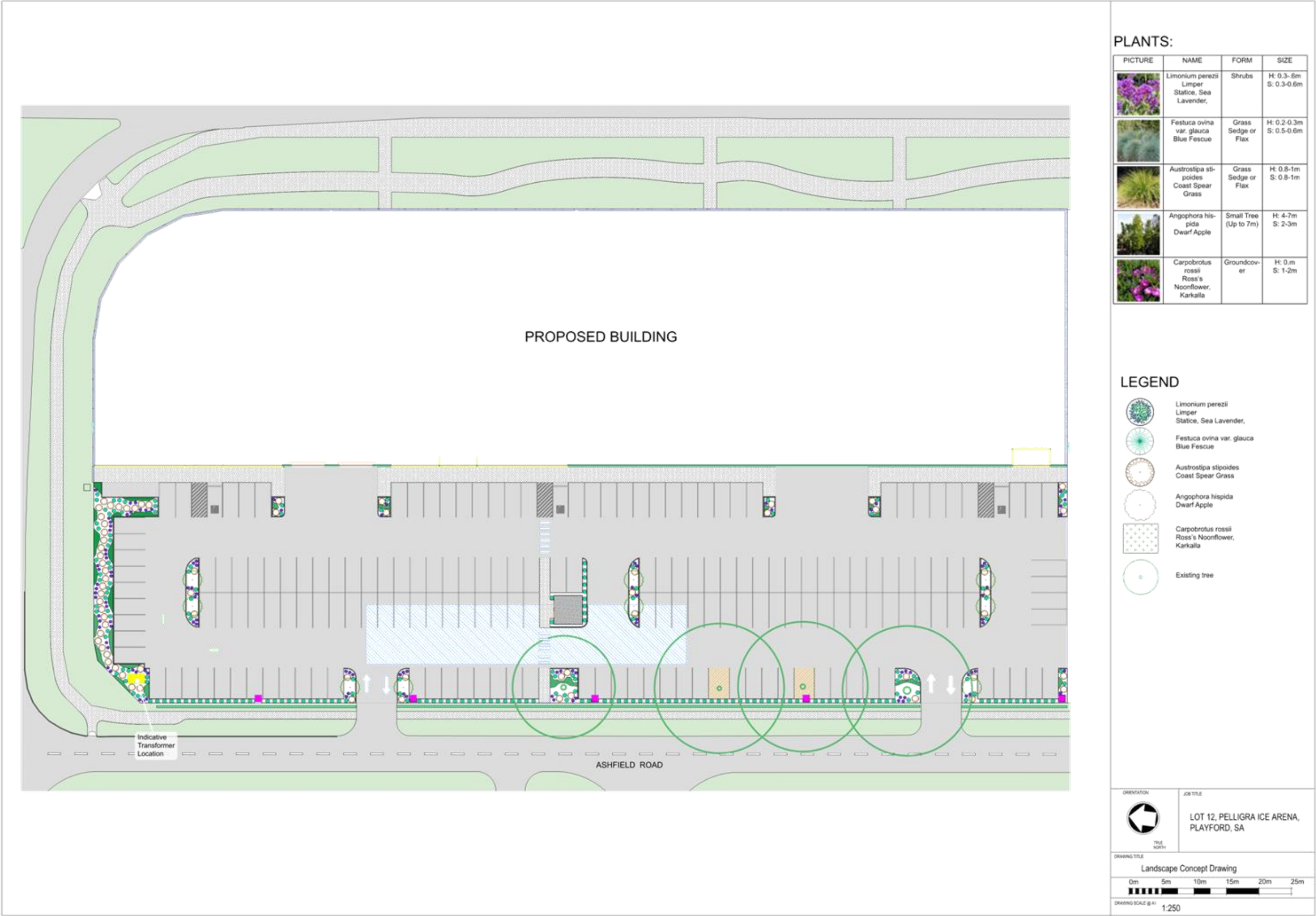














PLAYFORD ICE ARENA
ASHFIELD ROAD, ELIZABETH
TRAFFIC AND PARKING REPORT





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Version	Date	Details/status	Prepared by	Approved by
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V1.1	25 Sep 25	Minor update	BNW	BNW

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1. INTRODUCTION

CIRQA has been engaged to provide design and assessment advice for the proposed Playford Ice Arena and Entertainment Precinct at Ashfield Road, Elizabeth. Specifically, CIRQA has provided advice in respect to the traffic and parking aspects of the proposal.

This report provides a review of the subject site, the proposed development, its access and parking provisions and the associated traffic impact on the adjacent road network. The traffic and parking assessments have been based upon plans prepared by BELL Architecture (drawings no. DD2011 and DD2012, dated 16 September 2026).

2. BACKGROUND

2.1 SUBJECT SITE

The subject site is located within Lot 101 Ashfield Road, Elizabeth. The site is bound by Ashfield Road to the west, Philip Highway to the north, a public reserve strip to the east and south. Main North Road is located east of the adjacent reserve. Figure 1 illustrates the location of the subject site with regard to the adjacent road network.



Figure 1 - Location of the subject and adjacent road network



The Planning and Design Code identifies that the site is located within two zones, primarily an Urban Activity Centre Zone for the northern two-thirds (approximate) and a General Neighbourhood Zone for the remaining southern portion. The following Overlays are applicable:

- Advertising Near Signalised Intersections;
- Building Near Airfields;
- Defence Aviation Area (All structures over 15 metres);
- Future Road Widening;
- Hazards (Flooding - General);
- Major Urban Transport Routes;
- Prescribed Wells Area;
- Regulated and Significant Tree;
- Stormwater Management;
- Traffic Generating Development; and
- Urban Tree Canopy.

2.2 ADJACENT ROAD NETWORK

Ashfield Road is under the care and control of the City of Playford. The SA Government's Location Viewer identifies that Ashfield Road is classed as a 'collector road'. In the vicinity of the site, Ashfield Road comprises a two-way carriageway with a width of 7.2 m. The general urban speed limit of 50 km/h applies on Ashfield Road. Adjacent the site, Ashfield Road forms a T-intersection with Willcox Road (with the northern and western legs assigned priority). Available traffic data indicates that the northern end of Ashfield Road currently accommodates in the order of 800 daily vehicle movements (well below the capacity of a collector road).

Main North Road is a primary arterial road under the care and control of the Department for Infrastructure and Transport. Adjacent the site, Main North Road comprises three traffic lanes in each direction, separated by a 16 m wide median. Parking is not permitted on Main North Road. Traffic data obtained from DIT indicates that this section of Main North Road has an Annual Average Daily Traffic (AADT) volume in the order of 52,000 vehicles per day (vpd). Adjacent the site, an 80 km/h speed limit applies on Main North Road.

Philip Highway is an arterial road under the care and control of DIT. In the vicinity of the site, Philip Highway comprises three traffic lanes in each travel direction plus additional turn lanes at its intersections with Playford Boulevard and Main North Road. DIT data indicates that Philip Highway accommodates 21,200



vehicles per day. To the south-east of the subject site, Philip Highway interests with Main North Road (and Yorktown Road) at a four-way signalised intersection.

Ashfield Road forms a T-intersection with Philip Highway (with the Highway assigned priority). Due to the presence of the raised median on Philip Highway (and right turn lane provided for its intersection with Playford Boulevard), movements into and out of Ashfield Road are restricted to left turn movements only. Drivers seeking to exit Ashfield Road to Main North Road either need to utilise a U-turn facility on Philip Highway (west of Ashfield Road) or head south on Ashfield Road and utilise the signalised intersection of Fairfield Road/Main North Road/Midway Road.

2.3 WALKING AND CYCLING

The site is well serviced by footpaths including paths located within the verge/reserve adjacent Main North Road (east of the site), paths on both sides of Philip Highway and a path on the western side of Ashfield Road with connections to paths on other surrounding roads. Signalised crossing movements are accommodated on all legs of the intersection of Main North Road and Philip Highway. Cyclists are also able to utilise the paths (no formal on-road bicycle facilities are provided in the immediate vicinity of the site).

2.4 PUBLIC TRANSPORT

Regular public (bus) transport services operate in close proximity to the site. Bus stops are provided on a number of roads within 400 m of the subject site and serviced by the following services:

- J1 – Elizabeth Interchange to Adelaide Airport & Glenelg;
- 224/224F/224X – Elizabeth Interchange to City;
- 400 – Elizabeth Interchange to Salisbury North;
- 430 – Elizabeth Interchange to Salisbury Interchange;
- 440 – Munno Para Station to Elizabeth Interchange;
- 441 – Smithfield Interchange to Elizabeth Interchange;
- 442 – Smithfield Interchange to Elizabeth Interchange;
- 443 – Elizabeth Interchange to Munno Para Loop Service;
- 451 – Munno Para Centre Interchange to Elizabeth Interchange;
- 452 – Munno Para Centre Interchange to Elizabeth Interchange;
- 482 – Elizabeth Interchange to Roma Mitchell Secondary College;
- 483 – Elizabeth Interchange to Our Lady of Sacred Heart College;
- 560 – Elizabeth Interchange to Tea Tree Plaza Interchange; and



- N224 – After Midnight Saturday PM – Sunday AM Gawler to City.

In addition, the Elizabeth (bus and rail) interchange is located approximately 770 m west of the site. The station would be within approximately 10 minutes (or less) from the proposed development and be a viable option for access (particularly for staff).

2.5 PREVIOUS DEVELOPMENT APPROVAL

A development application for an ice arena and entertainment precinct was previously lodged and subsequently received planning consent for the subject site. The previously approved proposal included a variety of uses including an ice arena, a play café, a rock climbing facility, basketballs courts, a gym and food and beverage tenancies (with a total floor area of approximately 10,630 m²). The original proposal was approved with two access points via Ashfield Road and with a total of 236 on-site (off-street) parking spaces. A previous traffic and parking report was prepared by CIRQA in support of the original (approved) application. The approved proposal has not proceeded.

3. PROPOSED DEVELOPMENT

3.1 LAND USE AND YIELD

Subsequent to the receipt of development consent for the original application, a revised proposal has been prepared for the subject site. The revised development proposal represents a reduced extent of development compared to the previously approved scheme. Specifically, the revised proposal comprises the construction of an indoor recreation facility with the following components:

- a 3,307 m² ice arena (including first floor seating area);
- a 2,535 m² gym; and
- a 1,500 m² indoor recreation tenancy.

As with the previous proposal, it is anticipated that the ice arena will host infrequent events (up to 14 times a year). However, compared to the previous proposal, the current (revised) proposal has much reduced spectator seating provisions (approximately 42% of the upper floor seating area compared to the previous proposal).

3.2 ACCESS AND PARKING DESIGN

As previously approved, vehicle access is proposed via two access points on Ashfield Road (noting the site has no direct frontage to Main North Road and limited frontage to Philip Highway once restrictions associated with adjacent intersections are considered). One access is proposed north of the intersection



of Ashfield Road with Willcox Road and the other south of the intersection. The positions of the access points will conform with the location requirements of the Australian/New Zealand Standard, *Parking Facilities Part 1: Off-street car parking* (AS/NZS 2890.1:2004) in that they will not be opposite existing intersections. The dimensions of the access points will also conform with AS/NZS 2890.1 in that they will have a minimum width of 6.0 m (albeit widening/flaring should be confirmed during detailed design to accommodate service and delivery vehicle movements).

The access points will accommodate movements to and from the site's parking areas. These include a 179-space at-grade parking area. In addition, parallel bays will also accommodate mini-buses and service/delivery vehicles (when access to adjacent roller doors is not required, which will be infrequent).

Notably, the site is also within close walking distance of the Windsor (public off-street) car park which could accommodate a portion of demands associated with the site (particularly on weekends when its use by surrounding commercial land uses is reduced compared to weekdays). Further discussion in respect to parking conditions associated with the site is provided in Section 4.1 below.

The design of the parking area conforms with the requirements of Australian/New Zealand Standard, *Parking Facilities Part 1: Off-street car parking* (AS/NZS 2890.1:2004) and Australian/New Zealand Standard, *Parking Facilities Part 6: Off-street parking for people with disabilities* (AS/NZS 2890.6:2009) in that:

- regular parking spaces will be at least 2.5 m wide and 5.4 m long (or 4.8 m long with 0.6 m overhang);
- the parking spaces for use by persons with disabilities will be 2.4 m wide and 5.4 m long (with an adjacent shared space of the same dimension);
- parking aisles will be 6.2 m wide;
- 1.0 m end-of-aisle extension will be provided beyond the last parking space in blind aisles;
- 0.3 m clearance will be provided to all objects greater than 0.15 m in height or the car clearance envelope identified in the Standard shall be clear (including allowance for columns); and
- pedestrian sightlines will be provided at the site's access points.

Pedestrian access will be provided via Ashfield Road and Philip Highway (as well as emergency exits to the eastern boundary).

Bicycle parking is also proposed with a total of 22 bicycle parking spaces. End-of-trip facilities (including showers, change rooms etc.) are also anticipated



within the tenancies (these are identified within the ice arena but fit-out details for the other tenancies have not yet been determined – given the nature of these uses, however, it is highly likely that change rooms, showers etc. will be provided). Further discussion in respect to bicycle parking is provided in Section 4.2.

3.3 SERVICING AND DELIVERIES

A dedicated waste storage area has been provided within the centre of the car park. The waste storage area will be accessed by rear lift refuse collection vehicles (direct from the aisle). Such movements will be undertaken out of the primary operating periods associated with the site. Additional delivery movements can be undertaken via the parallel 'bays' located adjacent to the building.

Refuse collection will be undertaken via private contractor with the associated manoeuvres accommodated on-site (forward-in/forward-out). The site will be able to accommodate movements by an 10 m long rigid vehicle. As above, it is anticipated that such movements would be undertaken outside of peak periods (or opening hours). Figure 2 illustrates the turn path for such a vehicle to enter and exit the site in a forward direction.



Figure 2 – Rigid vehicle turning movements into, out of and within the site

4. PARKING ASSESSMENT

4.1 CAR PARKING

The Planning and Design Code identifies a minimum parking provision rate of 3.0 spaces per 100 m² to meet the Deemed-to-Satisfy/Designated Performance



Feature (DTS/DPF) rates for development within 'Designated Areas' (Urban Activity Centre Zone). A higher rate of applies to indoor recreation centres outside of the Urban Activity Centre Zone. However, given the majority of the site is within this zone (a 'Designated Area') and that there would be negligible different in accessibility considerations between the northern and southern portions of the site, it is considered appropriate that it be assessed as being wholly within a 'Designated Area'.

On the basis of the above rates, there would be a DTS/DPF requirement for approximately 221 parking spaces associated with the proposal.

While the proposal would not meet the Deemed to Satisfy criteria of the Code in respect to parking provision, it is noted that Performance Outcome 5.1 of the General Development Policies (Transport, Access and Parking) states the following:

"Sufficient on-site vehicle parking and specifically marked accessible car parking places are provided to meet the needs of the development or land use having regard to factors that may support a reduced on-site rate such as:

- (a) availability of on-street car parking*
- (b) shared use of other parking areas*
- (c) in relation to a mixed-use development, where the hours of operation of commercial activities complement the residential use of the site, the provision of vehicle parking may be shared*
- (d) the adaptive reuse of a State or Local Heritage Place."* (our emphases)

The Planning and Design Code therefore contemplates acceptance of lower parking provisions (than suggested by the specified rates) based on development and land use considerations. As was detailed for the previously approved proposal, consideration has therefore been given to the following factors which justify acceptance of reduced rates for the proposed development:

- **differing peak periods** – the temporal distribution of peak demand periods will vary between the different components. For instance, gyms typically generate peak demands early morning and mid to late evening on weekdays (prior to 9 am and after 6pm) and the ice arena would generate peak demands on Saturdays (mid-morning to early evening). While the remaining tenant is currently unknown, there are other forms of indoor recreation which could also generate different peak periods than the ice arena and gym (for example, a use such as a play café would be expected to generate peak demands during mid to late weekend mornings). As a result of the differing peak periods, the overall parking demand associated with the site would be lower than a direct theoretical assessment;



- **public transport accessibility** – the site is within 10 minutes' walk of the Elizabeth Train Station. Additionally, frequent bus connections are also available in closer proximity on Philip Highway, Playford Boulevard and Yorktown Road (and include connections to the Train Station). Ultimately, with increased demand generated by the subject proposal, there may be opportunity for South Australian Public Transport Authority (SAPTA) to provide additional bus stops in even closer proximity to the site; and
- **alternative parking opportunities** – the site is located in close proximity of the Windsor Car Park (publicly accessible off-street car park) with safe pedestrian access accommodated via the pedestrian crossing at the adjacent signalised intersection. There is available capacity within the car park to accommodate a proportion of demands associated with the site (particularly during peak demand periods which would occur on weekends when demand in the Windsor Car Park will generally be low). Council previously accepted a degree of reliance on the off-street car park and it is understood continue to consider this approach acceptable.

Most notably, the previous proposal was associated with a parking provision rate of 2.22 spaces per 100 m² of total floor area. The acceptance of such a rate included consideration of the above factors.

In comparison, the current proposal is associated with a parking provision rate of 2.44 spaces per 100 m² of total floor area. The proposal therefore represents an improvement in the overall parking rate compared to the previously approved application. Additionally, as detailed above, additional demands can be accommodated in the Windsor Car Park.

It is acknowledged that on-street parking could occur on Ashfield Road in the vicinity of the site. Council may wish to consider implementation of parking controls on Ashfield Road (and other side streets) in the future to manage parking conditions and maximise parking accessibility for existing residents of the surrounding area.

4.2 BICYCLE PARKING

The Planning and Design Code identifies the following rates relevant to the Urban Activity Centre Zone (while they do not apply in the General Neighbourhood Zone, it is considered appropriate to adopt them for the whole site, particularly given the adoption of 'Designated Area' rates for the assessment of car parking):

- **indoor recreation centre** – 1 space per 4 employees plus 1 space per 200 m² of gross leasable floor area for visitors.



The specific number of staff associated with the overall facility is not known at this stage. However, assuming 30 staff in total, the above rates would result in a total requirement for 44 bicycle parking spaces. The plans currently identify 22 bicycle parking spaces within the site. It is considered that the above theoretical rate is overly conservative and that the proposed level of provision is adequate (particularly noting different peak demand periods associated with the different tenants). However, if desired by Council, additional bicycle parking could easily be identified during detailed design.

5. TRAFFIC ASSESSMENT

5.1 TRAFFIC GENERATION AND DISTRIBUTION

A detailed traffic generation assessment was prepared as part of the previously approved application (refer the previous traffic and parking report prepared by CIRQA date 16 November 2023). This included review of relevant traffic generation rates for the various land use components. The previous assessment identified the following peak hour traffic volumes associated with the original proposal:

- am weekday peak hour – 55 trips;
- pm weekday peak hour – 72 trips; and
- Saturday afternoon peak hour – 219 trips.

Given the proposal has now been reduced in size compared to that previously approved, the level of traffic generation associated with the development will be reduced compared to that previously assessed. For consistency, the same generation assumptions regarding traffic generation rates from the original assessment have been adopted for the current proposal. Based on this approach, the revised forecast traffic volumes are as follows:

- am weekday peak hour – 47 trips;
- pm weekday peak hour – 61 trips; and
- Saturday afternoon peak hour – 185 trips.

The above forecasts equate to an approximate 15% reduction in traffic generation compared to that identified for the previously approved development.

It is acknowledged that, as per the original assessment, higher volumes may be generated during the ice arena's 'event mode' (i.e. ice hockey league games). However, this would occur infrequently and would not be considered a typical 'design level' for assessment of traffic impacts. Importantly, such infrequent



higher demands would occur outside of the peak periods of the surrounding road network. Additionally, there is reduced seating/spectator capacity with the current proposal compared to the previously approved application. Therefore, 'event mode' impacts will also be reduced.

As with traffic generation, the same distribution assumptions have been adopted for the revised proposal. Figure 3 illustrates the updated distribution forecasts associated with the revised proposal.



Figure 3 - Updated traffic distribution

5.2 TRAFFIC IMPACT

The original traffic and parking report detailed impact analyses and modelling (including SIDRA intersection analysis software) for the previously approved application. The original report as well as supplementary information provided to (and accepted by DIT) concluded that the additional volumes generated would be adequately accommodated on the surrounding road network. Given the current proposal will result in a lesser traffic generation, the traffic impacts will be reduced compared to that previously forecast. The forecast (reduced number of) movements will therefore also adequately accommodated on the adjacent road network. Specifically, in comparison to the previous assessment, it is noted that:

- average daily traffic volumes on the portion of Ashfield Road north of Wilcox Road would be forecast to increase by approximately 785 vehicles per day (compared to 900 vpd forecast for the previously approved development)



with ultimate volumes retained within the level of its designation as a collector road;

- average daily traffic volumes on the section of Ashfield Road south of the site would be forecast to increase by approximately 120 vehicles per day (compared to 140 vpd for the previously approved development). Volumes on this section will remain within levels typically associated with 'local roads' (i.e. less than 1,500 vpd); and
- the original analysis and modelling (accepted by both DIT and the City of Playford) indicated that the intersections of Ashfield Road/Philip Highway, Philip Highway/Playford Boulevard, Ashfield Road/Wilcox Road and Main North Road/Philip Highway/Yorktown Road would all adequately accommodate the forecast increases in movements associated with the original application. Noting the 15% reduction in peak hour movements compared to the original proposal, it follows that the lesser traffic volumes generated by the revised proposal would also be adequately accommodated at the intersections.

6. SUMMARY

The proposal comprises the development of a new ice arena and indoor recreation facility on the subject site. The proposal will be served by two access points on Ashfield Road and 179 car parking spaces within the site.

The proposed level of parking provision will be less than sought by direct application of the DTS/DPF rates identified in the Planning and Design Code. However, based on various land use and accessibility factors, it is considered the level of parking is sufficient (and also an overall improvement in the provision rate per total floor area compared to the previously approved proposal).

The proposal is forecast to generate in the order of 47 am and 61 pm peak hour trips on weekdays and 185 peak hour trips on Saturdays. While there will be an increase in movements distributed via the adjacent roads and intersections, it is considered the additional movements will be readily accommodated at the proposed site access and on the adjacent road network. Most notably, the revised proposal is associated with a reduction of 15% compared to the original application which received development consent. Impacts associated with the current proposal will therefore be less (better) than previously assessed and accepted by the City of Playford and DIT.



PLAYFORD ICE ARENA
ASHFIELD ROAD, ELIZABETH
WASTE MANAGEMENT PLAN





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1. INTRODUCTION

CIRQA has been engaged to prepare a Waste Management Plan for the proposed Playford Ice Arena and Entertainment Precinct at Ashfield Road, Elizabeth. This plan details the proposed management of waste for the development. The preparation of the statement has included consideration of the requirements and recommendations of Zero Waste SA's *"Better Practice Guide Waste Management for Residential and Mixed Use Developments"* (2014).

2. PREVIOUS DEVELOPMENT APPROVAL

A development application for an ice arena and entertainment precinct was previously lodged and subsequently received planning consent for the subject site. The previously approved proposal included a variety of uses including an ice arena, a play café, a rock climbing facility, basketballs courts, a gym and food and beverage tenancies (with a total floor area of approximately 10,630 m²).

A previous waste management report was prepared by Rawtec in support of the original (approved) application. The approved proposal has not proceeded.

3. PROPOSED DEVELOPMENT

Subsequent to the receipt of development consent for the original application, a revised proposal has been prepared for the subject site. The revised development proposal represents a reduced extent of development compared to the previously approved scheme. Specifically, the revised proposal comprises the construction of an indoor recreation facility with the following components:

- a 3,307 m² ice arena (including first floor seating area);
- a 2,535 m² gym; and
- a 1,500 m² indoor recreation tenancy.

The proposal is illustrated on the plans prepared by BELL Architecture (Drawings no. DD2011 and DD2012, dated 16 September 2026).

4. WASTE GENERATION ASSESSMENT

For consistency, the waste generation rates adopted in the earlier Rawtec assessment for the previously approved proposal have been retained for the following assessment. The rates applied by Rawtec have been prorated to the current proposal. For the ice arena proposal, this has included adoption of relevant rates to the food and beverage, office and retail components in addition



the primary indoor recreation component. Based on this approach, the following waste volumes are anticipated to be generated per week by the facility:

- **general waste** – 2,603 L;
- **comingled recycling (glass, cans, plastics etc.)** – 1,504 L;
- **cardboard recycling** – 603 L;
- **green organics** – 1,549 L.

5. WASTE MANAGEMENT SYSTEM

5.1 BIN STORAGE AND TRANSFER PROVISIONS

The development will include an at-grade bin storage area located within the at-grade car park. The bin store will be secured and ventilated. The storage area will accommodate separate general, recycling (both glass/cans and cardboard) and organics bins. In addition, bin wash provisions (taps, graded area and drain) have also been included within the bin store.

Based on the above waste generation assessment, Table 2 identifies the recommended number of bins and services for each waste stream and land use.

Table 1 – Number of bins and service rate for each waste stream

Commercial Waste	Size	Number	Capacity	
General Waste	1,100 L	2	2,200 L	twice per week
Comingled Recycling	1,100 L	2	2,200 L	twice per week
Cardboard Recycling	1,100 L	1	1,100 L	once per week
Green Organics	660 L	2	1,320 L	twice per week

Waste associated with the tenancies will be transported to the bin store by staff or cleaning contractors. Figure 1 illustrates that the bin storage room will be able to accommodate the above number of bins (and also identifies the primary waste transfer paths). The refuse collection vehicle can store within the 'break' (aisle) in the central rows of parking (without impacting general car park circulation). Nevertheless, it is anticipated that collection will be undertaken outside of the primary trading periods of the facility.

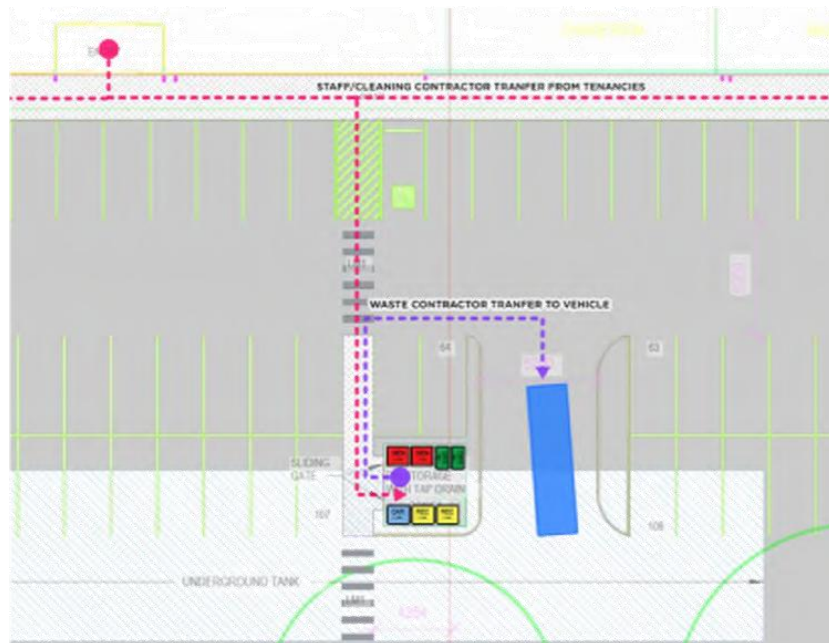


Figure 1 – Bin storage arrangement and key waste transfer paths

The above service schedule indicates that waste will be collected one or two times per week depending on the waste stream. Overall, it is anticipated that this will result in up to six individual collection trucks accessing per week (or 12 movements). Such a number is relatively low and, notably, a reduction compared to that identified for the previously proposed approval.

Given the reduced generation and overall site layout, it is proposed that waste be collected by rear-lift vehicles (generally 8.8 m or shorter, albeit the design accommodates 10.5 m rigid trucks). All refuse collection vehicles will be able to enter and exit the site in a forward direction (as detailed in the CIRQA traffic and parking report).

The waste storage and collection provisions outlined above are considered to address the relevant provisions of the Planning and Design Code. Most notably, the proposal allows for adequate on-site storage and collection of waste and in a location away from sensitive receivers.

5.2 OTHER WASTE

In addition to the primary waste streams, it is noted that, at times, there may be need for additional waste services (confidential documents/shredding, printer cartridges, light globes, etc.). Storage for these additional waste streams would



be undertaken within the individual tenancies and collection would be organised by the tenant and undertaken by private contractors, as and when required.

5.3 MANAGEMENT RESPONSIBILITIES

To ensure the waste system is appropriately managed and maintained, the following responsibilities are identified for relevant parties:

- **local disposal of waste** (from tenancies to bin store) – staff/cleaning contractors;
- **storage and collection of other waste types** – staff/cleaning contractors;
- **collection of commercial waste (general, recycling and organics)** – private waste collection contractor;
- **management and maintenance of bin store (including hygiene, vermin control etc.)** – building management/maintenance; and
- **education and training of staff/contractors (including induction)** – building management/maintenance.

In addition to the above responsibilities, the following specific tasks should be undertaken by building management/maintenance staff:

- ensure that the waste areas are secured to avoid theft and/or inappropriate use of the waste provisions;
- ensure that the waste area and transfer pathways are inspected and cleaned routinely to ensure these areas are kept hygienic and clear of loose waste;
- ensure that bins are routinely cleaned (noting the bin store will include a bin wash area with associated tap and drain);
- ensure that bins are labelled/signed appropriately to identify the relevant waste type as well as the site's property address;
- ensure that staff/contractors are inducted and educated in respect to the appropriate management and disposal of waste within the site; and
- ensure that staff/contractors adhere to the waste management arrangements and manage undesirable behaviour as and if required.



Stormwater Management Report

JOB NUMBER: S62279 - 283532
CLIENT: Pelligra Holdings Pty Ltd
SITE: Playford Ice Arena, ELIZABETH, SA 5112
DATE: 12/11/2025
REVISION: 04

**Engineering
your success.**

ADELAIDE
MELBOURNE
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Document Status

Rev No.	Status	Author	Reviewer			Approved for Issue		
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Introduction

FMG Engineering (FMG) has been engaged by Pelligra Holdings Pty Ltd to prepare a Stormwater Management Plan for the proposed works at Playford Ice Arena, ELIZABETH, SA 5112 for the purpose of planning approval. Engineering survey has been undertaken.

This Stormwater Management Plan has been prepared in accordance with the City of Playford's (Council's) Land Division Guideline, Appendix A-Stormwater Requirements. Council has provided additional stormwater requirements correspondence specific to this development. The intent of this report is to provide a high-level summary of feasible stormwater management principles which will be applied on site to meet Council's requirements and to demonstrate how stormwater can be managed on the proposed development, including detention storage requirements. This report will be updated if fundamental changes to design intent occur throughout the design process.

Site understanding

The site is located at Playford Ice Arena, ELIZABETH, SA 5112 as shown in Figure 1 and is currently covered in short native grasses / weeds, entirely pervious. The site naturally grades toward west with surface grade of 2.5% on average. The footprint of the site is approximately 11,270m²; Subject site is bound by Main North Road to the east, Ashfield Road to the west, vacant lot to the south and Philip Highway to the North.



Figure 1: Site location

FMG has identified an upstream catchment which is the stormflow toward the subject site from Council road reserve to the east. In addition, flood mapping (reference: *South Australian Property and Planning Atlas SAPPA*),

suggests that there is local flooding concern within the boundaries of this particular site during the 1% Annual Exceedance Probability (AEP) storm event as shown in Figure 3.



Figure 2-Flood inundation mapping (<https://sappa.plan.sa.gov.au/>)

A review of the GIS stormwater layer indicates that there is available data for existing stormwater network in both Philip Hwy and Playford Ave as shown in Figure 3.



Figure 3-Council Stormwater network (Location SA Website)

Tonkin Flood mapping (Reference: Playford CBD Strategic Directions-Stormwater Strategies Concept Designs-City Of Playford- July 2017) demonstrates that the flood waters crossing the site are estimated to $1.1\text{m}^3/\text{s}$, entering the site from main north road to the east and exiting towards from Philip Hwy and Ashfield Rd to the west as shown in Figure 4. In order to protect the proposed development, council have outlined that it is acceptable to divert flows around the subject site, conveyed within the road reserve and or verge.



Figure 4-Tonkin Flood mapping

Figure 5 shows that flood water enters the site from the broken kerb along Main North Road. Where in tact, existing kerb height is lower than 50mm along the extents identified as a flood plain.



Figure 5- Damaged kerb within flood plain (Google Map)

Proposed Development

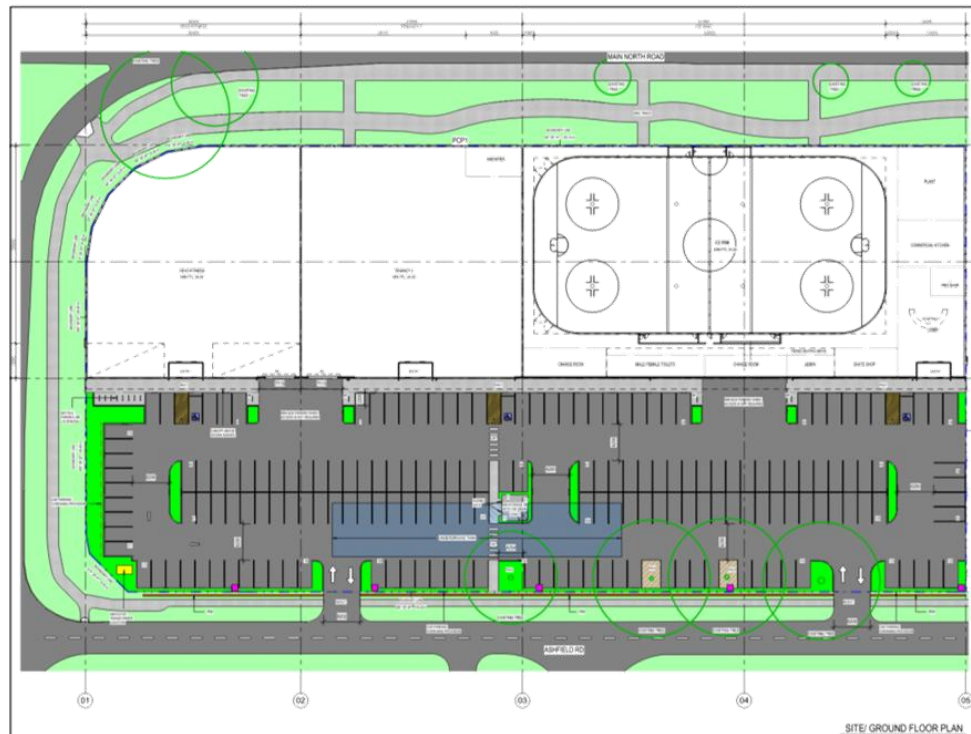


Figure 6-Proposed development

The concept site plan is shown in Appendix A (subject to minor amendments). The plan outlines the proposed development, which includes a two-storey building with offices, car parking areas, an ice arena, basketball courts, a gym, a rock-climbing facility, and a basement. This plan is provided for development application purposes only.

Stormwater Management Requirements

The following applies regarding the City of Playford's Land Division Guideline, Appendix A-Stormwater Requirements:

- The post development flow must not exceed the predevelopment flow.
- The stormwater system for all developments is to incorporate a major and minor drainage system. The major system is to provide a safe and unobstructed overland flow path for large flood events. The minor system shall be capable of conveying the majority of rainfall events that occur throughout the year in an underground system.
- For commercial developments the minor runoff system shall be capable of conveying the 1 in 10-year ARI rainfall event. There is to be no ponding in the street and the limit of the downstream drainage system is not to be exceeded.
- The major system is to be capable of conveying the 1 in 100-year rainfall event in conjunction with the underground system. Flows can be disposed of via a combination of the underground system and unobstructed overland flow routes.

- While the option for gravity drain exists, pumping shall not be considered.
- The use of Water Sensitive Urban Design treatment methods is encouraged and should be analysed and incorporated into the design.
- The detention system is to be sized to ensure that peak flow rate discharging from the post-developed sites does not exceed the peak flow rate for the pre-developed site. If Council has a set discharge limit, then the detention system is to be designed in order to detain the peak flow rate from the post developed site to the discharge limit.
- Typical forms of control to be used should be trash racks, gross pollutant traps and oil interceptors. On private owned properties these systems are to be located within the property boundaries of the development and are the responsibility of the developer.
- The minimum standard of treatment shall be able to remove following percentages of pollutants from the typical annual urban load:
 - 80% reduction in Total Suspended Solids
 - 60% reduction in Total Phosphorus
 - 45% reduction in Total Nitrogen
 - 70% reduction in Litter
- Finished Floor Levels are to be as follows:
 - Finished Floor Levels for industrial and commercial properties are to be a minimum of 150mm above the level of the post-development major storm flood level adjacent to the building – this is usually calculated within the adjacent carpark or hardstand area.
 - At least 300mm above the height of a 1% AEP flood event

Council also provided engineering comments for the subject site:

- The post development flows during minor and major events must not exceed the equivalent predevelopment flows. Minor and major storm events are to be considered 10% AEP and 1% AEP respectively. Internal stormwater network must have the capacity of conveying runoff from the site during up to and including minor storm event. Runoff generated during storm events greater than minor storm event (10% AEP) can be conveyed within the overland flows towards the stormwater network.
- Legal discharge point is the side entry at the north-western corner of the site.

Stormwater Management Plan

The outcomes of this preliminary investigation indicate the most efficient site management of stormwater will be achieved as follows:

- Under pre-development conditions the subject site is inundated during 100-year storm event. The exact flood levels are not available, however a peak flood conveyance of 1.1m³/s through the subject site has been advised.
- Finished Floor Levels (FFL) for the buildings are nominated at 34.00m AHD.
- Preliminary building finished floor level (FFL) will be raised 300mm above flood levels and flood can travel along the adjacent kerb and verge to the north and west if necessary. The proposed development will have a finished floor level of 34.00m AHD. All verges around the building will be adjusted to ensure the land does not grade toward the building.
- The proposed finished floor level of 34.00m AHD also allows for a gravity-based outfall solution. The downstream tailwater level at the discharge point (Side entry pit to the north-west of the subject site) will be inundated during most storm events.

- Given the proposed development and significant basement works, the elevated floor level will not result in significant additional earthworks and may in fact better balance site works.
- Runoff from the roof areas will be collected via downpipes and be diverted into an underground detention tank which will be located within the carpark to the northwest.
- Gap flow from gutter overflows will be captured at the ground level and conveyed into the underground detention tank during storms greater than 20-year ARI event.
- Surface area runoff will be collected via pits and pipes and diverted to the underground detention tank.
- In accordance with Council requirements, discharge from the site has been restricted from 1% AEP (100-year ARI) and 10% AEP storm event post-development peak flows to the equivalent pre-development peak flows. Hence, stormwater runoff from the site will be discharged to the discharge point via an orifice and weir pit with discharge rates of 45L/s and 209L/s during minor and major storm event respectively. The weir helps to restrict the discharge during minor storm event.
- Our analysis of the existing conditions on site has identified the existing kerbing to Main North Road is heavily dilapidated, and where intact, is 50mm or shorter, facilitating the current overflow path onto the subject site. FMG has considered the scenario where a new barrier kerb is constructed to Main North Road (either by the developer, or as part of DIT's future upgrades to this intersection) and identified (using overland flow routes in DRAINS) that the flood waters can be safely conveyed around the corner via the flow path created by a 150mm high barrier kerb, and 5% verge crossfall back towards the kerb. This section has been modelled in DRAINS assuming 1.1m³/s as the baseflow and result (figure 11) shows that there will be zero floodwater entering the site. The existing kerb and gutter along Main North Road will need to be repaired/replaced to achieve this result as it is currently damaged/inadequate height.:
 - o Kerb height: 100mm
 - o Road crossfall: 3%
 - o Verge (batter) slope towards footpath: 5%
- It is noted that future designs of the new left turn lane will need to consider minimum safe travel widths within the modified left turn lane with the flood water conveyance when undertaken by DIT.
- 8 x PSorb 690 Storm Filter cartridges installed in 18m² Storm Filter chamber inside the OSD tank plus Ocean Guards with 200µm mesh bags (OG-200) within proposed pits will be incorporated to treat carpark runoff upstream of the underground detention tank to remove hydrocarbons, oils, and gross pollutants from site runoff.

Stormwater Calculations

Catchment analysis

Site catchments were determined based on aerial imagery, and concept drawings as per Appendix A. Pre-development catchment includes 95% impervious area with a footprint of approximately 11,270m² based on current aerial imagery of the site. Table 1 summarises the post-development sub-catchment areas (shown in Figure 6) and percentages of pervious/impervious.

Table 1 – Post-development Catchment properties

Sub-Catchment	Area (m ²)	% Impervious	% Pervious
Roof	5,860	100	0
Surface	5,439	80	20
Total post-development	11,298	95%	5%

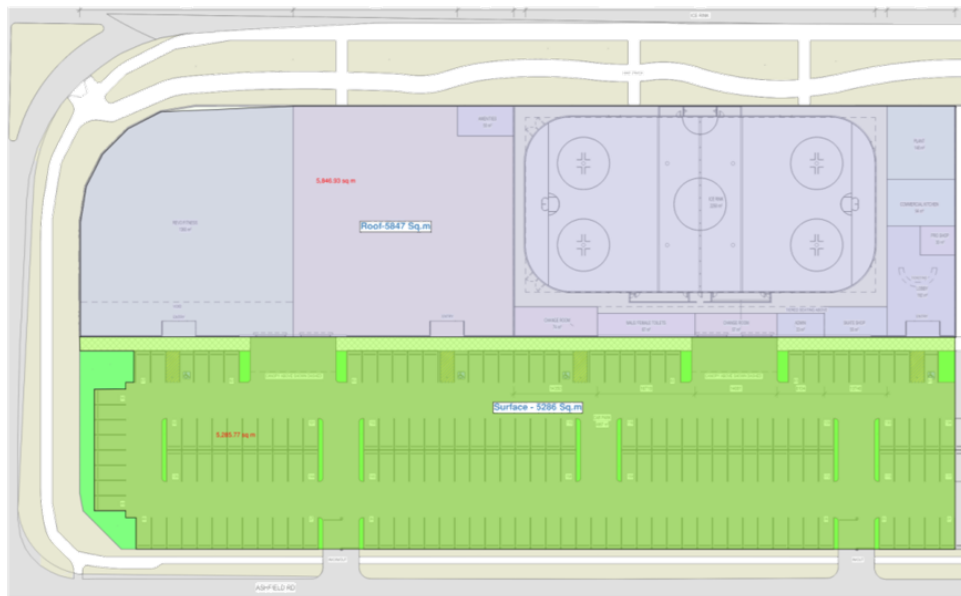


Figure 7 – Sub-catchment Plan

Hydrological parameters

The DRAINS model hydrological input parameters are outlined below:

Stormwater model:	
Pre-Development	ILSAX
Post-Development	ILSAX
Antecedent moisture condition:	3
Soil Type	3
Rainfall data	ARR Data Hub

A DRAINS model was prepared (Figure 8) to demonstrate the feasibility of controlling peak flows to match pre-development flows. It is anticipated that minor changes will be incorporated into the final construction documentation as design details are resolved, but holistically we do not anticipate any changes to the way in which stormwater will be managed on site.

DRAINS modelling results

The results of the DRAINS modelling for minor (10% AEP) and major (1% AEP) storm events are summarised in Figure 9-10. The results demonstrate that the post development peak discharge during the major/minor storm do not exceed the pre-development peak discharges from the site during equivalent storm events. An underground onsite detention tank within the extent of carpark/paving area is considered in modelling to satisfy council's detention requirements.

After analysing the DRAINS results during minor/major events, it can be concluded that post development flow during minor and major storm events should be restricted to 48L/s and 228L/s for the proposed development respectively. As mentioned in table 2, onsite detention storage shall provide a minimum capacity of 210m³ to restrict stormwater runoff to pre-development peak flows. The abovementioned detention volume has

incorporated with an orifice and a weir discharge which will be connected to the Council's stormwater infrastructure at the corner of Ashfield Rd and Philip Hwy.

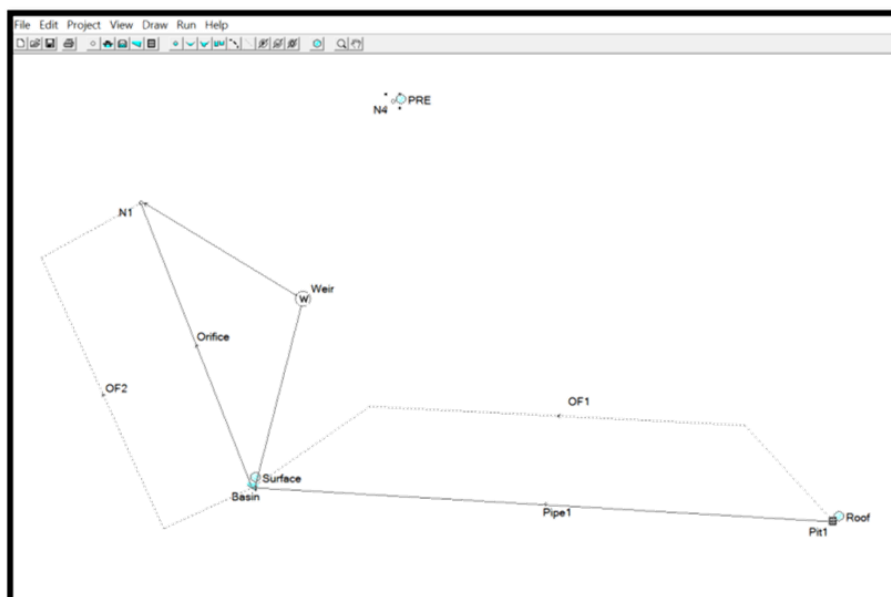


Figure 8: DRAINS model layout

Table 2: DRAINS modelling

Scenario	Minor storm 10% AEP / 1:10 year ARI		Major storm 1% AEP / 1:100-year ARI	
	Peak flow	Detention volume	Peak flow	Detention volume
Pre-Development	48 L/s	----	218 L/s	----
Post-development	45 L/s	150m ³ detention storage	209 L/s	210m ³ detention storage

DRAINS modelling using AR&R2016 procedures no longer reports all individual storm duration data as each storm duration is run as an ensemble of up to 10 storm events with different hydrographs (i.e. rear loaded, front-loaded storms) and the median result for each duration is adopted, with the 'worst case' volume reported to the user. More information on this can be found within the DRAINS guidelines if required.

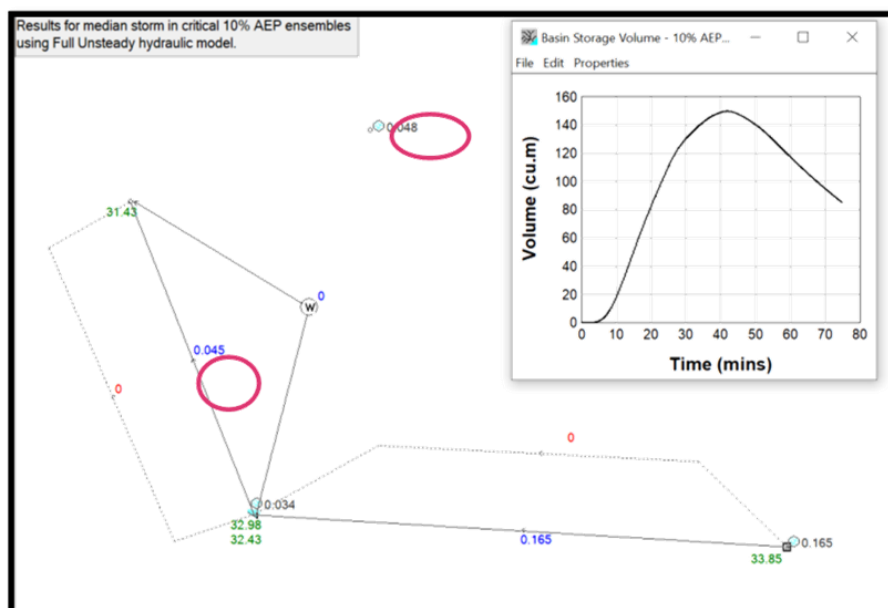


Figure 9: DRAINS modelling results during minor (10% AEP) storm events

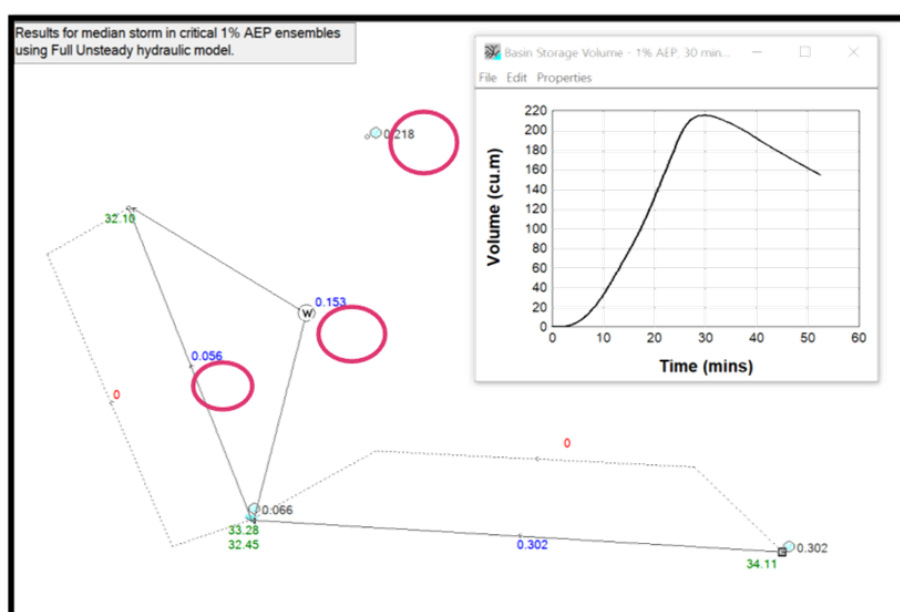


Figure 10: DRAINS modelling results during major (1% AEP) storm events

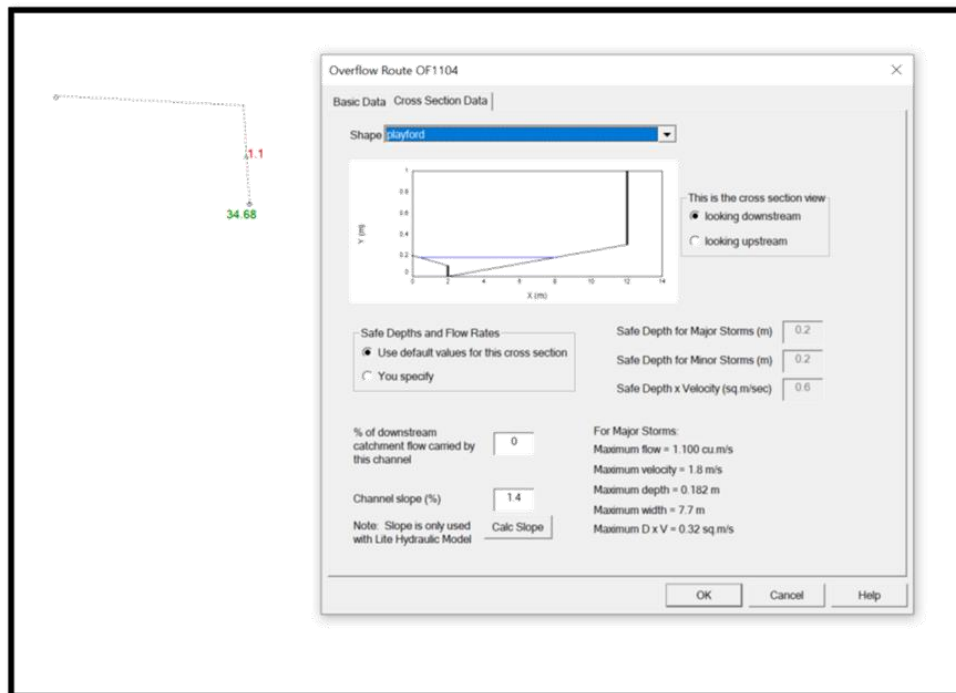


Figure 11-Flood Analysis assuming 100mm Kerb with 5% towards footpath and 3% road crossfall

Stormwater Quality

Treatable flow rate

The 4EY storm event (3-Month ARI) was run in accordance with the City of Playford guidelines to determine the treatable flow rate for the proposed water quality measures (litter basket, GPT, etc.). During the 4EY storm event approximately 48L/s will be generated by the proposed development as shown in Figure 9.

MUSIC model

The MUSIC model has been split into 2 sub catchments; building roof and carpark/surface.

- Runoff from roof, carpark, driveway and outdoor courts catchments is to be collected via a series of underground pits and pipes, conveyed through 8xPSorb 690 Storm Filter cartridges installed in 18m² Storm Filter chamber inside the OSD tank.
- Trash screens (i.e. Ocean Guards 200mm) are used for pre-treatment of the carpark, driveway, pavement, and landscape areas. Note for each field pit capturing overland flow from carpark, driveway, pavement, and landscape will require an Ocean Guard.
- The conceptual design is based on all inlets routing to the Storm Filter chamber in the detention tank as shown in the MUSIC model. There will need to be access to the detention tank via a manhole to access the cartridges for periodic maintenance.

We modelled with the following parameters:

- MUSIC Version 6.3.0
- Rainfall station Adelaide Dry.
- Water Sensitive SA: South Australian MUSIC Guidelines, utilizing modified % impervious area, rainfall threshold, soil properties & pollutant concentration

As seen in Table 3, this arrangement meets Council's stormwater quality targets. Further WSUD principals such as infiltration, tree pits and permeable paving were all considered, however, a review of local area soil maps and bore logs indicates the presence of reactive soils in the area would result in significant seasonal shrink / swell and future pavement failure.

Results for median storm in critical 4EY ensembles using Full Unsteady hydraulic model.

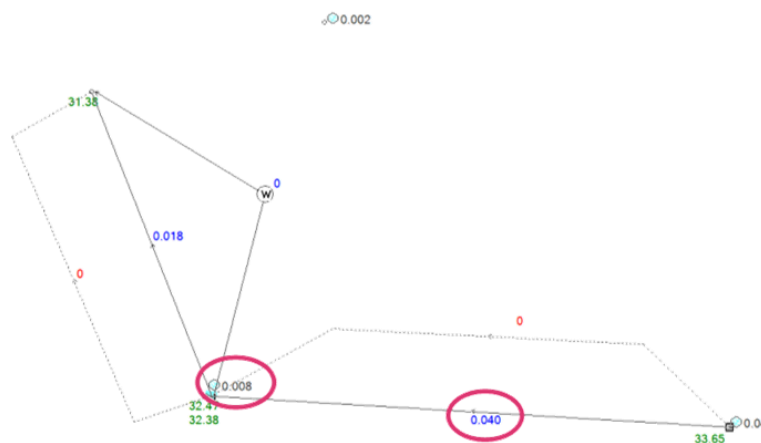


Figure 12: Treatable flow rate during the 4EY event

Table 3: MUSIC modelling results

	Council Reduction	target	% Reduction achieved in model	COMPLIANCE
TSS	80%		90.3%	Achieved
TP	60%		66.9%	Achieved
TN	45%		46.8%	Achieved
Gross Pollutants	90%		99.9%	Achieved

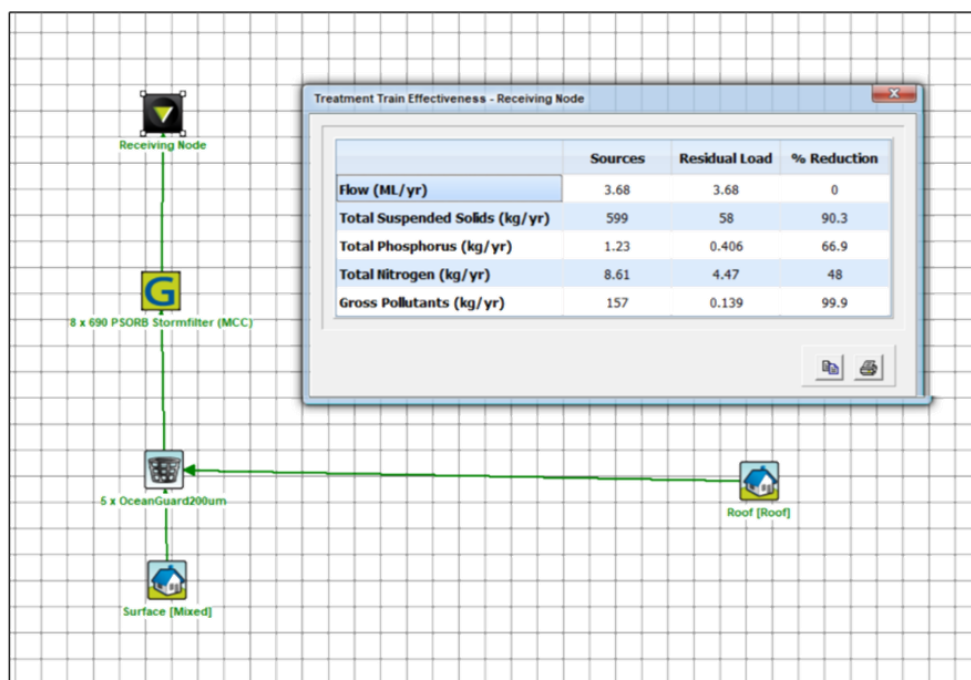


Figure 13 - MUSIC Model Layout

Conclusions

This SMP has been prepared to demonstrate how stormwater can be effectively managed on the site, and if any alterations are made to stormwater management during the detailed design phase this SMP will be updated accordingly.

This report has assessed the subject site proposed drainage to determine the necessary stormwater infrastructure to meet Council requirements. A 210m³ detention tank is proposed to be installed on site to ensure post development peak discharge during the major/minor storm events do not exceed the pre-development peak flow rate during equivalent events.

A new 150mm high barrier kerb along Main North Rd will safely divert existing overland floodwaters within the road reserve.

The proposed network within the proposed development will govern peak discharge flow rates from the site to match pre-development peaks, 45L/s and 209L/s for minor and major storms respectively. Finished Floor Levels have been determined to be 300mm above the flood level for building areas to ensure minimum boundary levels, site grading and vehicle access is maintained. FFL for the main building has been nominated to be 34.00mAHD. Finished floor level may be adjusted during detailed design, however, must maintain a minimum 150mm freeboard from the maximum 1% AEP ponding level within the site and min 300mm freeboard from the 1%AEP Flood level.

In conclusion this SMP demonstrates that the site can be developed as proposed without compromising downstream property or drainage networks, and Council's requirements for stormwater management may be achieved.

Limitations

FMG Engineering has prepared this report in accordance with our Proposal and the Brief where provided. The contents of the report are for the sole use of FMG Engineering and no responsibility or liability to any third party will be accepted. Data or opinions contained within the report may not be used in other contexts or for any other purposes without FMG Engineering's prior review and agreement.

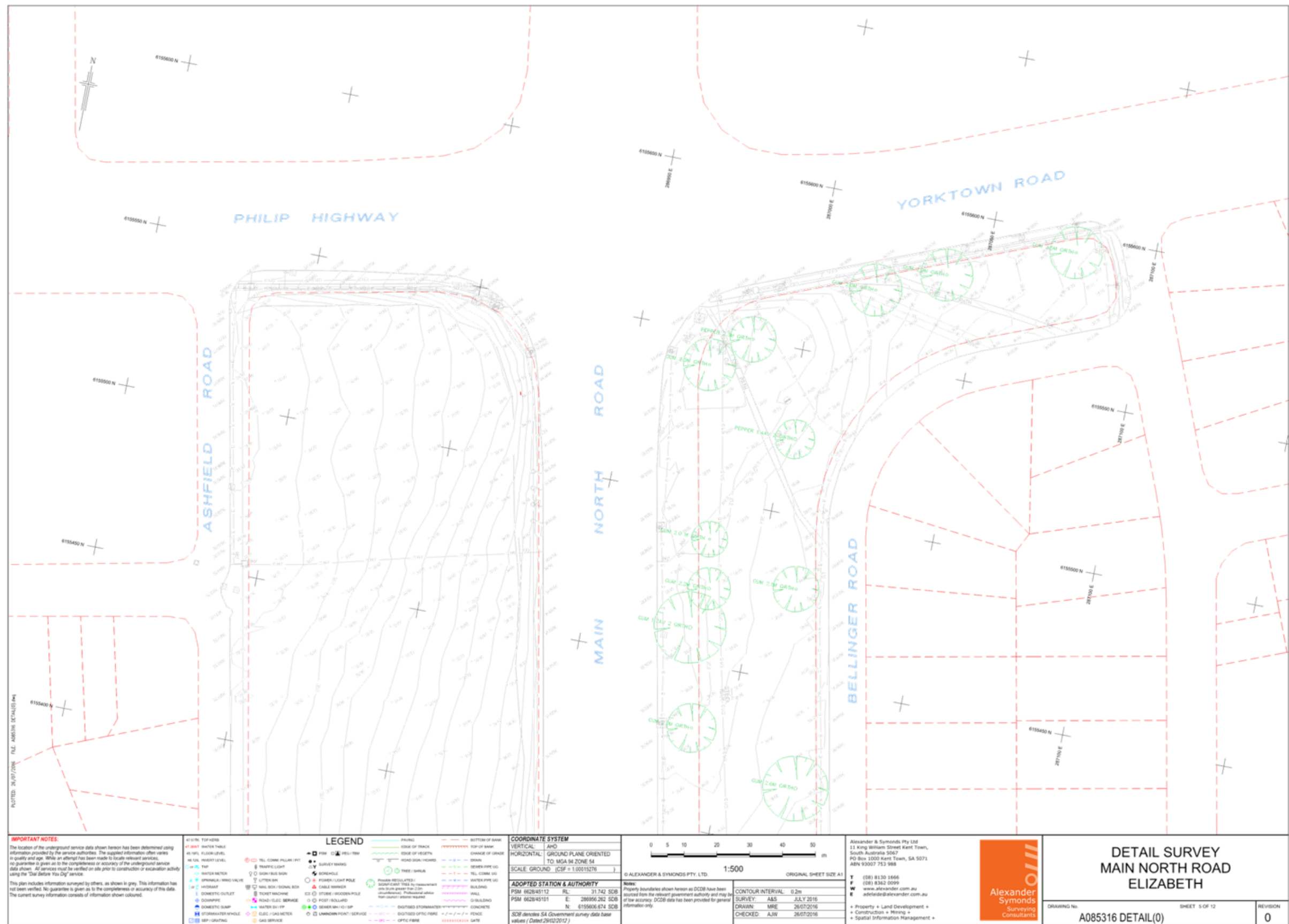
It is strongly recommended that any plans and specifications prepared by others and relating to the content of this report, or amendments to the original plans and specifications, are reviewed by FMG Engineering to verify that the intent of our recommendations is properly reflected in the design. During construction FMG Engineering requests the opportunity to review our interpretations if the exposed site conditions are significantly different from those inferred in this report.

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Appendix A

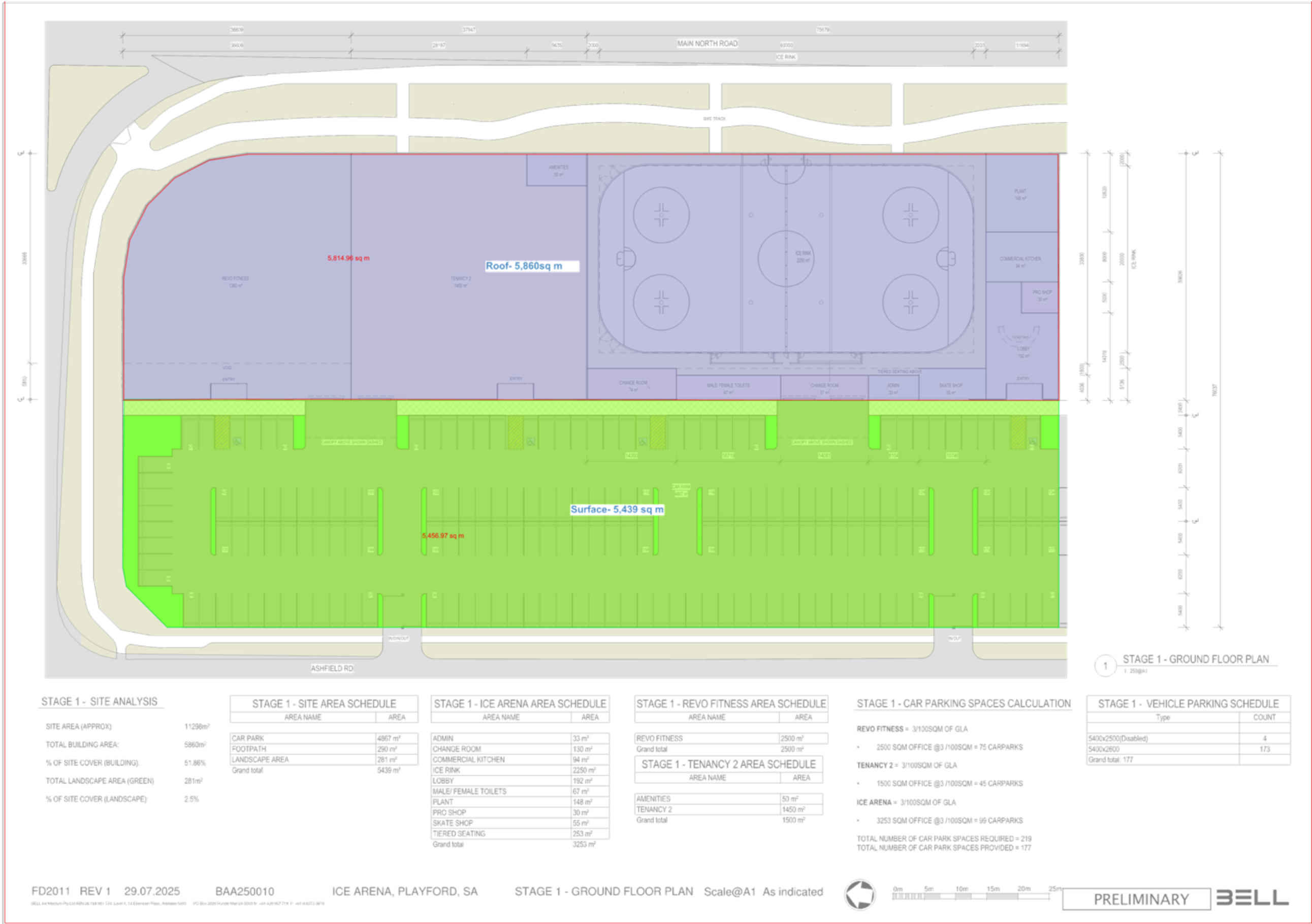
Proposed site plans
Site Survey

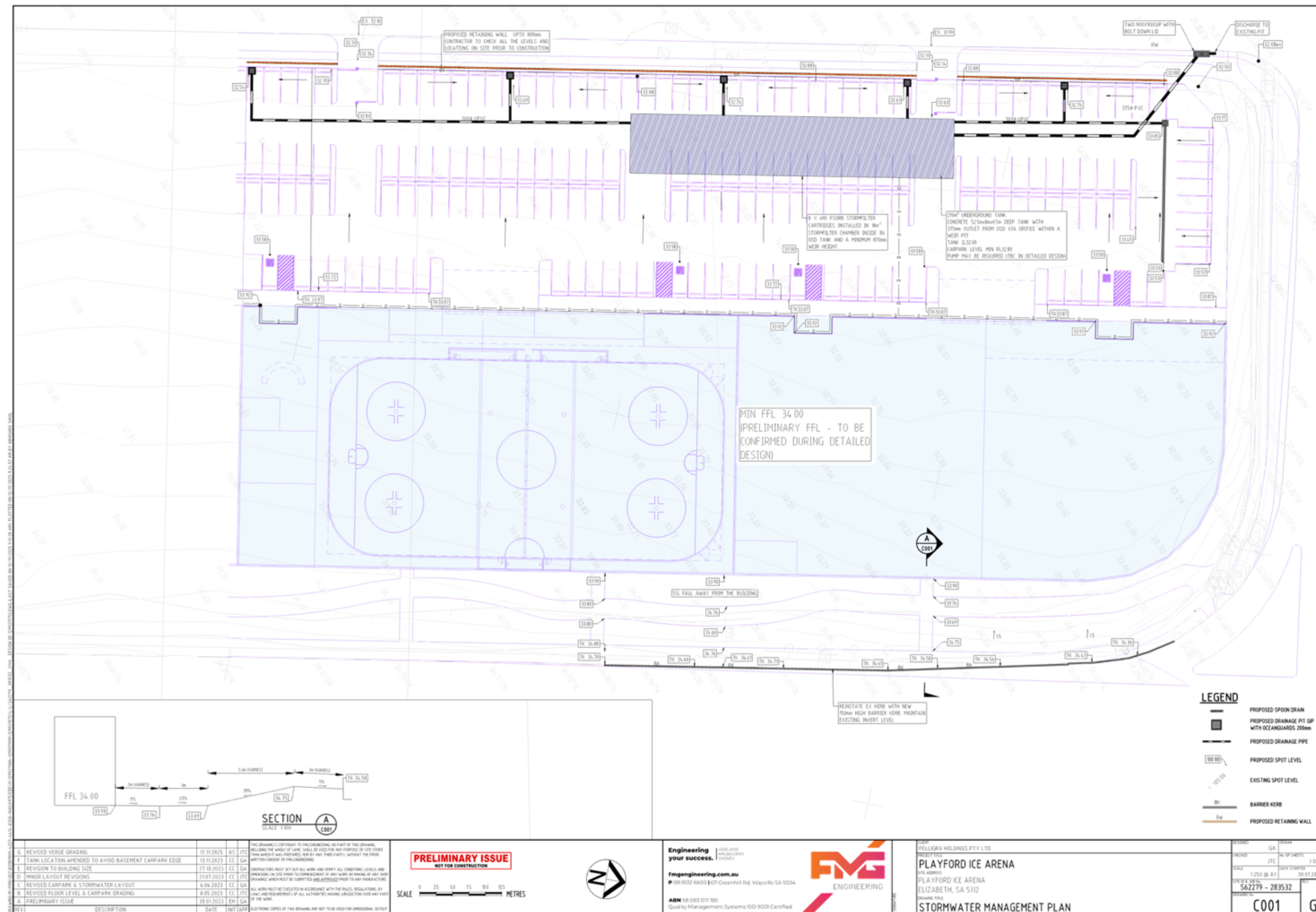


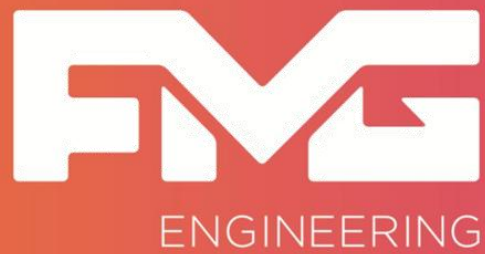


Appendix B

Catchment Plan
Preliminary Civil Design Plans





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Reference Number:
ATS7071-PlayfordIceAreAIA-R4

Wednesday, 15 October 2025

Blake Soroka
Pelligra
Email: blake.soroka@pelligra.com

Dear Blake,

Re: Playford Ice Arena, Main North Road, Elizabeth - Arboricultural Impact Assessment

Arborman Tree Solutions has assessed the identified Significant and Regulated trees at Playford Ice Arena, Main North Road, Elizabeth. The assessment has identified the potential impacts to the trees from the proposed development and supporting infrastructure and recommends mitigation strategies where appropriate. The development includes the demolition and preparation of the site, the construction of a new recreation, retail and sporting facilities on the undeveloped parklands.

The assessment considered twenty-three trees which are identified as a mix of locally indigenous, Australian native and exotic species. Three trees, Trees 8, 21 and 24 are in Good overall condition and have extended useful life expectancies. Eighteen are in Fair overall condition as a result of their reduced assessed attributes, such as moderate history of branch failure, epicormic growth, the presence of a stable included bark union and/or minor levels of decay. Trees 22 and 23 are considered to be in Poor overall condition due to the level of dieback and deadwood throughout the crown and/or poor-quality trunk and branch unions, with the decay and hollowing.

The encroachment for Trees 4-8 is more than 20% of the total NRZ area and is therefore classified as a 'Major Encroachment', Trees 10 and 11 have encroachment, more than 10% but less than 20% and the encroachment is classed as 'moderate' as defined in AS4970-2025. Australian Standard AS4970-2025 also identifies relevant factors that should be considered when determining the 'impact' of encroachments such as this; these considerations are listed under section 3.3.4 *TPZ encroachment considerations*. When considering these factors, the trees could be retained provided appropriate mitigation.

The following discusses the relevant factors of AS4970-2025 section 3.3.4 *TPZ encroachment considerations* for these trees:-

- 3.3.4 (a) '*Location and distribution of roots to be determined through non-destructive investigation methods*'
It is understood that an exploratory excavation has occurred by others and the tree owner, the City of Playford; which confirms this tree will not be impacted by the path installation.
- 3.3.4 (d) '*Age, vigour and size of the tree*'
Trees 4-8, 10 and 11 are displaying good health regardless of overall condition, indicating they can tolerate the proposed level of encroachment without noticeable impacts. Healthy and vigorous trees can manage various levels of pruning, demolition of existing structures, changes in soil grade and moisture, soil compaction and other root zone encroachments and are better able to adapt to the new site conditions once the development phase has been completed.



- 3.3.2 (f) '*Presence of other trees with overlapping NRZ or grafted roots*'
Trees 10 and 11 would remain adjacent to each other and are the same species. As the trees have overlapping NRZs, there is potential for root grafting and co-relationships through mycorrhizal interactions, effectively increasing the available resources these trees are able to access.
- 3.3.2 (g) '*Proposed staging and timing of excavation or root-cutting*'
The required root pruning could easily be completed prior to the initial excavation of the site. The remainder of the root zone that is to be retained in the proposal could easily be mulched and watered from the beginning of the site set up until the completion of the works. Watering and mulching will create a favourable environment for root growth thereby compensating for the minor level of root loss due to the required works.
- 3.3.2 (h) '*Proposed tree maintenance and tree care activities*'
The following tree maintenance and tree care activities are to be undertaken as part of the management of these trees through the development of this site:-
 - a. Maintenance Pruning – the tree will be subject to maintenance pruning in accordance with the guidelines and recommendations of Australian Standard AS4373-2007 *Pruning of amenity trees* and Minimum Industry Standard MIS308 *Tree pruning*.
 - b. Irrigation – the area of the root zone that is to be retained **will need to be** irrigated from the commencement of site works to the completion of all development activities. **A permanent irrigation system is recommended** as part of the landscape management around these trees. The use of irrigation is designed to promote additional root growth that will compensate for the minor level of root loss due to the required works.
 - c. Mulch – the area of the root zone that is to be retained will be mulched with a natural tree mulch, Forest Mulch, Arborist Mulch or similar. The mulch should be 75-100 mm thick and cover the irrigation system. The use of mulch is designed to retain soil moisture and promote additional root growth that will compensate for the minor level of root loss due to the required works.
 - d. Soil Amelioration – additional treatments will be applied to soil as required to maintain/improve the growing environment. This may include the application of a sucrose solution, fertiliser and/or other soil improvers.
- 3.3.2 (i) '*Proposed construction measures that reduce the impact on trees*'
There are low impact methodologies and materials which can be incorporated into a detailed design phase which could reduce the encroachment and minimise the impact to these trees. These measures may include the use of permeable surfaces constructed above existing grade and limiting any required ground preparation cuts where practicable.

The encroachment for the remaining sixteen trees (Trees 9 and 12-26) covers the entire NRZ, SRZ and the trunk and as such, they cannot be retained under the proposal. They are therefore considered to be Conflicted by the proposed development.

**Recommendation*****Tree Protection***

There is potential for incidental damage and as such, Tree Protection is recommended as part of this construction as well as health mitigation measures.

The following is recommended as a minimum:-

1. Ensure all work requirements/activities in the vicinity of these trees are discussed and designed in consultation with the Project Arborist, i.e. no machinery operation in the vicinity of the trees without a Tree Protection Plan.
2. A Tree Protection Zone (TPZ) fence is to be erected to ensure access to the main trunk for each tree to be retained is restricted. The fence is to be installed prior to the commencement of all other site works, including demolition.
3. If machinery access is required within the TPZ, ground protection is to be installed in consultation with the Project Arborist to ensure tree roots are not damaged.
4. Where practicable, improvements to the Tree Protection Zone (TPZ) may include the application of a natural, mixed green mulch composed of various particle sizes. The mulch layer should be applied to a depth of 75-100 mm to enhance soil moisture retention and improve soil structure. Additionally, when watering is required, it should be delivered slowly over an extended period, typically 2 to 3 hours, using an oscillating sprinkler to promote deep soil infiltration and support healthy root function.

Yours sincerely,

A handwritten signature in blue ink, appearing to read 'Tom Richardson', with a long horizontal flourish extending to the right.

TOM RICHARDSON
Consulting Arborist
Diploma of Arboriculture – AHC50516
VALID Tree Risk Assessment (VALID)
Native Vegetation Council Trained Arborist
ISA – Tree Risk Assessment Qualification



Appendix A - Tree Assessment Methodology



Tree Assessment Form (TAF©)

Record	Description
Tree	In botanical science, a tree is a perennial plant which consists of one or multiple trunks which supports branches and leaves. Trees are generally taller than 5 metres and will live for more than ten seasons, with some species living for hundreds or thousands of seasons.
Genus and Species	<p>Botanical taxonomy of trees uses the binominal system of a genus and species, often there are subspecies and subgenus as well as cultivars. When identifying tree species, identification techniques such as assessing the tree's form, flower, stem, fruit and location are used. Identifying the right species is critical in assessing the tree's legalisation and environmental benefit. All efforts are made to correctly identify each tree to species level, where possible.</p> <p>Genus is the broader group to which the tree belongs e.g. <i>Eucalyptus</i>, <i>Fraxinus</i> and <i>Melaleuca</i>. Species identifies the specific tree within the genus e.g. <i>Eucalyptus camaldulensis</i>, <i>Fraxinus griffithi</i> or <i>Melaleuca styphelioides</i>. Trees will also be assigned the most commonly used Common Name. Common Names are not generally used for identification due to their nonspecific use, i.e. <i>Melia azedarach</i> is commonly known as White Cedar in South Australia but is also called Chinaberry Tree, Pride of India, Bead-tree, Cape Lilac, Syringa Berrytree, Persian Lilac, and Indian Lilac; equally similar common names can refer to trees from completely different Genus e.g. Swamp Oak, Tasmanian Oak and English Oak are from the <i>Casuarina</i>, <i>Eucalyptus</i> and <i>Quercus</i> genus's respectively.</p>
Height	Tree height is estimated by the arborist at the time of assessment. Tree height is observed and recorded in the following ranges; <5m, 5-10m, 10-15m and >20m.
Spread	Tree crown spread is estimated by the arborist at the time of assessment and recorded in the following ranges <5m, 5-10m, 10-15m, 15-20m, >20m.
Health	Tree health is assessed using the Arborman Tree Solutions - Tree Health Assessment Method that is based on international best practice.
Structure	Tree structure is assessed using Arborman Tree Solutions - Tree Structure Assessment Method that is based on international best practice.
Tree Risk Assessment	Tree Risk is assessed using Tree Risk Assessment methodology. The person conducting the assessment has been trained in the International Society of Arboriculture Tree Risk Assessment Qualification (TRAQ), Quantified Tree Risk Assessment (QTRA) and/or VALID Tree Risk Assessment (VALID). Refer to the Methodology within the report for additional information.
Legislative Status	Legislation status is identified through the interpretation of the <i>Development Act 1993</i> , the <i>Natural Resource Management Act 2004</i> , the <i>Native Vegetation Act 1991</i> and/or any other legislation that may apply.
Mitigation	Measures to reduce tree risk, improve tree condition, remove structural flaws, manage other conditions as appropriate may be recommended in the form of pruning and is listed in the Tree Assessment Findings (Appendix B). Tree pruning is recommended in accordance with AS4373-2007 <i>Pruning amenity trees</i> where practicable. Where measures to mitigate risk is not possible and the risk is unacceptable, then tree removal or further investigation is recommended.



Useful Life Expectancy (ULE)

ULE Rating	Definition
Surpassed	The tree has surpassed its Useful Life Expectancy. Trees that achieve a surpassed ULE may do so due to poor health, structure or form. Additionally, trees that are poorly located such as under high voltage powerlines or too close to structures may also achieve a surpassed ULE. Trees that achieve this status will be recommended for removal as there are no reasonable options to retain them.
<10 years	The tree displays either or both Poor Health and/or Structure and is considered to have a short Useful Life Expectancy of less than ten years. Some short-lived species such as <i>Acacia sp.</i> may naturally achieve a short ULE.
>10 years	The tree displays Fair Health or Structure and Good Health or Structure and is considered to have a Useful Life Expectancy of ten years or more. Trees identified as having a ULE of >10, will require mitigation such as pruning, stem injections or soil amelioration to increase their ULE.
>20 years	The tree displays Good Health and Structure and is considered to have an extended Useful Life Expectancy of more than twenty years.

Maturity (Age)

Age Class	Definition
Senescent	The tree has surpassed its optimum growing period and is declining and/or reducing in size. May be considered as a veteran in relation to its ongoing management. Tree will have generally reached greater than 80% of its expected life expectancy.
Mature	A mature tree is one that has reached its expected overall size, although the tree's trunk is still expected to continue growing. Tree maturity is also assessed based on species; as some trees are much longer lived than others. Tree will have generally reached 20-80% of its expected life expectancy.
Semi Mature	A tree which has established but has not yet reached maturity. Normally tree establishment practices such as watering will have ceased. Tree will generally not have reached 20% of its expected life expectancy.
Juvenile	A newly planted tree or one which is not yet established in the landscape. Tree establishment practices such as regular watering will still be in place. Tree will generally be a newly planted specimen up to five years old; this may be species dependant.

Tree Health Assessment (THA©)

Category	Description
Good	Tree displays normal vigour, uniform leaf colour, no or minor dieback (<5%), crown density (>90%). When a tree is deciduous, healthy axillary buds and typical internode length is used to determine its health. A tree with good health would show no sign of disease and no or minor pest infestation was identified. The tree has little to no pest and/or disease infestation.
Fair	Tree displays reduced vigour abnormal leaf colour, a moderate level of dieback (<15%), crown density (>70%) and in deciduous trees, reduced axillary buds and internode length. Minor pest and/or disease infestation potentially impacting on tree health. Trees with fair health have the potential to recover with reasonable remedial treatments.
Poor	Tree displays an advanced state of decline with low or no vigour, chlorotic or dull leaf colour, with high crown dieback (>15%), low crown density (<70%) and/or in deciduous trees, few or small axillary buds and shortened internode length. Pest and or disease infestation is evident and/or widespread. Trees with poor health are highly unlikely to recover with any remedial treatments; these trees have declined beyond the point of reversal.
Dead	The tree has died and has no opportunity for recovery.



Tree Structural Assessment (TSA©)

Category	Description
Good	Little to no branch failure observed within the crown, well-formed unions, no included bark, good branch and trunk taper present, root buttressing and root plate are typical. Trees that are identified as having good health display expected condition for their age, species and location.
Fair	The tree may display one or more of the following a history of minor branch failure, included bark unions may be present however, are stable at this time, acceptable branch and trunk taper present, root buttressing and root plate are typical. Trees with fair structure will generally require reasonable remediation methods to ensure the tree's structure remains viable.
Poor	History of significant branch failure observed in the crown, poorly formed unions, unstable included bark unions present, branch and/or trunk taper is abnormal, root buttressing and/or root plate are atypical.
Failed	The structure of the tree has or is in the process of collapsing.

Tree Form Assessment (TFA©)

Category	Description
Good	Form is typical of the species and has not been altered by structures, the environment or other trees.
Fair	The form has minor impacts from structures, the environment or adjacent trees which has altered its shape. There may be slight phototropic response noted or moderate pruning which has altered the tree's form.
Poor	The tree's form has been substantially impacted by structures, the environment, pruning or other trees. Phototropic response is evident and unlikely to be corrected.
Atypical	Tree form is highly irregular due to structures or other trees impacting its ability to correctly mature. Extreme phototropic response is evident; or the tree has had a substantial failure resulting in its poor condition, or extensive pruning has altered the tree's form irreversibly.

Priority

Category	Description
Low	Identified works within this priority should be carried out within 12 months.
Medium	Identified works within this priority should be carried out within 6 months.
High	Identified works within this priority should be carried out within 3 months.
Urgent	Identified works within this priority should be carried out immediately. Works within this priority rating will be brought to attention of the responsible person at the time of assessment.



Development Impact Assessment

Potential development impacts were determined in accordance with Australian Standard AS4970-2025 *Protection of trees on development sites*. The identification of the impact of development considers a number of factors including the following:

- a. The extent of encroachment into a tree's Tree Protection Zone by the proposed development as a percentage of the area.
- b. Results of any non-destructive exploratory investigations that may have occurred to determine root activity.
- c. Any required pruning that may be needed to accommodate the proposed development.
- d. Tree species and tolerance to root disturbance.
- e. Age, vigour and size of the tree.
- f. Lean and stability of the tree.
- g. Soil characteristics and volume, topography and drainage.
- h. The presence of existing or past structures or obstacles potentially affecting root growth.
- i. Design factors incorporated into the proposed development to minimise impact.

The impacts on a tree can be varied and are not necessarily consistent with or directly correlated to a particular level of encroachment, to assist in providing consistency the levels of impact have been classified into the following categories: -

- No Impact - no encroachment into the TPZ has been identified.
- Low <10% - the identified encroachment is less than 10% of the TPZ area and not expected to impact tree viability.
- Low >10% - the identified encroachment is greater than 10% of the TPZ area however there are factors that indicate the proposed development will not negatively impact tree viability.
- High >10% - the identified encroachment is greater than 10% of the TPZ area and factors are present that indicate the proposed development will negatively impact tree viability. The impact is likely to lead to the long-term decline of the tree however it is unlikely to impact on its short-term stability.
- Conflicted - the identified encroachment is greater than 10% of the TPZ area and in most cases will also impact the SRZ and/or the trunk. There are factors present that indicate the proposed development will negatively impact tree viability to the point where its removal is required as part of the development.

Trees with calculated encroachments greater than 10% and with an Impact identified as 'Low' have features or considerations identified in clauses in AS4970-2025 3.3.4 *TPZ encroachment considerations* which indicate these trees should be sustainable.

Trees with calculated encroachments greater than 10% and with an Impact identified as 'High' do not have any features or considerations identified in clauses in AS4970-2025 3.3.4 and therefore alternative design solutions, additional root investigations and/or tree sensitive construction measures are required if the tree is to be retained. Where alternative protection methodologies are not available tree removal may be required to accommodate the development.

Trees with an Impact identified as 'Conflicted' are impacted over the majority of their root zone and/or over the SRZ or on the trunk, additional root investigations or tree sensitive construction measures are not available and the only option is alternative designs or tree removal.



Appendix B - Tree Assessment Findings

Schinus areira

Tree No: 4

Peppercorn Tree

Inspected:	10 January 2023
Height:	10-15 metres
Spread:	5-10 metres
Health:	Good
Structure:	Fair
Form:	Fair
Trunk Circumference:	>3 metres
Useful Life Expectancy:	>10 years
Notional Root Zone:	9.20 metres
Structural Root Zone:	3.08 metres

Observations

The tree is in good health, although it does retain an increased percentage of deadwood, and has a slightly reduced structure rating due to an included bark union in the primary structure.



Legislative Status	Exempt
This tree species is listed as exempt from control under Regulation 3F(4)(b) of the Planning, Development and Infrastructure (General) Regulations 2017.	
Development Impact	High
The proposed encroachment is more than 30% of the NRZ and extends into the periphery of the SRZ. However, the design allows for the retention of the tree, provided that the recommended mitigation measures are implemented and adhered to. With appropriate care and protection during construction, the tree's viability can be maintained.	
Action	Specialised Construction
This tree is located within a design feature intended for retention, despite high encroachment. Low-impact construction methods must be incorporated into the final design to minimise disturbance. Additional mitigation measures will be required to support long-term tree health.	

Eucalyptus leucoxylon

Tree No: 5

South Australian Blue Gum

Inspected:	10 January 2023
Height:	10-15 metres
Spread:	10-15 metres
Health:	Fair
Structure:	Good
Form:	Fair
Trunk Circumference:	>2 metres
Useful Life Expectancy:	>10 years
Notional Root Zone:	8.40 metres
Structural Root Zone:	2.97 metres

Observations

This tree is considered to be in fair overall condition due to the moderately increased level of deadwood and dieback evident throughout the crown.



Legislative Status

Regulated

This tree has a trunk circumference greater than two metres but less than three metres and is not subject to any exemption from regulation and therefore it is identified as a Regulated Tree as defined in the Planning, Development and Infrastructure Act 2016.

Development Impact

High

The proposed encroachment is more than 30% of the NRZ and extends into the periphery of the SRZ. However, the design allows for the retention of the tree, provided that the recommended mitigation measures are implemented and adhered to. With appropriate care and protection during construction, the tree's viability can be maintained.

Action

Specialised Construction

This tree is located within a design feature intended for retention, despite high encroachment. Low-impact construction methods must be incorporated into the final design to minimise disturbance. Additional mitigation measures will be required to support long-term tree health.



Eucalyptus cladocalyx

Tree No: 6

Sugar Gum

Inspected:	10 January 2023
Height:	10-15 metres
Spread:	10-15 metres
Health:	Good
Structure:	Fair
Form:	Poor
Trunk Circumference:	>2 metres
Useful Life Expectancy:	>10 years
Notional Root Zone:	9.60 metres
Structural Root Zone:	3.14 metres

Observations

This tree is considered to be in fair overall condition due to the presence of epicormic growth within the crown and moderate level of decay within the primary structure, otherwise it appears to be healthy and structurally sound.



Legislative Status

Regulated

This tree has a trunk circumference greater than two metres but less than three metres and is not subject to any exemption from regulation and therefore it is identified as a Regulated Tree as defined in the Planning, Development and Infrastructure Act 2016.

Development Impact

High

The proposed encroachment is more than 30% of the NRZ and extends into the periphery of the SRZ. However, the design allows for the retention of the tree, provided that the recommended mitigation measures are implemented and adhered to. With appropriate care and protection during construction, the tree's viability can be maintained.

Action

Specialised Construction

This tree is located within a design feature intended for retention, despite high encroachment. Low-impact construction methods must be incorporated into the final design to minimise disturbance. Additional mitigation measures will be required to support long-term tree health.



Eucalyptus cladocalyx

Tree No: 7

Sugar Gum

Inspected:	10 January 2023
Height:	10-15 metres
Spread:	10-15 metres
Health:	Good
Structure:	Fair
Form:	Good
Trunk Circumference:	>2 metres
Useful Life Expectancy:	>10 years
Notional Root Zone:	8.88 metres
Structural Root Zone:	3.03 metres

Observations

This tree is considered to be in fair overall condition due to the presence of epicormic growth within the crown, otherwise it appears to be healthy and structurally sound.



Legislative Status

Regulated

This tree has a trunk circumference greater than two metres but less than three metres and is not subject to any exemption from regulation and therefore it is identified as a Regulated Tree as defined in the Planning, Development and Infrastructure Act 2016.

Development Impact

High

The proposed encroachment is more than 30% of the NRZ and extends into the periphery of the SRZ. However, the design allows for the retention of the tree, provided that the recommended mitigation measures are implemented and adhered to. With appropriate care and protection during construction, the tree's viability can be maintained.

Action

Specialised Construction

This tree is located within a design feature intended for retention, despite high encroachment. Low-impact construction methods must be incorporated into the final design to minimise disturbance. Additional mitigation measures will be required to support long-term tree health.



Eucalyptus leucoxylon

Tree No: 8

South Australian Blue Gum

Inspected:	10 January 2023
Height:	10-15 metres
Spread:	10-15 metres
Health:	Good
Structure:	Good
Form:	Fair
Trunk Circumference:	>2 metres
Useful Life Expectancy:	>20 years
Notional Root Zone:	9.00 metres
Structural Root Zone:	3.04 metres

Observations

The health and structure of this tree indicate it is in good overall condition and has adapted to its local environment.

**Legislative Status****Regulated**

This tree has a trunk circumference greater than one metre but less than two metres, is not subject to any exemption from regulation and is therefore identified as a Regulated Tree as defined in the Planning, Development and Infrastructure Act 2016.

Development Impact**Low**

The identified encroachment is greater than 20% of the TPZ area however there are factors that indicate the proposed development will not negatively impact tree viability.

Action**Protect Root Zone**

Protect the root zone and crown in accordance with the recommendations and principles of AS4970-2025 Protection of trees on development sites.



Eucalyptus occidentalis

Tree No: 9

Swamp Yate

Inspected:	10 January 2023
Height:	10-15 metres
Spread:	10-15 metres
Health:	Good
Structure:	Fair
Form:	Fair
Trunk Circumference:	>3 metres
Useful Life Expectancy:	>10 years
Notional Root Zone:	10.78 metres
Structural Root Zone:	3.29 metres

Observations

This tree is in good health however has fair overall condition due to the presence of stable included bark in the primary trunk division. There is deadwood within the crown but not at a level that would indicate reduced health and it typical of the species.



Legislative Status

Significant

This tree has a trunk circumference greater than three metres and is not subject to any exemption from regulation and therefore it is identified as a Significant Tree as defined in the Planning, Development and Infrastructure Act 2016.

Development Impact

Conflicted

This tree is in the building envelope and the encroachment covers the entire TPZ area, the SRZ and the trunk. This tree cannot be successfully retained in this proposal.

Action

Removal Required

Tree removal is required to facilitate the proposed development.

Eucalyptus cladocalyx

Tree No: 10

Sugar Gum

Inspected:	10 January 2023
Height:	15-20 metres
Spread:	10-15 metres
Health:	Good
Structure:	Fair
Form:	Good
Trunk Circumference:	>2 metres
Useful Life Expectancy:	>10 years
Notional Root Zone:	10.32 metres
Structural Root Zone:	3.24 metres

Observations

This tree is considered to be in fair overall condition due to it having a moderate history of branch failure impacting its structural rating and therefore its overall condition. It is understood that an exploratory excavation has occurred by others and the tree owner, City of Playford; which confirms this tree will not be impacted by the path installation.

**Legislative Status****Regulated**

This tree has a trunk circumference greater than two metres but less than three metres and is not subject to any exemption from regulation and therefore it is identified as a Regulated Tree as defined in the Planning, Development and Infrastructure Act 2016.

Development Impact**High**

The proposed encroachment is more than 10% of the NRZ and extends into the SRZ. However, the design allows for the retention of the tree, provided that the recommended mitigation measures are implemented and adhered to. With appropriate care and protection during construction, the tree's viability can be maintained.

Action**Specialised Construction**

This tree is located within a design feature intended for retention, despite high encroachment. Low-impact construction methods must be incorporated into the final design to minimise disturbance. Additional mitigation measures will be required to support long-term tree health.



Eucalyptus cladocalyx

Tree No: 11

Sugar Gum

Inspected:	10 January 2023
Height:	10-15 metres
Spread:	10-15 metres
Health:	Good
Structure:	Fair
Form:	Good
Trunk Circumference:	>2 metres
Useful Life Expectancy:	>10 years
Notional Root Zone:	8.16 metres
Structural Root Zone:	2.93 metres

Observations

The tree previously declined and experienced branch failure, leading to dieback and deadwood in the upper crown. It has since recovered and formed a new crown, though there is moderate decay in the primary trunk. It is understood that an exploratory excavation has occurred by others and the tree owner, City of Playford; which confirms this tree will not be impacted by the path installation.



Legislative Status

Regulated

This tree has a trunk circumference greater than two metres but less than three metres and is not subject to any exemption from regulation and therefore it is identified as a Regulated Tree as defined in the Planning, Development and Infrastructure Act 2016.

Development Impact

High

The proposed encroachment is more than 10% of the NRZ and extends into the SRZ. However, the design allows for the retention of the tree, provided that the recommended mitigation measures are implemented and adhered to. With appropriate care and protection during construction, the tree's viability can be maintained.

Action

Specialised Construction

This tree is located within a design feature intended for retention, despite high encroachment. Low-impact construction methods must be incorporated into the final design to minimise disturbance. Additional mitigation measures will be required to support long-term tree health.



Corymbia citriodora

Tree No: 12

Lemon Scented Gum

Inspected:	10 January 2023
Height:	5-10 metres
Spread:	5-10 metres
Health:	Good
Structure:	Fair
Form:	Fair
Trunk Circumference:	>2 metres
Useful Life Expectancy:	>10 years
Notional Root Zone:	5.69 metres
Structural Root Zone:	2.51 metres

Observations

This tree is in good health, however has fair overall condition due to the presence of stable included bark in the primary trunk division. There is deadwood within the crown but not at a level that would indicate reduced health and it typical of the species.

**Legislative Status****Regulated**

This tree has a trunk circumference greater than two metres but less than three metres and is not subject to any exemption from regulation and therefore it is identified as a Regulated Tree as defined in the Planning, Development and Infrastructure Act 2016.

Development Impact**Conflicted**

This tree is in the building envelope and the encroachment covers the entire TPZ area, the SRZ and the trunk. This tree cannot be successfully retained in this proposal.

Action**Removal Required**

Tree removal is required to facilitate the proposed development.



Corymbia citriodora

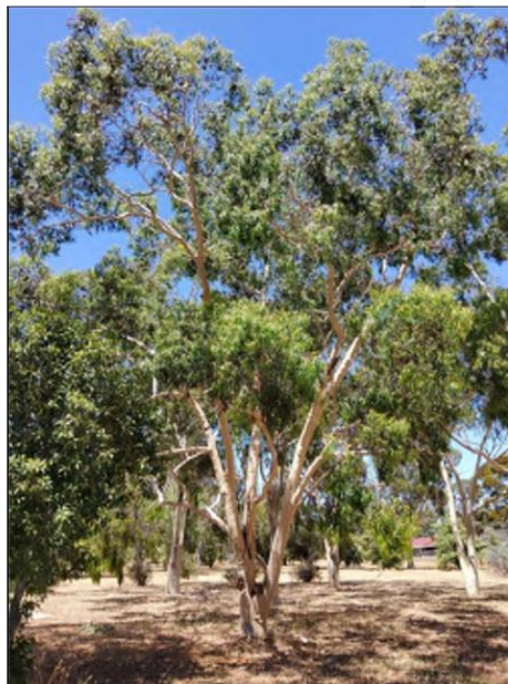
Tree No: 13

Lemon Scented Gum

Inspected:	10 January 2023
Height:	5-10 metres
Spread:	5-10 metres
Health:	Good
Structure:	Fair
Form:	Fair
Trunk Circumference:	>2 metres
Useful Life Expectancy:	>10 years
Notional Root Zone:	6.37 metres
Structural Root Zone:	2.65 metres

Observations

This tree is in good health, however has fair overall condition due to the presence of stable included bark in the primary trunk division. There is deadwood within the crown but not at a level that would indicate reduced health and it typical of the species.

**Legislative Status****Regulated**

This tree has a trunk circumference greater than two metres but less than three metres and is not subject to any exemption from regulation and therefore it is identified as a Regulated Tree as defined in the Planning, Development and Infrastructure Act 2016.

Development Impact**Conflicted**

This tree is in the building envelope and the encroachment covers the entire TPZ area, the SRZ and the trunk. This tree cannot be successfully retained in this proposal.

Action**Removal Required**

Tree removal is required to facilitate the proposed development.



Tamarix aphylla

Tree No: 14

Athel Tree

Inspected:	10 January 2023
Height:	5-10 metres
Spread:	5-10 metres
Health:	Good
Structure:	Fair
Form:	Fair
Trunk Circumference:	>2 metres
Useful Life Expectancy:	>10 years
Notional Root Zone:	7.68 metres
Structural Root Zone:	2.85 metres

Observations

This tree is considered to be in fair overall condition due to the presence of epicormic growth within the crown, otherwise it appears to be healthy and structurally sound.

**Legislative Status****Exempt**

This tree is a Declared Weed Species under the Natural Resources Management Act 2004 and is therefore exempt from control in this location.

Development Impact**Conflicted**

This tree is in the building envelope and the encroachment covers the entire TPZ area, the SRZ and the trunk. This tree cannot be successfully retained in this proposal.

Action**Removal Required**

Tree removal is required to facilitate the proposed development.



Tamarix aphylla

Tree No: 15

Athel Tree

Inspected:	10 January 2023
Height:	5-10 metres
Spread:	10-15 metres
Health:	Good
Structure:	Fair
Form:	Fair
Trunk Circumference:	>2 metres
Useful Life Expectancy:	>10 years
Notional Root Zone:	10.32 metres
Structural Root Zone:	3.23 metres

Observations

This tree is in good health, but does contain a small amount of deadwood. Its structural rating has been reduced due to the level of epicormic growth.

**Legislative Status**

Exempt

This tree has a trunk circumference greater than one metre but less than two metres, is not subject to any exemption from regulation and is therefore identified as a Regulated Tree as defined in the Planning, Development and Infrastructure Act 2016.

Development Impact

Conflicted

This tree is in the building envelope and the encroachment covers the entire TPZ area, the SRZ and the trunk. This tree cannot be successfully retained in this proposal.

Action

Removal Required

Tree removal is required to facilitate the proposed development.

Tamarix aphylla

Tree No: 16

Athel Tree

Inspected:	10 January 2023
Height:	5-10 metres
Spread:	10-15 metres
Health:	Good
Structure:	Fair
Form:	Fair
Trunk Circumference:	>2 metres
Useful Life Expectancy:	>10 years
Notional Root Zone:	7.04 metres
Structural Root Zone:	2.75 metres

Observations

This tree is in good health, but does contain a small amount of deadwood. Its structural rating has been reduced due to the level of epicormic growth.

**Legislative Status****Exempt**

This tree has a trunk circumference greater than one metre but less than two metres, is not subject to any exemption from regulation and is therefore identified as a Regulated Tree as defined in the Planning, Development and Infrastructure Act 2016.

Development Impact**Conflicted**

This tree is in the building envelope and the encroachment covers the entire TPZ area, the SRZ and the trunk. This tree cannot be successfully retained in this proposal.

Action**Removal Required**

Tree removal is required to facilitate the proposed development.



Eucalyptus cladocalyx

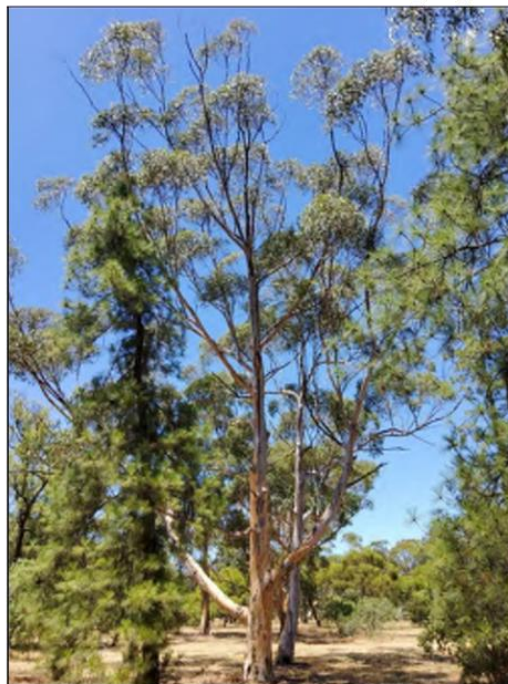
Tree No: 17

Sugar Gum

Inspected:	10 January 2023
Height:	10-15 metres
Spread:	10-15 metres
Health:	Fair
Structure:	Good
Form:	Fair
Trunk Circumference:	>2 metres
Useful Life Expectancy:	>10 years
Notional Root Zone:	8.40 metres
Structural Root Zone:	2.97 metres

Observations

This tree is considered to be in fair overall condition due to the volume of deadwood, reduced foliage density and level of dieback throughout the crown.



Legislative Status

Regulated

This tree has a trunk circumference greater than two metres but less than three metres and is not subject to any exemption from regulation and therefore it is identified as a Regulated Tree as defined in the Planning, Development and Infrastructure Act 2016.

Development Impact

Conflicted

This tree is in the building envelope and the encroachment covers the entire TPZ area, the SRZ and the trunk. This tree cannot be successfully retained in this proposal.

Action

Removal Required

Tree removal is required to facilitate the proposed development.



Tamarix aphylla

Tree No: 18

Athel Tree

Inspected:	10 January 2023
Height:	10-15 metres
Spread:	5-10 metres
Health:	Good
Structure:	Fair
Form:	Fair
Trunk Circumference:	>2 metres
Useful Life Expectancy:	>10 years
Notional Root Zone:	5.06 metres
Structural Root Zone:	2.39 metres

Observations

This tree is considered to be in fair overall condition due to a moderate history of branch failure, the presence of a stable included bark union in the primary structure and a low level of deadwood within the crown.

**Legislative Status****Exempt**

This tree has a trunk circumference greater than one metre but less than two metres, is not subject to any exemption from regulation and is therefore identified as a Regulated Tree as defined in the Planning, Development and Infrastructure Act 2016.

Development Impact**Conflicted**

This tree is in the building envelope and the encroachment covers the entire TPZ area, the SRZ and the trunk. This tree cannot be successfully retained in this proposal.

Action**Removal Required**

Tree removal is required to facilitate the proposed development.

Tamarix aphylla

Tree No: 19

Athel Tree

Inspected:	10 January 2023
Height:	10-15 metres
Spread:	5-10 metres
Health:	Good
Structure:	Fair
Form:	Fair
Trunk Circumference:	>2 metres
Useful Life Expectancy:	>10 years
Notional Root Zone:	7.65 metres
Structural Root Zone:	2.85 metres

Observations

This tree is considered to be in fair overall condition due to a moderate history of branch failure, the presence of a stable included bark union in the primary structure and a low level of deadwood within the crown.

**Legislative Status**

Exempt

This tree has a trunk circumference greater than one metre but less than two metres, is not subject to any exemption from regulation and is therefore identified as a Regulated Tree as defined in the Planning, Development and Infrastructure Act 2016.

Development Impact

Conflicted

This tree is in the building envelope and the encroachment covers the entire TPZ area, the SRZ and the trunk. This tree cannot be successfully retained in this proposal.

Action

Removal Required

Tree removal is required to facilitate the proposed development.

Tamarix aphylla

Tree No: 20

Athel Tree

Inspected:	10 January 2023
Height:	10-15 metres
Spread:	5-10 metres
Health:	Good
Structure:	Fair
Form:	Fair
Trunk Circumference:	>3 metres
Useful Life Expectancy:	>10 years
Notional Root Zone:	8.97 metres
Structural Root Zone:	3.05 metres

Observations

This tree is considered to be in fair overall condition due to a moderate history of branch failure, the presence of a stable included bark union in the primary structure and a low level of deadwood within the crown.

**Legislative Status****Exempt**

This tree has a trunk circumference greater than two metres, is not subject to any exemption from regulation and is therefore identified as a Significant Tree as defined in the Planning, Development and Infrastructure Act 2016.

Development Impact**Conflicted**

This tree is in the building envelope and the encroachment covers the entire TPZ area, the SRZ and the trunk. This tree cannot be successfully retained in this proposal.

Action**Removal Required**

Tree removal is required to facilitate the proposed development.

Eucalyptus cladocalyx

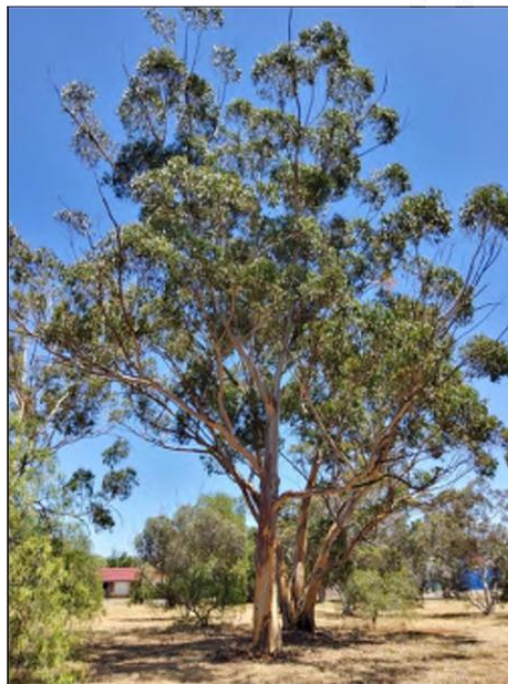
Tree No: 21

Sugar Gum

Inspected:	10 January 2023
Height:	10-15 metres
Spread:	5-10 metres
Health:	Good
Structure:	Good
Form:	Fair
Trunk Circumference:	>2 metres
Useful Life Expectancy:	>20 years
Notional Root Zone:	8.04 metres
Structural Root Zone:	2.92 metres

Observations

The health and structure of this tree indicate it is in good overall condition and has adapted to its local environment. There is obvious deadwood within the crown, however this is within normal levels and not an indicator of reduced health.



Legislative Status

Regulated

This tree has a trunk circumference greater than two metres but less than three metres and is not subject to any exemption from regulation and therefore it is identified as a Regulated Tree as defined in the Planning, Development and Infrastructure Act 2016.

Development Impact

Conflicted

This tree is in the building envelope and the encroachment covers the entire TPZ area, the SRZ and the trunk. This tree cannot be successfully retained in this proposal.

Action

Removal Required

Tree removal is required to facilitate the proposed development.

Eucalyptus cladocalyx

Tree No: 22

Sugar Gum

Inspected:	10 January 2023
Height:	10-15 metres
Spread:	5-10 metres
Health:	Poor
Structure:	Good
Form:	Fair
Trunk Circumference:	>3 metres
Useful Life Expectancy:	<10 years
Notional Root Zone:	10.66 metres
Structural Root Zone:	3.28 metres

Observations

This tree is considered to be in poor overall condition due to the level of dieback and deadwood throughout the crown.



Legislative Status

Significant

This tree has a trunk circumference greater than three metres and is not subject to any exemption from regulation and therefore it is identified as a Significant Tree as defined in the Planning, Development and Infrastructure Act 2016.

Development Impact

Conflicted

This tree is in the building envelope and the encroachment covers the entire TPZ area, the SRZ and the trunk. This tree cannot be successfully retained in this proposal.

Action

Removal Required

Tree removal is required to facilitate the proposed development.

Eucalyptus cladocalyx

Tree No: 23

Sugar Gum

Inspected:	29 January 2024
Height:	10-15 metres
Spread:	5-10 metres
Health:	Fair
Structure:	Poor
Form:	Fair
Trunk Circumference:	>2 metres
Useful Life Expectancy:	<10 years
Notional Root Zone:	6.38 metres
Structural Root Zone:	2.67 metres

Observations

The level of deadwood, dieback, poor quality trunk and branch unions, with the decay and hollowing in the primary structure indicate this tree is in poor overall condition. Additionally, there is an active beehive within the hollow in the main trunk division.



Legislative Status

Regulated

This tree has a trunk circumference greater than two metres but less than three metres and is not subject to any exemption from regulation and therefore it is identified as a Regulated Tree as defined in the Planning, Development and Infrastructure Act 2016.

Development Impact

Conflicted

This tree is in the building envelope and the encroachment covers the entire TPZ area, the SRZ and the trunk. This tree cannot be successfully retained in this proposal.

Action

Removal Required

Tree removal is required to facilitate the proposed development.

Eucalyptus cladocalyx

Tree No: 24

Sugar Gum

Inspected:	29 January 2024
Height:	15-20 metres
Spread:	10-15 metres
Health:	Good
Structure:	Good
Form:	Good
Trunk Circumference:	metres
Useful Life Expectancy:	>20 years
Notional Root Zone:	7.56 metres
Structural Root Zone:	3.11 metres

Observations

The health and structure of this tree indicate it is in good overall condition and has adapted to its local environment. There is obvious deadwood within the crown, however this is within normal levels and not an indicator of reduced health.



Legislative Status

Unregulated

This tree does not achieve a regulated trunk circumference and therefore is not regulated by the Planning, Development and Infrastructure Act 2016.

Development Impact

Conflicted

This tree is in the building envelope and the encroachment covers the entire TPZ area, the SRZ and the trunk. This tree cannot be successfully retained in this proposal.

Action

Removal Required

Tree removal is required to facilitate the proposed development.

Eucalyptus cladocalyx

Tree No: 25

Sugar Gum

Inspected:	29 January 2024
Height:	15-20 metres
Spread:	15-20 metres
Health:	Good
Structure:	Fair
Form:	Fair
Trunk Circumference:	>2 metres
Useful Life Expectancy:	>10 years
Notional Root Zone:	9.12 metres
Structural Root Zone:	3.11 metres

Observations

This tree is in good health but has reduced structure rating due to the presence of decay, epicormic growth and a history of branch failure. Additionally, there is minor hollowing at the base of the main trunk, with an active beehive.

**Legislative Status****Regulated**

This tree has a trunk circumference greater than two metres but less than three metres and is not subject to any exemption from regulation and therefore it is identified as a Regulated Tree as defined in the Planning, Development and Infrastructure Act 2016.

Development Impact**Conflicted**

This tree is in the building envelope and the encroachment covers the entire TPZ area, the SRZ and the trunk. This tree cannot be successfully retained in this proposal.

Action**Removal Required**

Tree removal is required to facilitate the proposed development.



Eucalyptus cladocalyx

Tree No: 26

Sugar Gum

Inspected:	29 January 2024
Height:	10-15 metres
Spread:	10-15 metres
Health:	Good
Structure:	Fair
Form:	Fair
Trunk Circumference:	>2 metres
Useful Life Expectancy:	>10 years
Notional Root Zone:	7.80 metres
Structural Root Zone:	3.08 metres

Observations

The tree is considered to be in fair overall condition as evidenced by the natural deadwood within the crown, the minor level of decay and epicormic growth.



Legislative Status

Regulated

This tree has a trunk circumference greater than two metres but less than three metres and is not subject to any exemption from regulation and therefore it is identified as a Regulated Tree as defined in the Planning, Development and Infrastructure Act 2016.

Development Impact

Conflicted

This tree is in the building envelope and the encroachment covers the entire TPZ area, the SRZ and the trunk. This tree cannot be successfully retained in this proposal.

Action

Removal Required

Tree removal is required to facilitate the proposed development.



Appendix C - Mapping





Appendix D - Tree Assessment Summary



Tree Assessment Summary

Tree No.	Botanic Name	Legislative Status	Development Impact	NRZ Radius	Observations	Action
4	<i>Schinus areira</i>	Exempt	High	9.20 metres	The tree is in good health, although it does retain an increased percentage of deadwood, and has a slightly reduced structure rating due to an included bark union in the primary structure.	Specialised Construction
5	<i>Eucalyptus leucoxylon</i>	Regulated	High	8.40 metres	This tree is considered to be in fair overall condition due to the moderately increased level of deadwood and dieback evident throughout the crown.	Specialised Construction
6	<i>Eucalyptus cladocalyx</i>	Regulated	High	9.60 metres	This tree is considered to be in fair overall condition due to the presence of epicormic growth within the crown and moderate level of decay within the primary structure, otherwise it appears to be healthy and structurally sound.	Specialised Construction
7	<i>Eucalyptus cladocalyx</i>	Regulated	High	8.88 metres	This tree is considered to be in fair overall condition due to the presence of epicormic growth within the crown, otherwise it appears to be healthy and structurally sound.	Specialised Construction
8	<i>Eucalyptus leucoxylon</i>	Regulated	Low	9.00 metres	The health and structure of this tree indicate it is in good overall condition and has adapted to its local environment.	Protect Root Zone
9	<i>Eucalyptus occidentalis</i>	Significant	Conflicted	10.78 metres	This tree is in good health however has fair overall condition due to the presence of stable included bark in the primary trunk division. There is deadwood within the crown but not at a level that would indicate reduced health and it typical of the species.	Removal Required
10	<i>Eucalyptus cladocalyx</i>	Regulated	High	10.32 metres	This tree is considered to be in fair overall condition due to it having a moderate history of branch failure impacting its structural rating and therefore its overall condition. It is understood that an exploratory excavation has occurred by others and the tree owner, City of Playford; which confirms this tree will not be impacted by the path installation.	Specialised Construction



Tree Assessment Summary

Tree No.	Botanic Name	Legislative Status	Development Impact	NRZ Radius	Observations	Action
11	<i>Eucalyptus cladocalyx</i>	Regulated	High	8.16 metres	The tree previously declined and experienced branch failure, leading to dieback and deadwood in the upper crown. It has since recovered and formed a new crown, though there is moderate decay in the primary trunk. It is understood that an exploratory excavation has occurred by others and the tree owner, City of Playford; which confirms this tree will not be impacted by the path installation.	Specialised Construction
12	<i>Corymbia citriodora</i>	Regulated	Conflicted	5.69 metres	This tree is in good health, however has fair overall condition due to the presence of stable included bark in the primary trunk division. There is deadwood within the crown but not at a level that would indicate reduced health and it typical of the species.	Removal Required
13	<i>Corymbia citriodora</i>	Regulated	Conflicted	6.37 metres	This tree is in good health, however has fair overall condition due to the presence of stable included bark in the primary trunk division. There is deadwood within the crown but not at a level that would indicate reduced health and it typical of the species.	Removal Required
14	<i>Tamarix aphylla</i>	Exempt	Conflicted	7.68 metres	This tree is considered to be in fair overall condition due to the presence of epicormic growth within the crown, otherwise it appears to be healthy and structurally sound.	Removal Required
15	<i>Tamarix aphylla</i>	Exempt	Conflicted	10.32 metres	This tree is in good health, but does contain a small amount of deadwood. Its structural rating has been reduced due to the level of epicormic growth.	Removal Required
16	<i>Tamarix aphylla</i>	Exempt	Conflicted	7.04 metres	This tree is in good health, but does contain a small amount of deadwood. Its structural rating has been reduced due to the level of epicormic growth.	Removal Required
17	<i>Eucalyptus cladocalyx</i>	Regulated	Conflicted	8.40 metres	This tree is considered to be in fair overall condition due to the volume of deadwood, reduced foliage density and level of dieback throughout the crown.	Removal Required
18	<i>Tamarix aphylla</i>	Exempt	Conflicted	5.06 metres	This tree is considered to be in fair overall condition due to a moderate history of branch failure, the presence of a stable included bark union in the primary structure and a low level of deadwood within the crown.	Removal Required



Tree Assessment Summary

Tree No.	Botanic Name	Legislative Status	Development Impact	NRZ Radius	Observations	Action
19	<i>Tamarix aphylla</i>	Exempt	Conflicted	7.65 metres	This tree is considered to be in fair overall condition due to a moderate history of branch failure, the presence of a stable included bark union in the primary structure and a low level of deadwood within the crown.	Removal Required
20	<i>Tamarix aphylla</i>	Exempt	Conflicted	8.97 metres	This tree is considered to be in fair overall condition due to a moderate history of branch failure, the presence of a stable included bark union in the primary structure and a low level of deadwood within the crown.	Removal Required
21	<i>Eucalyptus cladocalyx</i>	Regulated	Conflicted	8.04 metres	The health and structure of this tree indicate it is in good overall condition and has adapted to its local environment. There is obvious deadwood within the crown, however this is within normal levels and not an indicator of reduced health.	Removal Required
22	<i>Eucalyptus cladocalyx</i>	Significant	Conflicted	10.66 metres	This tree is considered to be in poor overall condition due to the level of dieback and deadwood throughout the crown.	Removal Required
23	<i>Eucalyptus cladocalyx</i>	Regulated	Conflicted	6.38 metres	The level of deadwood, dieback, poor quality trunk and branch unions, with the decay and hollowing in the primary structure indicate this tree is in poor overall condition. Additionally, there is an active beehive within the hollow in the main trunk division.	Removal Required
24	<i>Eucalyptus cladocalyx</i>	Unregulated	Conflicted	7.56 metres	The health and structure of this tree indicate it is in good overall condition and has adapted to its local environment. There is obvious deadwood within the crown, however this is within normal levels and not an indicator of reduced health.	Removal Required
25	<i>Eucalyptus cladocalyx</i>	Regulated	Conflicted	9.12 metres	This tree is in good health but has reduced structure rating due to the presence of decay, epicormic growth and a history of branch failure. Additionally, there is minor hollowing at the base of the main trunk, with an active beehive.	Removal Required
26	<i>Eucalyptus cladocalyx</i>	Regulated	Conflicted	7.80 metres	The tree is considered to be in fair overall condition as evidenced by the natural deadwood within the crown, the minor level of decay and epicormic growth.	Removal Required



Appendix E - Tree Protection Zone Guidelines



Tree Protection Zone General Specifications and Guidelines

The Tree Protection Zone(s) is identified on the site plan, the TPZ is an area where construction activities are regulated for the purposes of protecting tree viability. The TPZ should be established so that it clearly identifies and precludes development/construction activities including personnel.

If development activities are required within the TPZ then these activities must be reviewed and approved by the Project Arborist. Prior to approval, the Project Arborist must be certain that the tree(s) will remain viable as a result of this activity.

Work Activities Excluded from the Tree Protection Zone:

- a) Machine excavation including trenching;
- b) Excavation for silt fencing;
- c) Cultivation;
- d) Storage;
- e) Preparation of chemicals, including preparation of cement products;
- f) Parking of vehicles and plant;
- g) Refuelling;
- h) Dumping of waste;
- i) Wash down and cleaning of equipment;
- j) Placement of fill;
- k) Lighting of fires;
- l) Soil level changes;
- m) Temporary or permanent installation of utilities and signs, and
- n) Physical damage to the tree.
- o) Any other activity that could impact on the tree.



Protective Fencing

Protective fencing must be installed around the identified Tree Protection Zone (See Figure1). The fencing should be chain wire panels and compliant with AS4687-2007 *Temporary fencing and hoardings*. Shade cloth or similar material should be attached around the fence to reduce dust, other particulates and liquids entering the protected area.

Temporary fencing on 28kg bases are recommended for use as this eliminates any excavation requirements to install fencing. Excavation increases the likelihood of root damage therefore should be avoided where possible throughout the project.

Existing perimeter fencing and other structures may be utilised as part of the protective fencing.

Any permanent fencing should be post and rail with the set out determined in consultation with the Project Arborist.

Where the erection of the fence is not practical the Project Arborist is to approve alternative measures.

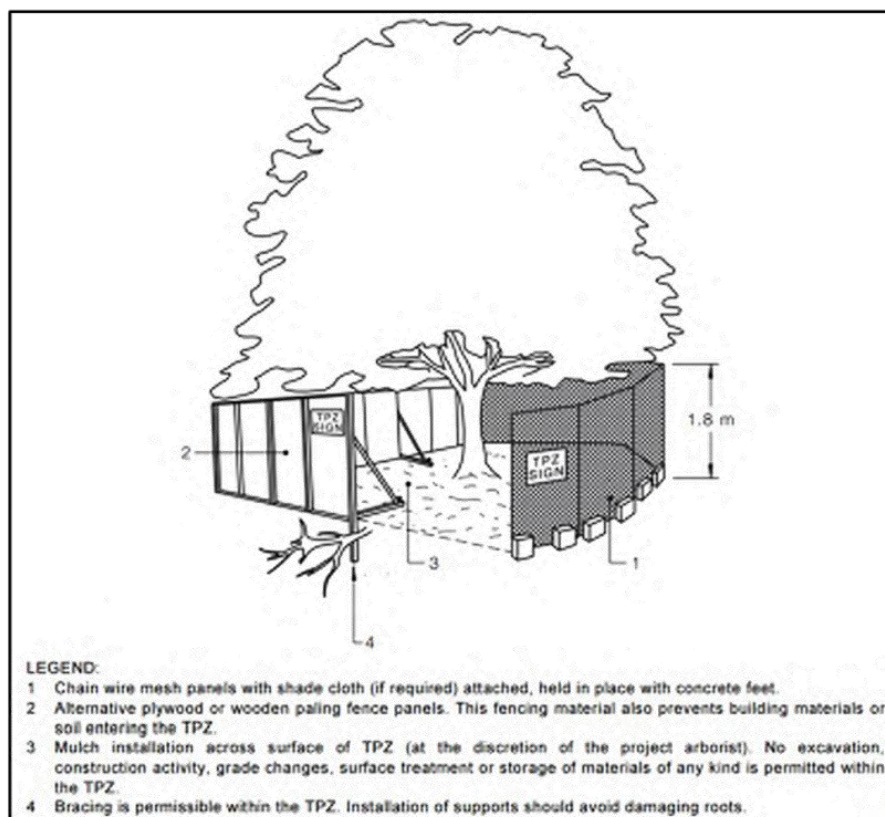


Figure 1 Showing example of protection fencing measures suitable.

Signage

The TPZ must be clearly identified with signs placed around the edge of the TPZ and be visible within the development site. Example of a Tree Protection Zone Sign at the end of this document.



Other Protection Measures

There are other protection methods that should be implemented within the development site and these include:-

General

When a TPZ exclusion area cannot be established due to practical reasons or the area needs to be entered to undertake construction activities then additional tree protection measures may need to be adopted. Protection measures should be compliant with AS4970-2025 and approved by the Project Arborist.

Installation of Scaffolding within Tree Protection Area.

Where scaffolding is required within the TPZ branch removal should be minimised. Any branch removal required should be approved by the Project Arborist and performed by a certified Arborist and performed in accordance with AS4373-2007. Approval to prune branches must be documented and maintained.

Ground below scaffold should be protected by boarding (e.g. scaffold board or plywood sheeting) as shown in Figure below. The boarding should be left in place until scaffolding is removed.

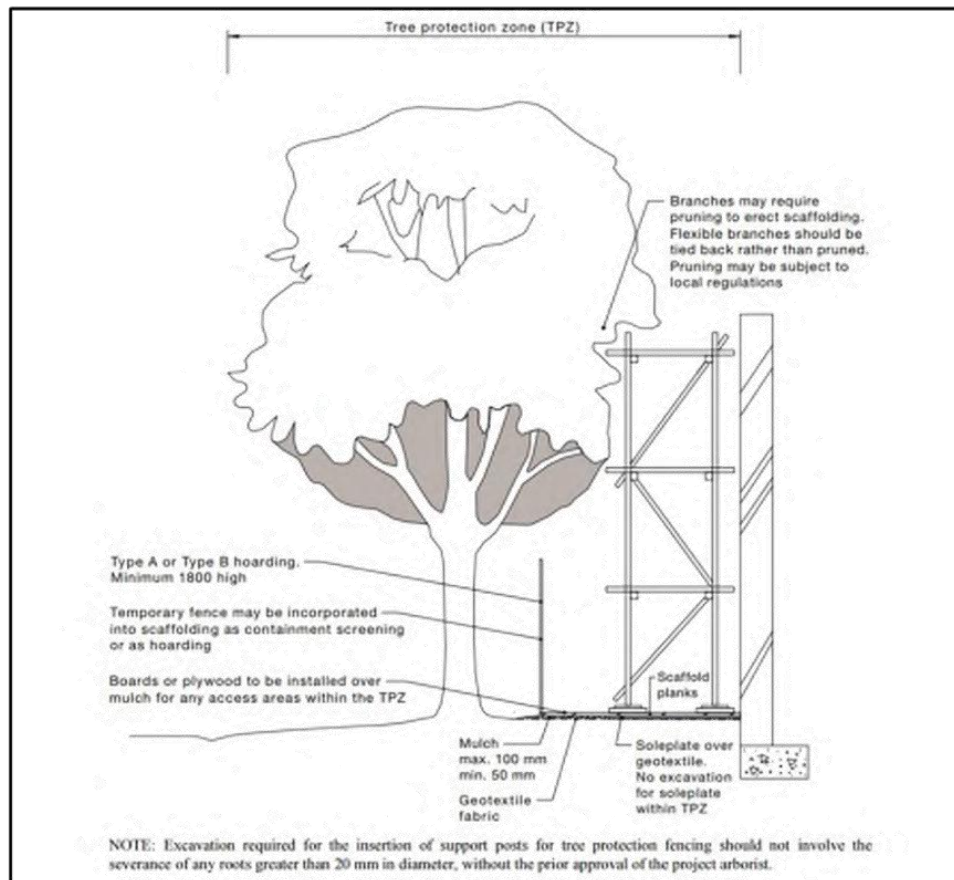


Figure 2 – Showing scaffold constructed within TPZ.



Ground Protection and TPZ Access

Temporary access within the TPZ can be achieved by the installation of suitable ground protection. The purpose of ground protection is to prevent damage to tree roots and avoid compaction of the soil.

Ground protection methods include the placement of a permeable membrane beneath a layer of non-compactable material such as mulch or a no fines gravel which is in turn covered with rumble boards or steel plates.

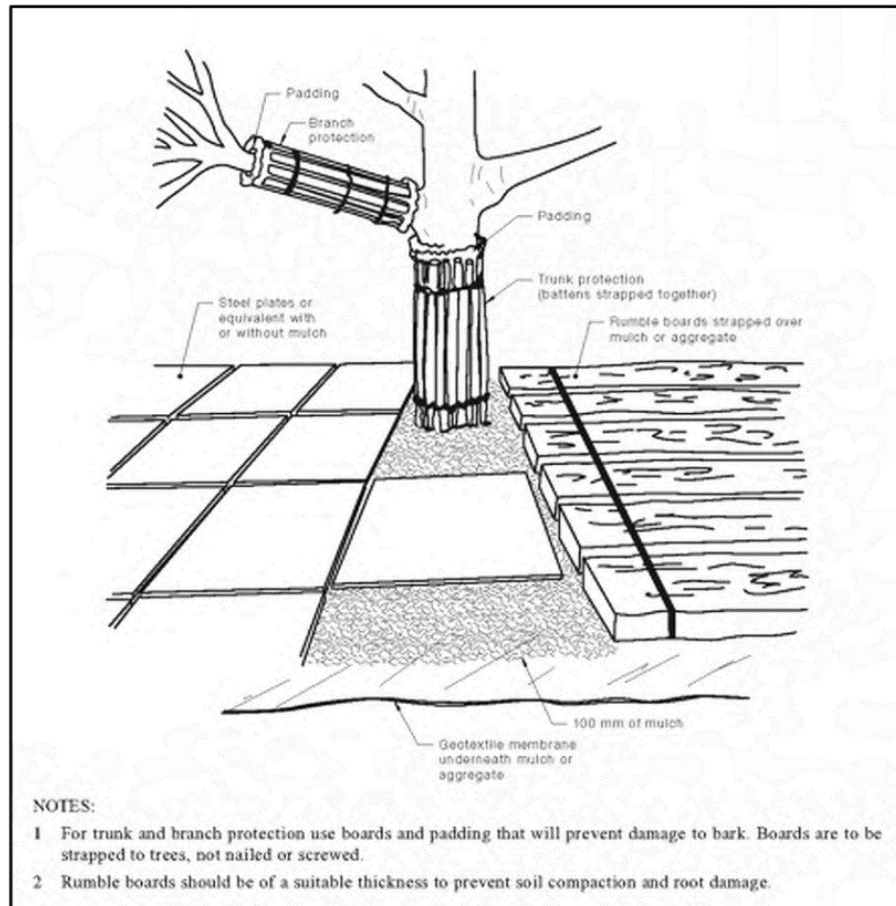


Figure 3 – Ground protection methods.

Document Source:

The previous three diagrams in this document are sourced from AS4970-2025 Protection of trees on development sites. Further information and guidelines are available in within that document.



Paving Construction within a Tree Protection Zone

Paving within any Tree Protection Zone (TPZ) must be carried out above natural ground level unless it can be shown with non-destructive excavation (AirSpade® or similar) that no or insignificant root growth occupies the proposed construction area.

Due to the adverse effect filling over a Tree Protection Zone (TPZ) can have on tree health; alternative mediums other than soil must be used. Available alternative mediums include structural soils or the use of a cellular confinement system such as *Ecocell*®.

Ecocell®

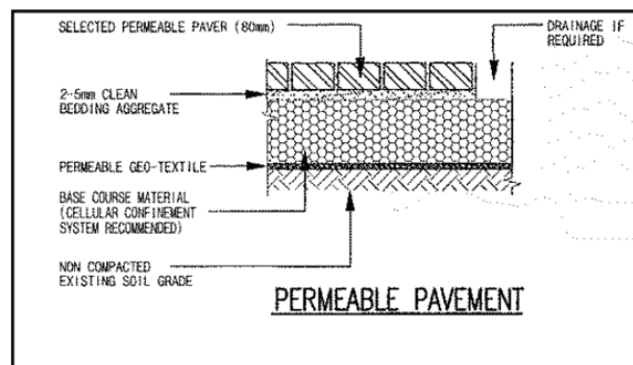
Ecocell® systems are a cellular confinement system that can be filled with large particle sized gravels as a sub-base for paving systems to reduce compaction to the existing grade.

Site preparation

- Clearly outline to all contracting staff entering the site the purpose of the TPZ's and the contractors' responsibilities. No fence is to be moved and no person or machinery is to access the TPZ's without consent from the local council and/or the Project Arborist.
- Fence off the unaffected area of the TPZ with a temporary fence leaving a 1.5 metre gap between the work area and the fence; this will prevent machinery access to the remaining root zone.

Installation of Ecocell® and EcoTrihex Paving®

- Install a non-woven geotextile fabric for drainage and separation from sub base with a minimum of 600mm overlap on all fabric seams as required.
- Add Ecocell®, fill compartments with gravel and compact to desired compaction rate.
- If excessive groundwater is expected incorporate an appropriate drainage system within the bedding sand level.
- Add paving sand to required depth and compact to paving manufacturer's specifications.
- Lay EcoTrihex Paving® as per manufactures specifications and fill gaps between pavers with no fines gravel.
- Remove all debris, vegetation cover and unacceptable in-situ soils. No excavation or soil level change of the sub base is allowable for the installation of the paving.
- Where the finished soil level is uneven, gullies shall be filled with 20 millimetre coarse gravel to achieve the desired level.



This construction method if implemented correctly can significantly reduce and potentially eliminated the risk of tree decline and/or structural failure and effectively increase the size of the Tree Protection Zone to include the area of the paving.



Certificates of Control

Stage in development	Tree management process	
	Matters for consideration	Actions and certification
Development submission	Identify trees for retention through comprehensive arboricultural impact assessment of proposed construction. Determine tree protection measures Landscape design	Provide arboricultural impact assessment including tree protection plan (drawing) and specification
Development approval	Development controls Conditions of consent	Review consent conditions relating to trees
Pre-construction (Sections 4 and 5)		
Initial site preparation	State based OHS requirements for tree work Approved retention/removal Refer to AS 4373 for the requirements on the pruning of amenity trees Specifications for tree protection measures	Compliance with conditions of consent Tree removal/tree retention/transplanting Tree pruning Certification of tree removal and pruning Establish/delineate TPZ Install protective measures Certification of tree protection measures
Construction (Sections 4 and 5)		
Site establishment	Temporary infrastructure Demolition, bulk earthworks, hydrology	Locate temporary infrastructure to minimize impact on retained trees Maintain protective measures Certification of tree protection measures
Construction work	Liaison with site manager, compliance Deviation from approved plan	Maintain or amend protective measures Supervision and monitoring
Implement hard and soft landscape works	Installation of irrigation services Control of compaction work Installation of pavement and retaining walls	Remove selected protective measures as necessary Remedial tree works Supervision and monitoring
Practical completion	Tree vigour and structure	Remove all remaining tree protection measures Certification of tree protection
Post construction (Section 5)		
Defects liability/maintenance period	Tree vigour and structure	Maintenance and monitoring Final remedial tree works Final certification of tree condition

Document Source:

This table has been sourced from AS4970-2025 Protection of trees on development sites. Further information and guidelines are available in within that document.



APPENDIX 7: ACOUSTIC REPORT

Large Format Leisure, Playford

Lot 12 Ashfield Road, Elizabeth

Environmental Noise Assessment

S7578C5

September 2025

sonus.

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Large Format Leisure, Playford
Environmental Noise Assessment
S7578C5
September 2025

sonus.

Document Title : Large Format Leisure, Playford
Environmental Noise Assessment
Document Reference : S7578C5
Date : September 2025
Author : Tim Aplin, MAAS
Reviewer : Simon Moore, MAAS

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Large Format Leisure, Playford
Environmental Noise Assessment
S7578C5
September 2025

sonus.

1 INTRODUCTION

An environmental noise assessment has been undertaken for the proposed development (the **Development**) at Lot 12 Ashfield Road, Elizabeth (the **Subject Site**).

The Development consists of the construction of a new building which is proposed to house a new Ice Arena, a gym, and an additional retail/commercial tenancy. An at-grade car park is also included as part of the Development. The new Ice Arena is proposing to host Australian Ice Hockey League (**AIHL**) games on Saturday afternoons. Thirteen AIHL games are proposed to be held at the facility every year.

The closest existing sensitive receivers to the site are located to the west of the Subject Site on the opposite side of Ashfield Road. An overview of the Subject Site and surrounding area is shown in Figure 1.

The assessment considers noise levels at sensitive receivers in the locality from the Development, including car parking and vehicular activity, mechanical plant, and music.

The assessment has been based on:

- PLANNING DOCUMENTATION SEPTEMBER 2025 by *Bell* for "Lot 12, Pelligra Ice Arena, Playford, SA", job number "BAA250010", Revision "C", issued on 16 September 2025.
- Traffic estimates provided by email on 1 February 2023.
- Estimates of gym attendance provided by email on 4 September 2025
- Previous noise measurements and sound power data obtained from similar sites for the car parking and vehicular activity, mechanical plant, and music.
- The proposed operating hours being as follows:
 - 6:00am to 10:00pm for the Ice Arena, with peak activity associated with AIHL games occurring during the following times:
 - 6:00am to 7:00am on Saturday mornings for practices
 - 4:00pm to 7:00pm on Saturday afternoon/evenings for games
 - 24/7 operation for the gym
 - The retail/commercial tenancy operating between 7:00am and 10:00pm
- Any private rubbish collection at the site will be limited to the times of the day defined to not constitute a 'local nuisance' by the *Local Nuisance and Litter Control Act 2016*, which are:
 - Between 9:00am and 7:00pm on any Sunday or public holiday.
 - Between 7:00am and 7:00pm on any other day.

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Figure 1: Subject Site and locality.

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2 PLANNING AND DESIGN CODE

The Development is subject to the provisions of the South Australian *Planning and Design Code* (the **Code**) under the *Planning, Development and Infrastructure Act 2016*.

In accordance with the Code, the Subject Site is located in both the *Urban Activity Centre Zone* and *General Neighbourhood Zone*. Existing sensitive receivers are located to the west of the Subject Site on the opposite side of Ashfield Road.

There is vacant land in the *General Neighbourhood Zone* adjacent the southern boundary of the Subject Site and to the east on the opposite side of Main North Road. As the *General Neighbourhood Zone* is primarily intended to accommodate sensitive receivers, in accordance with Performance Outcome (PO) 1.2 of the *Interface between Land Uses* module of the *General Development Policies* of the Code, future sensitive receivers within these allotments have also been considered.

The Code has been reviewed and the provisions considered relevant to the assessment are included in Appendix A.

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3 EXISTING NOISE ENVIRONMENT

In order to inform the assessment, continuous noise monitoring was conducted at the location shown in Figure 1 on the Subject Site. The location was chosen to be representative of the existing acoustic environment at the sensitive receivers on the opposite side of Ashfield Road.

The noise monitoring was conducted from 22 to 30 November 2022 to provide an indication of the noise level variation over the course of each day and over the course a typical week.

The results of the noise monitoring indicate that the existing acoustic environment is dominated by traffic noise and can be characterised as follows for the day (7:00am to 10:00pm) and night (10:00pm to 7:00am):

- Background (L_{90}) noise levels:
 - Generally in the range of 50-55 dB(A) during the day with the lowest measured being 46 dB(A).
 - Generally in the range of 35-45 dB(A) during the night with the lowest measured being 31 dB(A).
- Average (L_{eq}) noise levels:
 - Generally in the range of 55-60 dB(A) during the day with the lowest measured being 53 dB(A).
 - Generally in the range of 45-55 dB(A) during the night with the lowest measured being 42 dB(A).
- Maximum instantaneous (L_{max}) noise levels:
 - Generally in the range of 70-80 dB(A) during the day with the lowest measured being 63 dB(A).
 - Generally in the range of 60-70 dB(A) during the night with the lowest measured being 54 dB(A).

The following noise levels were the lowest measured during specific peak periods of the Development:

- Between 6:00am and 7:00am on Saturday morning:
 - A background noise level of 50 dB(A)
 - An average noise level of 55 dB(A)
 - A maximum instantaneous noise level of 72 dB(A).
- Between 4:00pm and 7:00pm on Saturday afternoon:
 - A background noise level of 53 dB(A)
 - An average noise level of 57 dB(A)
 - A maximum instantaneous noise level of 69 dB(A).

The results of the noise monitoring are provided in Appendix C.

4 MUSIC NOISE

4.1 Criteria

PO 4.6 of the *Interface between Land Uses* module under the *General Development Policies* of the Code requires that any development incorporating music *achieves suitable acoustic amenity when measured at the boundary of an adjacent sensitive receiver (or lawfully approved sensitive receiver) or zone primarily intended to accommodate sensitive receivers*. The Deemed-to-Satisfy / Designated Performance Feature (DTS/DPF) for PO 4.6 establishes music noise criteria based on the existing background noise levels in the environment. Therefore, the background noise levels have been measured continuously at a location on the subject site (shown in Figure 1) selected to be representative of the most noise affected sensitive receivers. The results of the noise monitoring are provided in Appendix C.

The results of the noise monitoring have been analysed to determine the lowest background noise levels measured. Criteria for the assessment of music have been developed in accordance with the provisions of DTS/DPF 4.6, which is consistent with the requirements of the Environment Protection Authority (EPA) guidelines *Assessing music noise from indoor venues* (the **EPA Guidelines**).

The lowest measured background noise levels ($L_{A90,15min}$) are shown in Table 1. Also shown are the resulting music noise criteria ($L_{A10,15min}$) applicable at nearby sensitive receivers.

Table 1: Lowest background noise levels and music noise criteria.

	Noise Level [dB(A)] in Octave Band Centre Frequency [Hz]						
	63	125	250	500	1000	2000	4000
Lowest measured background noise level ($L_{A90,15min}$)	17	23	20	23	26	21	17
Music noise criteria ($L_{A10,15min}$) (8 dB above background noise level in octave bands)	25	31	28	31	34	29	25

4.2 Assessment

Typical music noise levels played within gyms have previously been measured at other similar facilities and is considered to be a typical level for gyms without group classes. The levels shown in Table 2 are based on the previous measurements and have been used as the basis for this assessment.

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Table 2: Music level within the gym.

	Noise Level ($L_{10,15min}$ dB(A)) by Octave Band Centre Frequency (Hz)							Overall Noise Level (dB(A))
	63	125	250	500	1000	2000	4000	
Gym music level (reverberant level)	52	63	66	70	71	65	58	75

A noise model of the site and surrounding area has been developed using *SoundPLANnoise* noise modelling software and the *ISO 9613-2: 2024-01* prediction method to predict noise levels for sensitive receivers in the locality. The noise model considers the music level shown in Table 2, the construction of the building envelope, the distance to receivers, and the effects of barriers such as fences and buildings.

4.3 Recommendations

Based on the noise model, and music playing at the level shown in Table 2, the following acoustic treatments are recommended to achieve the music noise criteria at all sensitive receivers:

- The roof should be constructed using at least 0.42mm BMT sheet steel (*Colorbond or similar*).
- All glazing to the Western Elevation of the gym tenancy should be constructed using at least 10mm thick monolithic or laminated glass.
- Construct an airlock entry/exit to the gym which incorporates self-closing mechanisms on the doors. Note that the airlock should be long enough to ensure that the first door closes before the second door is opened.
- Ensure that any music playing within the gym is turned off while the roller door is opened.
- If higher music levels are needed for group fitness classes, it is recommended that a dedicated acoustically treated internal space be constructed within the larger space (note that if located along the Western Elevation the glazing may need to be upgraded). This can be investigated during the detail design stage if necessary.

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4.4 Predicted Music Noise Levels

Based on the incorporation of the recommended acoustic treatments, the highest predicted noise level for any sensitive receiver is shown and compared with the music noise criteria in Table 3.

Table 3: Comparison of the music noise criteria and highest noise level predicted at sensitive receivers.

	Noise Level [dB(A)] in Octave Band Centre Frequency [Hz]						
	63	125	250	500	1000	2000	4000
Music noise criteria (LA10,15min)	25	31	28	31	34	29	25
Highest predicted noise level (LA10,15min)	23	27	26	31	32	26	17

As shown, subject to the incorporation of the recommended acoustic treatments, the music noise criteria are predicted to be achieved at all sensitive receivers in the locality.

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5 OTHER NOISE SOURCES

5.1 Criteria

5.1.1 Planning and Design Code

PO 4.1 of the *Interface between Land Uses* module under the *General Development Policies* of the Code specifically requires that noise from a development *does not unreasonably impact the amenity of sensitive receivers (or lawfully approved sensitive receivers)*. The DTS/DPF for PO 4.1 references the *Environment Protection (Commercial and Industrial Noise) Policy 2023* (the **Policy**).

5.1.2 Environment Protection (Commercial and Industrial Noise) Policy 2023

The Policy provides objective noise criteria to assess the environmental noise emissions from a proposed development. The noise criteria provided by the Policy are based on the *World Health Organisation Guidelines for Community Noise (1999)* (the **WHO Guidelines**), which provides recommendations for appropriate noise levels to prevent community annoyance, sleep disturbance and adverse impact on the amenity of a locality. Therefore, compliance with the Policy is considered to satisfy the WHO Guidelines and the subjective requirements of the Code relating to environmental noise.

The Policy establishes indicative noise levels to be achieved at sensitive receivers based on the zones in which the noise source (the Development) and sensitive receivers (the residences) are located. For new developments, Part 5 of the Policy provides goal noise levels which are 5 dB(A) more onerous than those which would otherwise apply. It is noted that this 5 dB(A) reduction in the criteria does not apply to future sensitive receivers.

The Policy defines the day period as the hours from 7:00am to 10:00pm (the **day period**) and the night period as the hours from 10:00pm to 7:00am (the **night period**).

Based on the above, the Policy provides the following goal noise levels to be achieved at sensitive receivers:

- At existing sensitive receivers:
 - An average (L_{eq}) noise level of 50 dB(A) during the day period.
 - An average (L_{eq}) noise level of 43 dB(A) during the night period.
 - A maximum instantaneous (L_{max}) noise level of 60 dB(A) during the night period.
- At future sensitive receivers:
 - An average (L_{eq}) noise level of 55 dB(A) during the day period.
 - An average (L_{eq}) noise level of 48 dB(A) during the night period.

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Clause 19 of the Policy, which contains the provisions for governing development authorisation applications, describes other factors to consider when determining compliance with the overarching goals of the Policy, beyond simply complying with the noise goals. The Policy allows consideration to be given to components of the ambient noise environment that has a noise level similar to or greater than the predicted noise level. That is, the noise goals under the Policy may be relaxed where it is shown that elevated noise levels are present in the existing environment.

One method of demonstrating compliance with the Policy is as described in Part 4 Clause 17(2)(a) which states:

The noise from a noise source complies with the noise goals if measurements taken in relation to the noise source and the noise-affected premises show that—

(a) the noise source level (continuous) does not exceed the background noise level plus 5 dB(A).

It is proposed that this method (and subtracting 5 dB(A) for a new Development Application where applicable) be used for certain periods where peak activity at the site coincides with elevated background noise levels.

Therefore, the following criteria will be adopted for the assessment of noise levels at receivers during the peak periods shown:

- Between 6:00am and 7:00am on Saturday mornings:
 - An average (L_{eq}) noise level of 50 dB(A) at existing sensitive receivers
 - An average (L_{eq}) noise level of 55 dB(A) at future sensitive receivers
- Between 4:00pm and 7:00pm on Saturday afternoon / evenings:
 - An average (L_{eq}) noise level of 53 dB(A) at existing sensitive receivers
 - An average (L_{eq}) noise level of 58 dB(A) at future sensitive receivers

A summary of the criteria used for the assessment is shown in Table 4.

When predicting noise levels for comparison with the Policy, adjustments may be made to the average noise levels for each “annoying” characteristic of tonality, impulsiveness, intermittency, low frequency, and modulation of the noise source. The characteristic must be dominant in the acoustic environment and therefore the application of penalties can vary depending on the assessment location, time of day, the noise source being assessed, and the predicted noise level. The application of penalties is discussed further in the Assessment section.

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Table 4: Summary of assessment criteria.

Location	Period	Criterion
Existing receivers	Day	50 dB(A) (L_{eq})
	Night	43 dB(A) (L_{eq}) / 60 dB(A) (L_{max})
	6:00am to 7:00am Saturdays	50 dB(A) (L_{eq})
	4:00pm to 7:00pm Saturdays	53 dB(A) (L_{eq})
Future receivers	Day	55 dB(A) (L_{eq})
	Night	48 dB(A) (L_{eq})
	6:00am to 7:00am Saturdays	55 dB(A) (L_{eq})
	4:00pm to 7:00pm Saturdays	58 dB(A) (L_{eq})

5.2 Assessment

5.2.1 Noise Sources

The noise levels resulting at sensitive receivers from activity at the site have been predicted based on a range of previous noise measurements and observations at similar facilities, including:

- Car park activity, including the opening and closing of doors, talking, vehicles starting and idling, and moving into or out of their parked position.
- Vehicle movements on site.
- Operation of mechanical plant.

The detailed design of the mechanical plant systems typically takes place after the Development Approval is granted. As such, the assessment has considered typical mechanical plant noise data collected from similar facilities to provide an indicative assessment. It is recommended that the assessment be updated if the noise from the selected units or the number of units is greater than those assumed for this assessment. The assessment has been based on air conditioning condenser units located on the roof above each tenancy, and dedicated refrigeration plant for the ice arena within the designated plant room.

The sound power levels for the noise sources used in the assessment are provided in Appendix B.

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5.2.2 Operational Assumptions

The predictions of noise at sensitive receivers have been based on the following operational assumptions for the level of activity in any typical 15-minute¹ period:

- During the day period
 - 100 vehicle movements through the site and into or out of the car parking spaces.
 - General activity in 100 of the car parking spaces.
 - Continuous operation of all mechanical plant.
- During the night period
 - 15 vehicle movements through the site and into or out of the car parking spaces.
 - General activity in 15 of the car parking spaces.
 - Continuous operation of all mechanical plant.

The predictions of noise at sensitive receivers have been based on the following operational assumptions for the level of activity in any 15-minute period during the peak periods:

- Between 6:00am and 7:00am on Saturdays:
 - 50 vehicle movements through the site and into or out of the car parking spaces.
 - General activity in 50 of the car parking spaces.
 - Continuous operation of all mechanical plant.
- Between 4:00pm and 7:00pm on Saturdays:
 - 300 vehicle movements through the site and into or out of the car parking spaces.
 - General activity in 150 car parking spaces
 - Continuous operation of all mechanical plant.

The predictions of noise from car parking and vehicular activity, and mechanical plant have been made using the same noise model described in the Music Noise section of this report and accounts for the levels of operational activity described.

¹ The default assessment period of the Policy.

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5.2.3 Recommendations

Based on the noise model, the noise goals under the Policy are predicted to be exceeded unless specific acoustic treatments are incorporated. Therefore, the following acoustic treatments are recommended:

- Ensure that any ventilation louvres for the plant room servicing the ice arena face toward Main North Road.
- The ventilation louvres for the plant room servicing the ice arena should be acoustic louvres such as the *Flexshield Sonic Series 200 Chevron Louvre*. The minimum insertion loss for the acoustic louvres is shown in Table 5.
- Install acoustic absorption with a thickness of at least 75mm and a minimum density of 32 kg/m³ to the full extent of the underside of the plant room soffit. An example of a suitable material is *CSR Bradford "Supertel"* glasswool insulation.
- The documented louvres surrounding the roof-mounted mechanical plant should perform as well as the *Flexshield Sonic Series 200 Chevron Louvre* or better. It is noted that the acoustic louvres are proposed to be 1.8m tall. This will be sufficient provided the louvres are at least 0.5m taller than the tallest condenser unit. The minimum insertion loss for the acoustic louvres is shown in Table 5
- Construct a 1.8m high boundary fence for the extent shown as **GREEN** in Figure 2. A suitable material is 0.35mm BMT sheet steel (*Colorbond* or similar).

Table 5: Minimum insertion loss for acoustic louvres.

Octave Band Centre Frequency	125 Hz	250 Hz	500 Hz	1000 Hz	2000 Hz	4000 Hz
Minimum insertion loss (dB)	5	6	15	20	28	28

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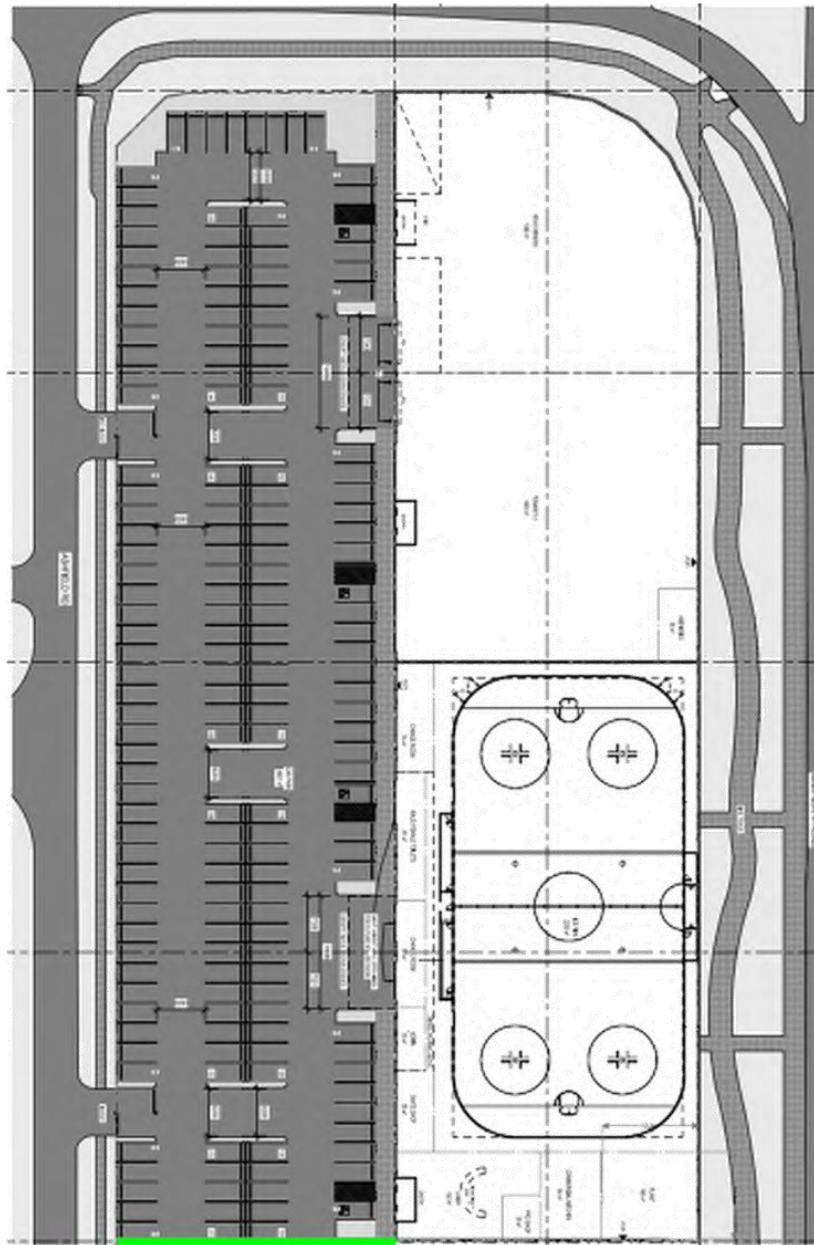


Figure 2: Acoustic boundary fence extent.

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5.2.4 Predicted Noise Levels

Average Noise Levels

Subject to the incorporation of the recommended acoustic treatments, the average (L_{eq}) noise levels predicted for sensitive receivers will be no higher than those shown in Table 6.

Table 6: Summary of predicted average noise levels for sensitive receivers.

Location	Period	Criterion	Lowest Measured Background (L_{90}) Noise Level	Predicted Noise Level
Existing receivers	Day	50 dB(A) (L_{eq})	46 dB(A)	49 dB(A)
	Night	43 dB(A) (L_{eq})	31 dB(A)	37 dB(A) ¹
	6:00am to 7:00am Saturdays	50 dB(A) (L_{eq})	50 dB(A)	46 dB(A)
	4:00pm to 7:00pm Saturdays	53 dB(A) (L_{eq})	53 dB(A)	52 dB(A)
Future receivers	Day	55 dB(A) (L_{eq})	46 dB(A)	47 dB(A)
	Night	48 dB(A) (L_{eq})	31 dB(A)	47 dB(A) ¹
	6:00am to 7:00am Saturdays	55 dB(A) (L_{eq})	50 dB(A)	47 dB(A)
	4:00pm to 7:00pm Saturdays	58 dB(A) (L_{eq})	53 dB(A)	49 dB(A)

Note 1. See discussion below.

As shown in Table 6, the predicted noise levels achieve the assessment criterion in all cases, subject to the incorporation of the recommended acoustic treatments. In order to assess the application of any penalties for noise character, reference is made to the results of the background noise monitoring. Where the predicted noise level is within 5 dB(A) of the lowest measured background (L_{90}) noise level for the period, it is considered that the character of noise from the site cannot dominate the acoustic environment and no penalty is warranted. The results of the noise monitoring show that in all cases except for the night time period (Note 1 in Table 6) the predicted noise level is within 5 dB(A) of the lowest background noise level measured for the period and therefore no adjustments to the predicted noise levels are required.

For the case of night time period, the predicted noise levels of 37 dB(A) for the existing sensitive receiver and 47 dB(A) for the future sensitive receiver are controlled by mechanical plant units on the roof of the building. The noise from mechanical plant units is broadband and at a constant level. Therefore, it is considered that no penalties for noise character are warranted and no adjustments to the predicted noise levels are required.

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Maximum Instantaneous Noise Levels

The maximum instantaneous noise levels (L_{\max}) have also been predicted for existing sensitive receivers in the locality. Predictions have been made based on measurements at a variety of similar sites and include short-term transient events such as car doors slamming and vehicles accelerating. The maximum noise level predicted from such activity is 59 dB(A).

Based on the above, the 60 dB(A) criterion will be achieved at all sensitive receivers.

Where the Policy is satisfied, it is considered that all relevant Performance Outcomes of the South Australian *Planning and Design Code* will be satisfied.

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6 CONCLUSION

An environmental noise assessment has been prepared for the proposed development at Lot 12 Ashfield Road, Elizabeth.

The assessment considers noise levels at nearby sensitive receivers from activity at the site, including car parking and vehicular activity, mechanical plant and music from a proposed gym.

Appropriate noise criteria have been determined based on the South Australian *Planning and Design Code*, the *Environment Protection (Commercial and Industrial Noise) Policy 2023*, and the Environment Protection Authority Guidelines *Assessing music noise from indoor venues*.

It is predicted that the proposed development will achieve the relevant noise criteria where the acoustic treatments recommended in this report are incorporated, including:

- Specific roof / ceiling construction for the gym
- Specific glazing for the gym facade
- Constructing an airlock for the gym entry
- Facing the ventilation for the ice arena plant room louvres in a specific direction
- Specific locations for the roof-mounted mechanical plant
- Construction of rooftop mechanical plant screens using acoustic louvres (or a solid barrier) with specific height requirements
- Specific performance requirements for the acoustic louvres
- Installation of acoustic absorption to the underside of the plant room soffit
- Construction of a specific boundary fence

Based on the implementation of these noise mitigation measures, the proposed development will *not unreasonably impact the amenity of sensitive receivers*, thereby achieving the relevant provisions of the South Australian *Planning and Design Code* related to environmental noise.

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APPENDIX A: PLANNING AND DESIGN CODE – RELEVANT PROVISIONS

Part 4 – General Development Policies – Interface between Land Uses

<i>Desired Outcome</i>	
D01	<i>Development is located and designed to mitigate adverse effects on or from neighbouring and proximate land uses.</i>

<i>Performance Outcome</i>	<i>Deemed-to-Satisfy Criteria / Designated Performance Feature</i>
General Land Use Compatibility	
<p>PO 1.2</p> <p><i>Development adjacent to a site containing a sensitive receiver (or lawfully approved sensitive receiver) or zone primarily intended to accommodate sensitive receivers is designed to minimise adverse impacts.</i></p>	<p>DTS/DPF 1.2</p> <p><i>None are applicable.</i></p>
Activities Generating Noise or Vibration	
<p>PO 4.1</p> <p><i>Development that emits noise (other than music) does not unreasonably impact the amenity of sensitive receivers (or lawfully approved sensitive receivers).</i></p>	<p>DTS/DPF 4.1</p> <p><i>Noise that affects sensitive receivers achieves the relevant Environment Protection (Commercial and Industrial Noise) Policy criteria.</i></p>
<p>PO 4.2</p> <p><i>Areas for the on-site manoeuvring of service and delivery vehicles, plant and equipment, outdoor work spaces (and the like) are designed and sited to not unreasonably impact the amenity of adjacent sensitive receivers (or lawfully approved sensitive receivers) and zones primarily intended to accommodate sensitive receivers due to noise and vibration by adopting techniques including:</i></p> <ul style="list-style-type: none"> <i>(a) locating openings of buildings and associated services away from the interface with the adjacent sensitive receivers and zones primarily intended to accommodate sensitive receivers</i> <i>(b) when sited outdoors, locating such areas as far as practicable from adjacent sensitive receivers and zones primarily intended to accommodate sensitive receivers</i> <i>(c) housing plant and equipment within an enclosed structure or acoustic enclosure</i> <i>(d) providing a suitable acoustic barrier between the plant and / or equipment and the adjacent sensitive receiver boundary or zone.</i> 	<p>DTS/DPF 4.2</p> <p><i>None are applicable.</i></p>

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PO 4.6

Development incorporating music achieves suitable acoustic amenity when measured at the boundary of an adjacent sensitive receiver (or lawfully approved sensitive receiver) or zone primarily intended to accommodate sensitive receivers.

DTS/DPF 4.6

Development incorporating music includes noise attenuation measures that will achieve the following noise levels:

Assessment location	Music noise level
Externally at the nearest existing or envisaged noise sensitive location	Less than 8dB above the level of background noise ($L_{90,15min}$) in any octave band of the sound spectrum ($LOCT_{10,15} < LOCT_{90,15} + 8dB$)

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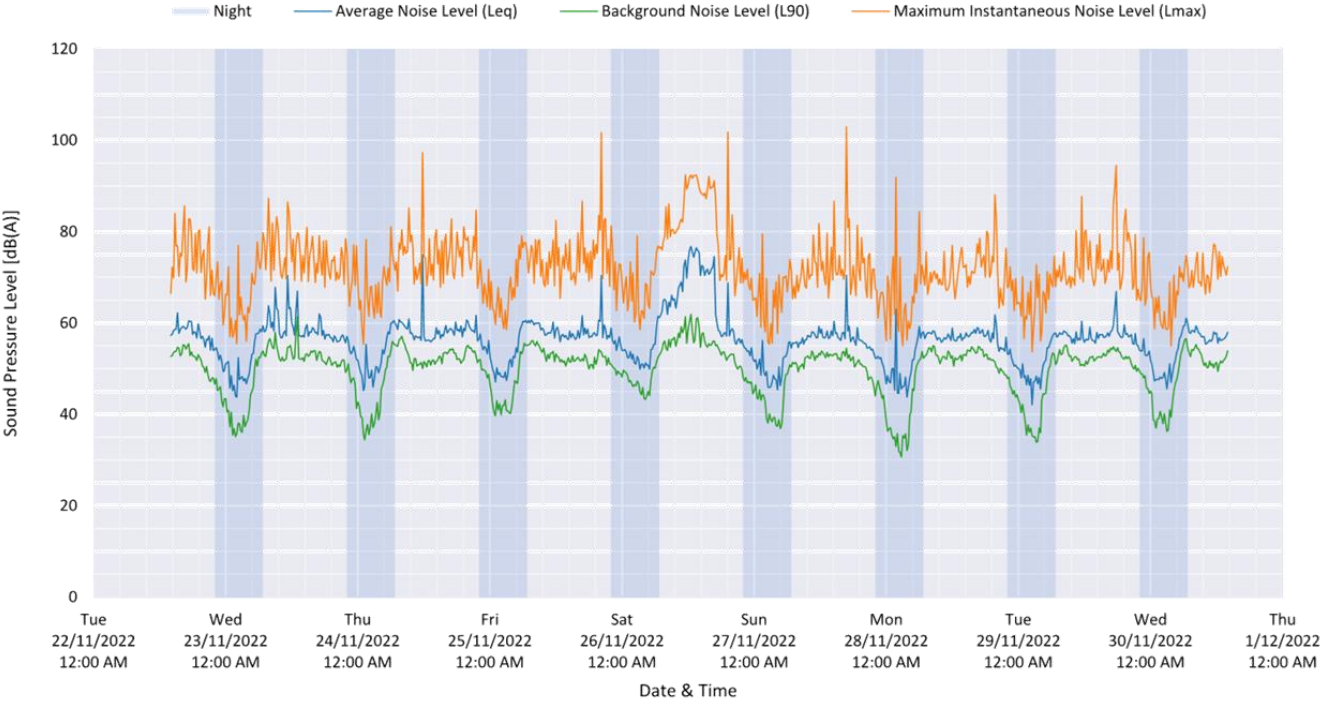
APPENDIX B: SOUND POWER DATA

Activity		Sound Power Level (dB(A))
Car Park Activity	General activity	83
	Moving car	82
	Car door slam (L _{max})	96
	Car accelerating (L _{max})	93
Mechanical Plant	Air conditioning condenser unit (each)	83
	Ice arena chiller	100

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APPENDIX C: NOISE MONITORING RESULTS



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1 INTRODUCTION

An environmental noise assessment has been undertaken for the proposed development (the **Development**) at Lot 12 Ashfield Road, Elizabeth (the **Subject Site**).

The Development consists of the construction of a new building which is proposed to house a new Ice Arena, a gym, and an additional retail/commercial tenancy. An at-grade car park is also included as part of the Development. The new Ice Arena is proposing to host Australian Ice Hockey League (**AIHL**) games on Saturday afternoons. Thirteen AIHL games are proposed to be held at the facility every year.

The closest existing sensitive receivers to the site are located to the west of the Subject Site on the opposite side of Ashfield Road. An overview of the Subject Site and surrounding area is shown in Figure 1.

The assessment considers noise levels at sensitive receivers in the locality from the Development, including car parking and vehicular activity, mechanical plant, and music.

The assessment has been based on:

- PLANNING DOCUMENTATION SEPTEMBER 2025 by Bell for "Lot 12, Pelligra Ice Arena, Playford, SA", job number "BAA250010", Revision "C", issued on 16 September 2025.
- Traffic estimates provided by email on 1 February 2023.
- Estimates of gym attendance provided by email on 4 September 2025
- Previous noise measurements and sound power data obtained from similar sites for the car parking and vehicular activity, mechanical plant, and music.
- The proposed operating hours being as follows:
 - 6:00am to 10:00pm for the Ice Arena, with peak activity associated with AIHL games occurring during the following times:
 - 6:00am to 7:00am on Saturday mornings for practices
 - 4:00pm to 7:00pm on Saturday afternoon/evenings for games
 - 24/7 operation for the gym
 - The retail/commercial tenancy operating between 7:00am and 10:00pm
- Any private rubbish collection at the site will be limited to the times of the day defined to not constitute a 'local nuisance' by the *Local Nuisance and Litter Control Act 2016*, which are:
 - Between 9:00am and 7:00pm on any Sunday or public holiday.
 - Between 7:00am and 7:00pm on any other day.

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Figure 1: Subject Site and locality.

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2 PLANNING AND DESIGN CODE

The Development is subject to the provisions of the South Australian *Planning and Design Code* (the **Code**) under the *Planning, Development and Infrastructure Act 2016*.

In accordance with the Code, the Subject Site is located in both the *Urban Activity Centre Zone* and *General Neighbourhood Zone*. Existing sensitive receivers are located to the west of the Subject Site on the opposite side of Ashfield Road.

There is vacant land in the *General Neighbourhood Zone* adjacent the southern boundary of the Subject Site and to the east on the opposite side of Main North Road. As the *General Neighbourhood Zone* is primarily intended to accommodate sensitive receivers, in accordance with Performance Outcome (PO) 1.2 of the *Interface between Land Uses* module of the *General Development Policies* of the Code, future sensitive receivers within these allotments have also been considered.

The Code has been reviewed and the provisions considered relevant to the assessment are included in Appendix A.

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3 EXISTING NOISE ENVIRONMENT

In order to inform the assessment, continuous noise monitoring was conducted at the location shown in Figure 1 on the Subject Site. The location was chosen to be representative of the existing acoustic environment at the sensitive receivers on the opposite side of Ashfield Road.

The noise monitoring was conducted from 22 to 30 November 2022 to provide an indication of the noise level variation over the course of each day and over the course a typical week.

The results of the noise monitoring indicate that the existing acoustic environment is dominated by traffic noise and can be characterised as follows for the day (7:00am to 10:00pm) and night (10:00pm to 7:00am):

- Background (L_{90}) noise levels:
 - Generally in the range of 50-55 dB(A) during the day with the lowest measured being 46 dB(A).
 - Generally in the range of 35-45 dB(A) during the night with the lowest measured being 31 dB(A).
- Average (L_{eq}) noise levels:
 - Generally in the range of 55-60 dB(A) during the day with the lowest measured being 53 dB(A).
 - Generally in the range of 45-55 dB(A) during the night with the lowest measured being 42 dB(A).
- Maximum instantaneous (L_{max}) noise levels:
 - Generally in the range of 70-80 dB(A) during the day with the lowest measured being 63 dB(A).
 - Generally in the range of 60-70 dB(A) during the night with the lowest measured being 54 dB(A).

The following noise levels were the lowest measured during specific peak periods of the Development:

- Between 6:00am and 7:00am on Saturday morning:
 - A background noise level of 50 dB(A)
 - An average noise level of 55 dB(A)
 - A maximum instantaneous noise level of 72 dB(A).
- Between 4:00pm and 7:00pm on Saturday afternoon:
 - A background noise level of 53 dB(A)
 - An average noise level of 57 dB(A)
 - A maximum instantaneous noise level of 69 dB(A).

The results of the noise monitoring are provided in Appendix C.

4 MUSIC NOISE

4.1 Criteria

PO 4.6 of the *Interface between Land Uses* module under the *General Development Policies* of the Code requires that any development incorporating music *achieves suitable acoustic amenity when measured at the boundary of an adjacent sensitive receiver (or lawfully approved sensitive receiver) or zone primarily intended to accommodate sensitive receivers*. The Deemed-to-Satisfy / Designated Performance Feature (DTS/DPF) for PO 4.6 establishes music noise criteria based on the existing background noise levels in the environment. Therefore, the background noise levels have been measured continuously at a location on the subject site (shown in Figure 1) selected to be representative of the most noise affected sensitive receivers. The results of the noise monitoring are provided in Appendix C.

The results of the noise monitoring have been analysed to determine the lowest background noise levels measured. Criteria for the assessment of music have been developed in accordance with the provisions of DTS/DPF 4.6, which is consistent with the requirements of the Environment Protection Authority (EPA) guidelines *Assessing music noise from indoor venues* (the **EPA Guidelines**).

The lowest measured background noise levels ($L_{A90,15min}$) are shown in Table 1. Also shown are the resulting music noise criteria ($L_{A10,15min}$) applicable at nearby sensitive receivers.

Table 1: Lowest background noise levels and music noise criteria.

	Noise Level [dB(A)] in Octave Band Centre Frequency [Hz]						
	63	125	250	500	1000	2000	4000
Lowest measured background noise level ($L_{A90,15min}$)	17	23	20	23	26	21	17
Music noise criteria ($L_{A10,15min}$) (8 dB above background noise level in octave bands)	25	31	28	31	34	29	25

4.2 Assessment

Typical music noise levels played within gyms have previously been measured at other similar facilities and is considered to be a typical level for gyms without group classes. The levels shown in Table 2 are based on the previous measurements and have been used as the basis for this assessment.

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Table 2: Music level within the gym.

	Noise Level (L _{10,15min} dB(A)) by Octave Band Centre Frequency (Hz)							Overall Noise Level (dB(A))
	63	125	250	500	1000	2000	4000	
Gym music level (reverberant level)	52	63	66	70	71	65	58	75

A noise model of the site and surrounding area has been developed using *SoundPLANnoise* noise modelling software and the *ISO 9613-2: 2024-01* prediction method to predict noise levels for sensitive receivers in the locality. The noise model considers the music level shown in Table 2, the construction of the building envelope, the distance to receivers, and the effects of barriers such as fences and buildings.

4.3 Recommendations

Based on the noise model, and music playing at the level shown in Table 2, the following acoustic treatments are recommended to achieve the music noise criteria at all sensitive receivers:

- The roof should be constructed using at least 0.42mm BMT sheet steel (*Colorbond or similar*).
- All glazing to the Western Elevation of the gym tenancy should be constructed using at least 10mm thick monolithic or laminated glass.
- Construct an airlock entry/exit to the gym which incorporates self-closing mechanisms on the doors. Note that the airlock should be long enough to ensure that the first door closes before the second door is opened.
- Ensure that any music playing within the gym is turned off while the roller door is opened.
- If higher music levels are needed for group fitness classes, it is recommended that a dedicated acoustically treated internal space be constructed within the larger space (note that if located along the Western Elevation the glazing may need to be upgraded). This can be investigated during the detail design stage if necessary.

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4.4 Predicted Music Noise Levels

Based on the incorporation of the recommended acoustic treatments, the highest predicted noise level for any sensitive receiver is shown and compared with the music noise criteria in Table 3.

Table 3: Comparison of the music noise criteria and highest noise level predicted at sensitive receivers.

	Noise Level [dB(A)] in Octave Band Centre Frequency [Hz]						
	63	125	250	500	1000	2000	4000
Music noise criteria (LA10,15min)	25	31	28	31	34	29	25
Highest predicted noise level (LA10,15min)	23	27	26	31	32	26	17

As shown, subject to the incorporation of the recommended acoustic treatments, the music noise criteria are predicted to be achieved at all sensitive receivers in the locality.

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5 OTHER NOISE SOURCES

5.1 Criteria

5.1.1 Planning and Design Code

PO 4.1 of the *Interface between Land Uses* module under the *General Development Policies* of the Code specifically requires that noise from a development *does not unreasonably impact the amenity of sensitive receivers (or lawfully approved sensitive receivers)*. The DTS/DPF for PO 4.1 references the *Environment Protection (Commercial and Industrial Noise) Policy 2023* (the **Policy**).

5.1.2 Environment Protection (Commercial and Industrial Noise) Policy 2023

The Policy provides objective noise criteria to assess the environmental noise emissions from a proposed development. The noise criteria provided by the Policy are based on the *World Health Organisation Guidelines for Community Noise (1999)* (the **WHO Guidelines**), which provides recommendations for appropriate noise levels to prevent community annoyance, sleep disturbance and adverse impact on the amenity of a locality. Therefore, compliance with the Policy is considered to satisfy the WHO Guidelines and the subjective requirements of the Code relating to environmental noise.

The Policy establishes indicative noise levels to be achieved at sensitive receivers based on the zones in which the noise source (the Development) and sensitive receivers (the residences) are located. For new developments, Part 5 of the Policy provides goal noise levels which are 5 dB(A) more onerous than those which would otherwise apply. It is noted that this 5 dB(A) reduction in the criteria does not apply to future sensitive receivers.

The Policy defines the day period as the hours from 7:00am to 10:00pm (the **day period**) and the night period as the hours from 10:00pm to 7:00am (the **night period**).

Based on the above, the Policy provides the following goal noise levels to be achieved at sensitive receivers:

- At existing sensitive receivers:
 - An average (L_{eq}) noise level of 50 dB(A) during the day period.
 - An average (L_{eq}) noise level of 43 dB(A) during the night period.
 - A maximum instantaneous (L_{max}) noise level of 60 dB(A) during the night period.
- At future sensitive receivers:
 - An average (L_{eq}) noise level of 55 dB(A) during the day period.
 - An average (L_{eq}) noise level of 48 dB(A) during the night period.

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Clause 19 of the Policy, which contains the provisions for governing development authorisation applications, describes other factors to consider when determining compliance with the overarching goals of the Policy, beyond simply complying with the noise goals. The Policy allows consideration to be given to components of the ambient noise environment that has a noise level similar to or greater than the predicted noise level. That is, the noise goals under the Policy may be relaxed where it is shown that elevated noise levels are present in the existing environment.

One method of demonstrating compliance with the Policy is as described in Part 4 Clause 17(2)(a) which states:

The noise from a noise source complies with the noise goals if measurements taken in relation to the noise source and the noise-affected premises show that—

(a) the noise source level (continuous) does not exceed the background noise level plus 5 dB(A).

It is proposed that this method (and subtracting 5 dB(A) for a new Development Application where applicable) be used for certain periods where peak activity at the site coincides with elevated background noise levels.

Therefore, the following criteria will be adopted for the assessment of noise levels at receivers during the peak periods shown:

- Between 6:00am and 7:00am on Saturday mornings:
 - An average (L_{eq}) noise level of 50 dB(A) at existing sensitive receivers
 - An average (L_{eq}) noise level of 55 dB(A) at future sensitive receivers
- Between 4:00pm and 7:00pm on Saturday afternoon / evenings:
 - An average (L_{eq}) noise level of 53 dB(A) at existing sensitive receivers
 - An average (L_{eq}) noise level of 58 dB(A) at future sensitive receivers

A summary of the criteria used for the assessment is shown in Table 4.

When predicting noise levels for comparison with the Policy, adjustments may be made to the average noise levels for each “annoying” characteristic of tonality, impulsiveness, intermittency, low frequency, and modulation of the noise source. The characteristic must be dominant in the acoustic environment and therefore the application of penalties can vary depending on the assessment location, time of day, the noise source being assessed, and the predicted noise level. The application of penalties is discussed further in the Assessment section.

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Table 4: Summary of assessment criteria.

Location	Period	Criterion
Existing receivers	Day	50 dB(A) (L_{eq})
	Night	43 dB(A) (L_{eq}) / 60 dB(A) (L_{max})
	6:00am to 7:00am Saturdays	50 dB(A) (L_{eq})
	4:00pm to 7:00pm Saturdays	53 dB(A) (L_{eq})
Future receivers	Day	55 dB(A) (L_{eq})
	Night	48 dB(A) (L_{eq})
	6:00am to 7:00am Saturdays	55 dB(A) (L_{eq})
	4:00pm to 7:00pm Saturdays	58 dB(A) (L_{eq})

5.2 Assessment

5.2.1 Noise Sources

The noise levels resulting at sensitive receivers from activity at the site have been predicted based on a range of previous noise measurements and observations at similar facilities, including:

- Car park activity, including the opening and closing of doors, talking, vehicles starting and idling, and moving into or out of their parked position.
- Vehicle movements on site.
- Operation of mechanical plant.

The detailed design of the mechanical plant systems typically takes place after the Development Approval is granted. As such, the assessment has considered typical mechanical plant noise data collected from similar facilities to provide an indicative assessment. It is recommended that the assessment be updated if the noise from the selected units or the number of units is greater than those assumed for this assessment. The assessment has been based on air conditioning condenser units located on the roof above each tenancy, and dedicated refrigeration plant for the ice arena within the designated plant room.

The sound power levels for the noise sources used in the assessment are provided in Appendix B.

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5.2.2 Operational Assumptions

The predictions of noise at sensitive receivers have been based on the following operational assumptions for the level of activity in any typical 15-minute¹ period:

- During the day period
 - 100 vehicle movements through the site and into or out of the car parking spaces.
 - General activity in 100 of the car parking spaces.
 - Continuous operation of all mechanical plant.
- During the night period
 - 15 vehicle movements through the site and into or out of the car parking spaces.
 - General activity in 15 of the car parking spaces.
 - Continuous operation of all mechanical plant.

The predictions of noise at sensitive receivers have been based on the following operational assumptions for the level of activity in any 15-minute period during the peak periods:

- Between 6:00am and 7:00am on Saturdays:
 - 50 vehicle movements through the site and into or out of the car parking spaces.
 - General activity in 50 of the car parking spaces.
 - Continuous operation of all mechanical plant.
- Between 4:00pm and 7:00pm on Saturdays:
 - 300 vehicle movements through the site and into or out of the car parking spaces.
 - General activity in 150 car parking spaces
 - Continuous operation of all mechanical plant.

The predictions of noise from car parking and vehicular activity, and mechanical plant have been made using the same noise model described in the Music Noise section of this report and accounts for the levels of operational activity described.

¹ The default assessment period of the Policy.

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5.2.3 Recommendations

Based on the noise model, the noise goals under the Policy are predicted to be exceeded unless specific acoustic treatments are incorporated. Therefore, the following acoustic treatments are recommended:

- Ensure that any ventilation louvres for the plant room servicing the ice arena face toward Main North Road.
- The ventilation louvres for the plant room servicing the ice arena should be acoustic louvres such as the *Flexshield Sonic Series 200 Chevron Louvre*. The minimum insertion loss for the acoustic louvres is shown in Table 5.
- Install acoustic absorption with a thickness of at least 75mm and a minimum density of 32 kg/m³ to the full extent of the underside of the plant room soffit. An example of a suitable material is *CSR Bradford "Supertel"* glasswool insulation.
- The documented louvres surrounding the roof-mounted mechanical plant should perform as well as the *Flexshield Sonic Series 200 Chevron Louvre* or better. It is noted that the acoustic louvres are proposed to be 1.8m tall. This will be sufficient provided the louvres are at least 0.5m taller than the tallest condenser unit. The minimum insertion loss for the acoustic louvres is shown in Table 5
- Construct a 1.8m high boundary fence for the extent shown as **GREEN** in Figure 2. A suitable material is 0.35mm BMT sheet steel (*Colorbond* or similar).

Table 5: Minimum insertion loss for acoustic louvres.

Octave Band Centre Frequency	125 Hz	250 Hz	500 Hz	1000 Hz	2000 Hz	4000 Hz
Minimum insertion loss (dB)	5	6	15	20	28	28

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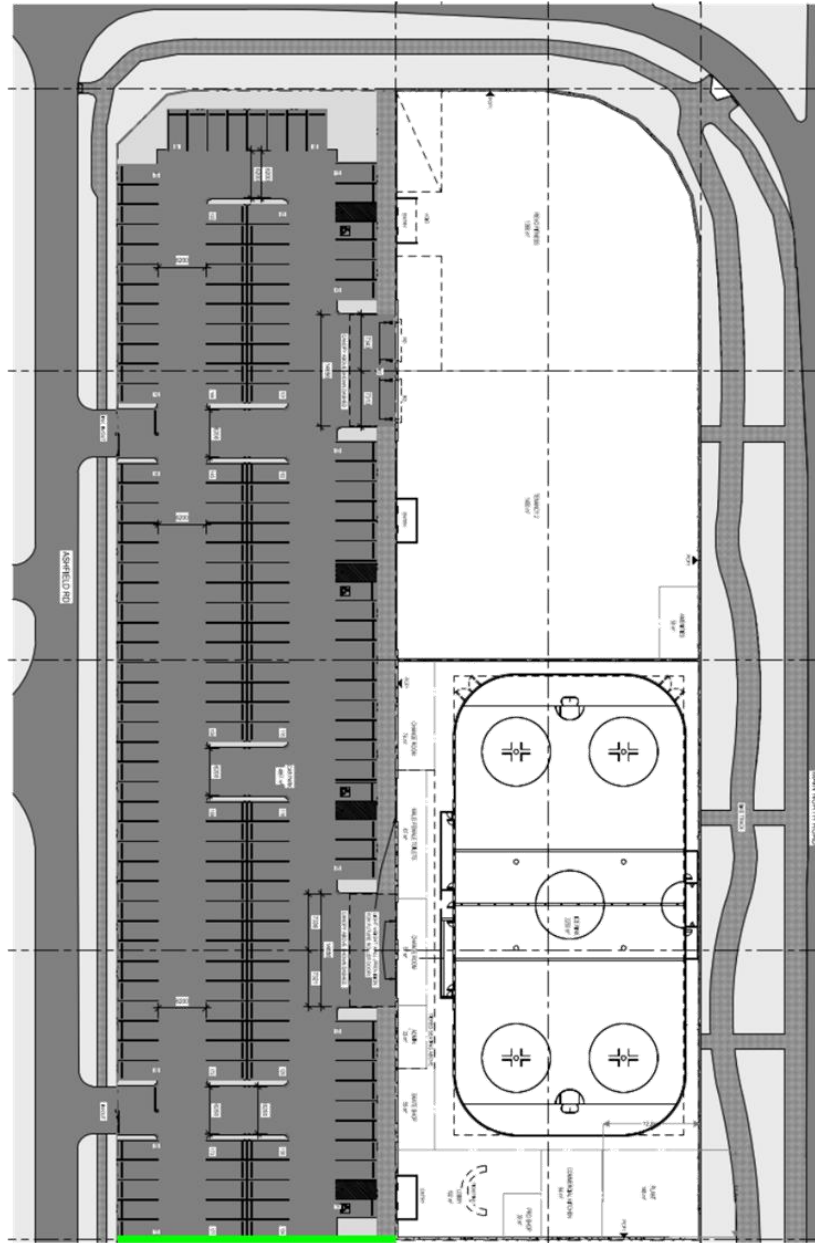


Figure 2: Acoustic boundary fence extent.

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5.2.4 Predicted Noise Levels

Average Noise Levels

Subject to the incorporation of the recommended acoustic treatments, the average (L_{eq}) noise levels predicted for sensitive receivers will be no higher than those shown in Table 6.

Table 6: Summary of predicted average noise levels for sensitive receivers.

Location	Period	Criterion	Lowest Measured Background (L_{90}) Noise Level	Predicted Noise Level
Existing receivers	Day	50 dB(A) (L_{eq})	46 dB(A)	49 dB(A)
	Night	43 dB(A) (L_{eq})	31 dB(A)	37 dB(A) ¹
	6:00am to 7:00am Saturdays	50 dB(A) (L_{eq})	50 dB(A)	46 dB(A)
	4:00pm to 7:00pm Saturdays	53 dB(A) (L_{eq})	53 dB(A)	52 dB(A)
Future receivers	Day	55 dB(A) (L_{eq})	46 dB(A)	47 dB(A)
	Night	48 dB(A) (L_{eq})	31 dB(A)	47 dB(A) ¹
	6:00am to 7:00am Saturdays	55 dB(A) (L_{eq})	50 dB(A)	47 dB(A)
	4:00pm to 7:00pm Saturdays	58 dB(A) (L_{eq})	53 dB(A)	49 dB(A)

Note 1. See discussion below.

As shown in Table 6, the predicted noise levels achieve the assessment criterion in all cases, subject to the incorporation of the recommended acoustic treatments. In order to assess the application of any penalties for noise character, reference is made to the results of the background noise monitoring. Where the predicted noise level is within 5 dB(A) of the lowest measured background (L_{90}) noise level for the period, it is considered that the character of noise from the site cannot dominate the acoustic environment and no penalty is warranted. The results of the noise monitoring show that in all cases except for the night time period (Note 1 in Table 6) the predicted noise level is within 5 dB(A) of the lowest background noise level measured for the period and therefore no adjustments to the predicted noise levels are required.

For the case of night time period, the predicted noise levels of 37 dB(A) for the existing sensitive receiver and 47 dB(A) for the future sensitive receiver are controlled by mechanical plant units on the roof of the building. The noise from mechanical plant units is broadband and at a constant level. Therefore, it is considered that no penalties for noise character are warranted and no adjustments to the predicted noise levels are required.

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Maximum Instantaneous Noise Levels

The maximum instantaneous noise levels (L_{\max}) have also been predicted for existing sensitive receivers in the locality. Predictions have been made based on measurements at a variety of similar sites and include short-term transient events such as car doors slamming and vehicles accelerating. The maximum noise level predicted from such activity is 59 dB(A).

Based on the above, the 60 dB(A) criterion will be achieved at all sensitive receivers.

Where the Policy is satisfied, it is considered that all relevant Performance Outcomes of the South Australian *Planning and Design Code* will be satisfied.

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6 CONCLUSION

An environmental noise assessment has been prepared for the proposed development at Lot 12 Ashfield Road, Elizabeth.

The assessment considers noise levels at nearby sensitive receivers from activity at the site, including car parking and vehicular activity, mechanical plant and music from a proposed gym.

Appropriate noise criteria have been determined based on the South Australian *Planning and Design Code*, the *Environment Protection (Commercial and Industrial Noise) Policy 2023*, and the Environment Protection Authority Guidelines *Assessing music noise from indoor venues*.

It is predicted that the proposed development will achieve the relevant noise criteria where the acoustic treatments recommended in this report are incorporated, including:

- Specific roof / ceiling construction for the gym
- Specific glazing for the gym facade
- Constructing an airlock for the gym entry
- Facing the ventilation for the ice arena plant room louvres in a specific direction
- Specific locations for the roof-mounted mechanical plant
- Construction of rooftop mechanical plant screens using acoustic louvres (or a solid barrier) with specific height requirements
- Specific performance requirements for the acoustic louvres
- Installation of acoustic absorption to the underside of the plant room soffit
- Construction of a specific boundary fence

Based on the implementation of these noise mitigation measures, the proposed development will *not unreasonably impact the amenity of sensitive receivers*, thereby achieving the relevant provisions of the South Australian *Planning and Design Code* related to environmental noise.

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APPENDIX A: PLANNING AND DESIGN CODE – RELEVANT PROVISIONS

Part 4 – General Development Policies – Interface between Land Uses

<i>Desired Outcome</i>	
D01	<i>Development is located and designed to mitigate adverse effects on or from neighbouring and proximate land uses.</i>

<i>Performance Outcome</i>	<i>Deemed-to-Satisfy Criteria / Designated Performance Feature</i>
General Land Use Compatibility	
<p>PO 1.2</p> <p><i>Development adjacent to a site containing a sensitive receiver (or lawfully approved sensitive receiver) or zone primarily intended to accommodate sensitive receivers is designed to minimise adverse impacts.</i></p>	<p>DTS/DPF 1.2</p> <p><i>None are applicable.</i></p>
Activities Generating Noise or Vibration	
<p>PO 4.1</p> <p><i>Development that emits noise (other than music) does not unreasonably impact the amenity of sensitive receivers (or lawfully approved sensitive receivers).</i></p>	<p>DTS/DPF 4.1</p> <p><i>Noise that affects sensitive receivers achieves the relevant Environment Protection (Commercial and Industrial Noise) Policy criteria.</i></p>
<p>PO 4.2</p> <p><i>Areas for the on-site manoeuvring of service and delivery vehicles, plant and equipment, outdoor work spaces (and the like) are designed and sited to not unreasonably impact the amenity of adjacent sensitive receivers (or lawfully approved sensitive receivers) and zones primarily intended to accommodate sensitive receivers due to noise and vibration by adopting techniques including:</i></p> <ul style="list-style-type: none"> <i>(a) locating openings of buildings and associated services away from the interface with the adjacent sensitive receivers and zones primarily intended to accommodate sensitive receivers</i> <i>(b) when sited outdoors, locating such areas as far as practicable from adjacent sensitive receivers and zones primarily intended to accommodate sensitive receivers</i> <i>(c) housing plant and equipment within an enclosed structure or acoustic enclosure</i> <i>(d) providing a suitable acoustic barrier between the plant and / or equipment and the adjacent sensitive receiver boundary or zone.</i> 	<p>DTS/DPF 4.2</p> <p><i>None are applicable.</i></p>

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<p><i>PO 4.6</i></p> <p><i>Development incorporating music achieves suitable acoustic amenity when measured at the boundary of an adjacent sensitive receiver (or lawfully approved sensitive receiver) or zone primarily intended to accommodate sensitive receivers.</i></p>	<p><i>DTS/DPF 4.6</i></p> <p><i>Development incorporating music includes noise attenuation measures that will achieve the following noise levels:</i></p> <table><tr><th data-bbox="810 512 1048 560"><i>Assessment location</i></th><th data-bbox="1048 512 1289 560"><i>Music noise level</i></th></tr><tr><td data-bbox="810 560 1048 759"><i>Externally at the nearest existing or envisaged noise sensitive location</i></td><td data-bbox="1048 560 1289 759"><i>Less than 8dB above the level of background noise (L_{90,15min}) in any octave band of the sound spectrum (LOCT10,15 < LOCT90,15 + 8dB)</i></td></tr></table>	<i>Assessment location</i>	<i>Music noise level</i>	<i>Externally at the nearest existing or envisaged noise sensitive location</i>	<i>Less than 8dB above the level of background noise (L_{90,15min}) in any octave band of the sound spectrum (LOCT10,15 < LOCT90,15 + 8dB)</i>
<i>Assessment location</i>	<i>Music noise level</i>				
<i>Externally at the nearest existing or envisaged noise sensitive location</i>	<i>Less than 8dB above the level of background noise (L_{90,15min}) in any octave band of the sound spectrum (LOCT10,15 < LOCT90,15 + 8dB)</i>				

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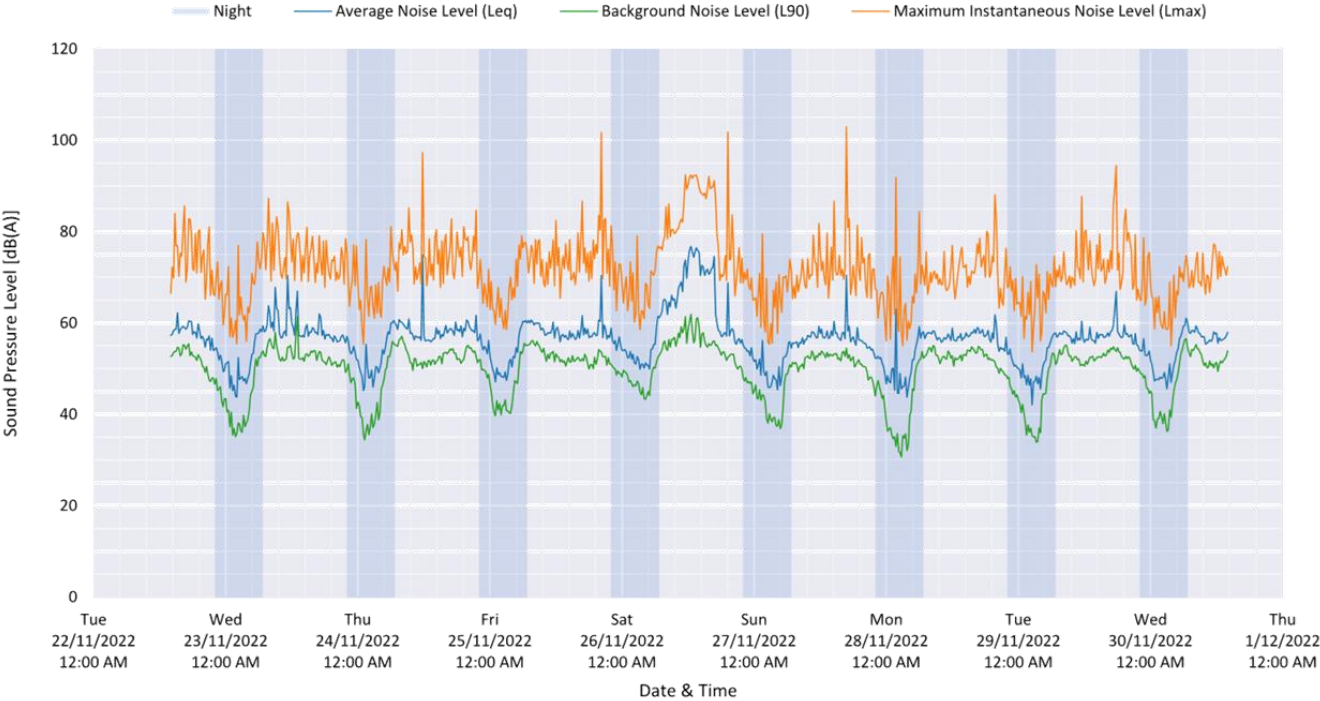
APPENDIX B: SOUND POWER DATA

Activity		Sound Power Level (dB(A))
Car Park Activity	General activity	83
	Moving car	82
	Car door slam (L _{max})	96
	Car accelerating (L _{max})	93
Mechanical Plant	Air conditioning condenser unit (each)	83
	Ice arena chiller	100

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APPENDIX C: NOISE MONITORING RESULTS



Details of Representations

Application Summary

Application ID	25030642
Proposal	Construction of a two-storey building comprising a mix of indoor recreation facilities with associated ground level car parking, landscaping, advertisement and tree damaging activity in the form of the removal of 8 regulated and 2 significant trees (Variation to Application 23022101)
Location	LOT 101 ASHFIELD RD ELIZABETH SA 5112

Representations

Representor 1 - Yeshaiah Varona

Name	Yeshaiah Varona
Address	15 Broadwater Place BLAKEVIEW SA, 5114 Australia
Submission Date	14/11/2025 11:07 AM
Submission Source	Online
Late Submission	No
Would you like to talk to your representation at the decision-making hearing for this development?	No
My position is	I oppose the development

Reasons

I do not believe that the Ice Skating arena should be built in Elizabeth. First there is not consideration or consultation from the community, being that they would be the primary users and beneficiaries. If there were it would be quite evident that families in this area are not able to afford continuous use of the arena, given the average cost of use across the nation is surrounding \$75 to \$100 for families of four, and per person being \$20 to \$35. It is the likely hood that the arena will be an expensive underutilised facility that will fail to deliver what was originally promised, as well as fail to deliver lasting social and economic benefit. The disparity between what was original planned development and what is now being delivered is not only different but lacks the key aspects which brough the wider social benefit to the community. The original plan was to have and ice arena, cafe, basketball court, rock climbing, children's play area and a gym. What is now being put is only an ice arena and and gym, of which we have plenty. There are also concerns surrounding the economical sustainability of the project. There is a notoriety that ice arenas consistently rely on government subsidy due to their high costs of maintenances and low engagement, only placing further pressure on government or council, restricting funding from real services, and projects that could further assist and benefit the community. In essence that ice arena does not belong in Playford.

Attached Documents

Representations**Representor 2** - Beverly Rudnick

Name	Beverly Rudnick
Address	17 Fletcher Road ELIZABETH EAST SA, 5112 Australia
Submission Date	19/11/2025 01:25 PM
Submission Source	Online
Late Submission	No
Would you like to talk to your representation at the decision-making hearing for this development?	No
My position is	I oppose the development

Reasons

Removal of 8 regulated and 2 significant trees. Degradation of prominent features of main entrance to Elizabeth City Centre. Loss of opportunity to upgrade area near entrance to City Centre to enhance parkland feel in conjunction with upgrade of Fremont Park. Potential for the area to become rundown and untidy, detracting from the fresh green natural ambience of the entrance to Elizabeth City Centre. Increase in traffic congestion and small particle pollution in the area potentially affecting people in the Playford Plaza and surrounding area, within 200 metres and even beyond. I have noticed that the air stinks of exhaust fumes even on the far side of the vegetated buffer zone. It is not a good place for people to exercise in.

Attached Documents

Pelligra-Proposed-Development-Lot-101-Ashfield-Road-Elizabeth-City-Centre-1559723.pdf

LOT 101 ASHFIELD RD ELIZABETH SA 5112
CT6305/58

D133867AL101

Applicant:

Pelligra Group C/- Future Urban

Application ID:

25030642

Mayor Docherty and Playford Councillors,

With respect, I wish to oppose the proposal by Pelligra to develop Lot 101 Ashfield Road for the following reasons.

1. Removal of 8 regulated and 2 significant trees. These are mature trees, halfway to developing hollows needed by birds and other wildlife. These cannot be replaced by planting saplings which will take 60 or more years to reach a similar size.
2. Degradation of prominent features of main entrance to Elizabeth City Centre. If you aim to create an inviting and beautiful entry to the city, you need to have more native trees and bushes to create a fresh natural look.
3. Loss of opportunity to upgrade the area near the entrance to City Centre to enhance parkland feel in conjunction with upgrade of Fremont Park. What you have done in upgrading Fremont Park and Yorktown Road is beautiful. The addition of endemic trees and low bushes to Lot 101 Ashfield Road would enhance this look and vegetation endemic to this area can survive with minimal care once established. I really believe this area should be reclaimed for the people of Playford and Elizabeth.
4. Potential for the area to become rundown and untidy, detracting from the fresh green natural ambience of the entrance to Elizabeth City Centre. We are in danger of creating a rundown strip of boring shops and businesses like Main North Road in Enfield which offers few incentives to stop and explore.
5. Increase in traffic congestion and small particle pollution in the area. potentially affecting people in the Playford Plaza and surrounding area, within 200 metres and even beyond. I have noticed that the air stinks of exhaust fumes even on the far side of the vegetated buffer zone on Nimitz and Bellinger Roads. It is not a good place for people to exercise in.

Yours respectfully,

Beverly A Rudnick

Representations**Representor 3 - Fran E**

Name	Fran E
Address	12 Wilcox Street ADELAIDE SA, 5000 Australia
Submission Date	31/10/2025 12:05 PM
Submission Source	Online
Late Submission	No
Would you like to talk to your representation at the decision-making hearing for this development?	No
My position is	I oppose the development

Reasons

The amount of indigenous trees required for removal, is extremely distressing. City of Playford should not approve this application for the loss of crucial tree canopy that is needed for cooling, shade and habitat for the area. A carpark takes up 50% of the plan, why can't this be under the building? Why can't they find one of the many sites in the area, that doesn't have this many trees? An ice arena is not necessary infrastructure, the amount of electricity this property will take, alongside the amount of trees removed, will mean it is an extremely environmentally negative development. The fact this is even being considered, Pelligra should be ashamed of themselves!

Attached Documents

Representations**Representor 4** - Conrad William Isterling

Name	Conrad William Isterling
Address	60 ASHFIELD ROAD ELIZABETH SA, 5112 Australia
Submission Date	06/11/2025 10:02 AM
Submission Source	Over Counter
Late Submission	No
Would you like to talk to your representation at the decision-making hearing for this development?	Yes
My position is	I oppose the development
Reasons please see attached	

Attached Documents

Scan_rpengilly_2025-11-06-09-57-11-12718876.pdf
ConradIsterlingRepresentation-AdditionalDocument-12811733.pdf

REPRESENTATION ON APPLICATION

Planning, Development and Infrastructure Act 2016

Applicant: [applicant name] CONRAD WILLIAM ISTERLING
Development Number: 25030642 [development application number]
Nature of Development: ZCITY SPOTS, TALKING [development description of performance assessed elements or aspects of outline consent application]
Zone/Sub-zone/Overlay: 512 [zone/sub-zone/overlay of subject land]
Subject Land: 607 101 ASHFIELD RD ELIZABETH 5112 [street number, street name, suburb, postcode]
 [lot number, plan number, certificate of title number, volume & folio]
Contact Officer: Click here to enter text. [relevant authority name]
Phone Number: Click here to enter text. [authority phone]
Close Date: 20 Feb 2025 [closing date for submissions]
My name*: Click here to enter text. CONRAD WILLIAM ISTERLING
My phone number: Click here to enter text. 08 403081359
My postal address*: Click here to enter text. 60 ASHFIELD RD ELIZABETH
My email: Click here to enter text. conrad@sterling.com.au
 * Indicates mandatory information

My position is: ☐ I support the development
☐ I support the development with some concerns
☒ I oppose the development

The specific reasons I believe that consent should be granted/refused are:

AIR & SOUND POLLUTION ENVIRONMENT.
 INADEQUATE ROADS - RESIDENTIAL
 POOR AREA CAN NOT AFFORD
 NO COMMUNITY MERIDAL RIDGE
 HAVE UNDERUSE GYMNASIUM ON
 VALLIV HIGHWAY 80 M FROM SITE
 DISTRACTION OF TRAFFIC FILTERING
 AIR & PROVIDING COOLING ENVIRONMENT

[attach additional pages as needed]



Government of South Australia
 Department for Housing
 and Urban Development

Note: In order for this submission to be valid, it must:

- be in writing; and
- include the name and address of the person (or persons) who are making the representation; and
- set out the particular reasons why consent should be granted or refused; and
- comment only on the performance-based elements (or aspects) of the proposal, which does not include the:
 - Click here to enter text. *[list any accepted or deemed-to-satisfy elements of the development]*.

I: ☒ wish to be heard in support of my submission*
☐ do not wish to be heard in support of my submission

By: ☒ appearing personally
☐ being represented by the following person: Click here to enter text.

**You may be contacted if you indicate that you wish to be heard by the relevant authority in support of your submission*

Signature:



Date: Click here to enter text.

2-11-2025

Return Address: Click here to enter text. *[relevant authority postal address]* or

Email: Click here to enter text. *[relevant authority email address]* or

Complete online submission: plan.sa.gov.au/have_your_say/notified_developments

MATT BURNELL MP

FIGHTING FOR THE NORTH

14 NOV 2025

Mr Glenn Docherty
Mayor, City of Playford
12 Bishopstone Rd
DAVOREN PARK SA 5113

Dear Mayor Docherty,

Glenn,

I write in regard to enquiries made to the Spence Electorate Office by Mr Conrad Isterling of Ashfield Rd, Elizabeth SA 5112.

Mr Isterling is concerned with the proposed Revo Fitness development at Lot 101 Ashfield Road, Elizabeth SA 5112.

Please find enclosed correspondence provided to my Electorate Office by Mr Isterling concerning this matter.

It would be appreciated if your office could provide information and background on the state of this issue and any proposed action planned for the future.

If you would like to discuss this matter further, please do not hesitate to contact my office on 08 8258 6300 or by email at Matt.Burnell.MP@aph.gov.au.

Yours sincerely,

Matt Burnell

Matt Burnell MP
Federal Member for Spence

5 November 2025

CC Hon Nick Champion MP, Minister for Planning



08 8258 6300

Matt.Burnell.MP@aph.gov.au[MattBurnellMP](#)

8/2-10 Jamies St, Salisbury SA 5108

<https://mattburnell.com.au>

Planning, Development & Infrastructure Act 2016 - Notice under Section 107(3)(a)(ii)

Proposed Development

LOT 101 ASHFIELD RD ELIZABETH SA 5112



APPLICATION NUMBER

25030642

NATURE OF DEVELOPMENT

Construction of a two-storey building comprising a mix of indoor recreation facilities with associated ground level car parking, landscaping, advertisement and tree damaging activity in the form of the removal of 8 regulated and 2 significant trees (Variation to Application 23022101)

VIEW THE PLANS AND HAVE YOUR SAY ON THE APPLICATION

www.plan.sa.gov.au/en/public_notices

MAKE A REPRESENTATION

Up until 11:59pm on the 20-11-2025



FOR MORE INFORMATION

CONTACT
City of Playford

PHONE
8256 0331

EMAIL
plan@playford.sa.gov.au

It is an offence to damage, destroy, obscure or remove this notice. Penalties apply.

We ask you to tell Playford Council to stop putting a gymnasium on top of our trees at the abandoned ice rink site at the corner of Phillip Highway and Main north road opposite the multistorey car park. This area is polluted with tailpipe emissions according to the World Health Organisation.

The benefits of exercising are destroyed in this area because this area is alongside a main road. **This effect is called TRAP. What happens with TRAP is that tailpipe air pollutants PPM 2.5 are drawn deep into the lungs of gym people affecting the living cells. The particles increase the likelihood of sickness and death.**

Historically, the Elizabeth Centre was designed as residential accommodation with a treed buffer zone against the main North Road sound and air pollution. If this REVO building is to be introduced, it would require strong roads around it with a width of 9m to accommodate for buses, which, it does not. The roads are 6m in width; lightly built and narrow enough for local traffic with roundabouts blocking buses and trucks. Not suitable!

The proposed gymnasium is within 20m of a U of 3 highways, the Main North Road, Philip Highway and Ashfield Road. **The World Health Organisation states that no buildings should be placed within 100 m of a major road because of airborne pollution which is now additionally a suspected contributor to Alzheimer's disease (USA Harvard School of Health Analysis).**

Duplications and oversupply. We already have a gymnasium facing Philip Highway near Officeworks. Which incidentally is only just paying its rent because there's too many nearby gyms for it to get enough customers! (Does that sound like the Ramada Hotel fiasco?)

Examples of faulted Playford planning – all placed within the 120 m main North Road pollution band.

The Council Chambers – built on top of the Octagon Theatre and the Brin Whiting lounge which killed off in Elizabeth the blue light disco and big show entertainment such as the Bee Gees.

The Grenfell Centre – awkward with the very tall staircase in the pollution zone and difficult parking.

The Ramada Hotel – abandoned. (The Elizabeth motel can hardly fill its 30 rooms).

The Multistorey Car Park – failed. Only good for its advertising. (250 m is too far for people to walk to Elizabeth Centre!)

The Australian Bureau of Census and Statistics places Playford in the bottom 40 out of the 500+ councils in Australia. Playford are an 8 percentile Council on their research; they need to do better!

"Secret" Tree Report... The Playford Action Group is aware of the contents of **The Ashfield Road Tree Report** which Playford has still not released to the general public. **Local MP Lee Oldenwalder** has had an application to see the report which Playford have held onto for over 2 years. What is wrong with Playford? The tree report allegedly concludes Playford Council has been doing a substandard job of maintaining the Ashfield Road buffer area. Including for example Playford driving a tractor over it dragging a heavy mower which cuts down young shrubs and trees as it cuts the grass, eliminating essential undergrowth. Just have a look!

Complain to or make a representation.

Playford Mayor Glenn Docherty – Council CEO Sam Green 82560333.

Minister for Local Government Joe Szakacs MP. (08) 7133 2070 minister.szakacs@sa.gov.au

Local MP The Right Honourable Lee Oldenwalder 82553030 his office is on Elizabeth way.

Minister for State Planning Nick Champion MP. Office Address: Shop 86B, 600 Main North Road, Smithfield SA 5114. Phone: (08) 8235 5580. Email: ministerchampion@sa.gov.au

You can also make a representation in the council chambers. You are allowed to speak for a few minutes on the topic. Please contact Playford 8256 0331

See you there!

The Save the Trees Birds Animals and Us Playford Action Group.

To leave a message call Secretarial Service 24 Hour 82552439

3 -11- 2025

23rd of (8) August 2022 – main Council meeting 2nd to last Tuesday of the month quoted as the discussion time (which was when Council discussed offering the land to the Pelligra group) presumably and in confidence meeting was held mentioning the then secret tree reports moving the tree reports into official secrecy.

7 September 2022 (following 5 Sept. letter) Conrad sends letter to Geff Brock claiming Pelligra to be given the land cheaply.

14th of January 2024 – in the Playford Council minutes for September 2023 they said the property is 120 m in from the main North Road which is a designated

Road. The distance to the road will minimise any area in noise and air emissions.
The noise pollution and environmental protection policy 2007.

Representations**Representor 5** - Troy Davill

Name	Troy Davill
Address	9 Whitehead Crossing GREENWITH SA, 5125 Australia
Submission Date	20/11/2025 01:49 PM
Submission Source	Online
Late Submission	No
Would you like to talk to your representation at the decision-making hearing for this development?	No
My position is	I oppose the development

Reasons

I have significant concerns on the cost that this proposed facility may charge to local residents. I believe that the required ticketing pricing for access to the ice arena in particular would not be affordable for local residents and would then leave the facility with minimal cash flow. This would put the staff and business in a precarious financial position. The reduction in scope from what was originally promised and what is now being proposed is also an area of concern. If the proposal has already reduced in size, I believe there is a chance that this could happen again. I question the resources that the developer has to contribute to this project if they are already downscaling what was originally promised. I believe a larger council run community facility would be better suited for the area.

Attached Documents

Representations**Representor 6** - Ronald Peter Stirling

Name	Ronald Peter Stirling
Address	29 WILCOX ROAD ELIZABETH SA, 5112 Australia
Submission Date	21/11/2025 12:42 PM
Submission Source	Post
Late Submission	No
Would you like to talk to your representation at the decision-making hearing for this development?	No
My position is	I oppose the development
Reasons please see attached correspondence	

Attached Documents

RepresentationRonaldStirling-12867327.pdf

Rebecca Pengilly

From: Mahmoud Hasaneen
Sent: Friday, 21 November 2025 12:37 PM
To: Laura Goulden
Cc: Rebecca Pengilly
Subject: Resident Representation - 25030642 - Lot 101 Ashfield Rd, Elizabeth

Hi Laura,

Hope you're well. Just wanted to advise you that I've spoken to a resident earlier this week who has posted a representation to us regarding the above development application.

Due to Aus Post timeframes, the physical letter will not be received in time, however, the rep is valid and must be addressed as part of the response to representations.

The representative's details are as follows:

- Mr Ronald Peter Stirling
- Post address: 29 Wilcox Road, Elizabeth
- Does not wish to be heard
- Objects the development

Concerns raised are as follows:

- *Objection to the development due to the land use not being appropriate to the zoning and the removal of the subject trees are not appropriate given the habitat within the site and the importance of open space to the health of nearby residents.*

If you can please address this rep as part of your report that would be appreciated. Separately, I'll send through the rep report today so you can be able to address the rest of the reps prior to the admin tasks being completed on portal i.e. removal of PN sign and evidence signed off by Assessment Manager.

Any questions, feel free to reach out as always.

Kind regards,
Mooda



Mahmoud Hasaneen
Principal Planner - Major Projects
City of Playford

P. (08) 8256 0465 • E. MHasaneen@playford.sa.gov.au
12 Bishopstone Road, Davoren Park, SA 5113

playford.sa.gov.au

playford.sa.gov.au/stayconnected



We acknowledge that we work on Kaurna Country and pay our respects to the Kaurna people and their ongoing spiritual connection to country.



REQUEST FOR DEPUTATION

Before you start

Please read the below information carefully before proceeding with this application.

A deputation may be made by a person or group who wish to appear personally before a Council meeting to speak on a particular matter. The subject matter of the deputation must be within the power of Council to consider, or is a matter that Council is willing to advocate for. Any matters of concern regarding Council Members and Council staff can be raised formally in writing with the Chief Executive Officer or as per the Complaints Handling Procedure.

Each deputation is allocated a five-minute time limit in which to address the meeting. Council Members are invited to ask questions after each deputation (this is not included as part of the allocated 5 minutes).

This form must be received by Council by 9:00am on the Monday morning prior to the meeting. Any request for a deputation received after this time will be treated as a request to appear at the next meeting.

Upon the receipt of a deputation request, the following process will be followed:

- The Chief Executive Officer (CEO) will advise the Presiding Member of the receipt of a deputation request.
- The Presiding Member will consider the request and advise the CEO of acceptance or refusal of the request.
- The CEO will advise the requester in writing of the acceptance of their deputation request (including the details of the date, time, location of the meeting and the length of time allocated for the deputation), or the CEO will advise the requester in writing if their deputation request has been refused.

Please note: If your deputation is accepted, appropriate behaviour shall be maintained at all times by the deputation and the Council Members with all persons being considerate of other people's points of view. Sarcasm, criticism, and other inappropriate behaviour and conduct are not appropriate and will not be tolerated.

Disclaimer: Please be advised that filming, audio-recording and livestreaming may take place at the Council Meeting. By attending this meeting, your image may be broadcasted via the City of Playford YouTube channel.

Deputation Request

Date Received:

1



REQUEST FOR DEPUTATION

To: The Chief Executive Officer

I/We hereby request to be heard at the Council meeting on:

(Please refer to the Council Meeting Calendar on our website for details of Council meetings)

<input checked="" type="checkbox"/> Next available meeting of Council	OR	
---	----	--

Applicant details

Applicant Name:	Miss Kate Isterling
Applicant Email:	william.isterling@gmail.com
Applicant Address:	8 Shipton Street, Elizabeth
Applicant Phone:	0403399700

What is your relationship to the City of Playford?

<input checked="" type="checkbox"/> Resident	<input type="checkbox"/> Ratepayer	<input type="checkbox"/> Other
--	------------------------------------	--------------------------------

Speaker details

Who will you be speaking for?

<input checked="" type="checkbox"/> On my own behalf	<input type="checkbox"/> As the spokesperson of a group of persons Group Name:
--	---

Subject of Deputation:

The subject/topic, along with your name will be recorded in the minutes of the meeting.

Concerning the Proposed Development at Lot 101 Ashfield Road, Elizabeth

Deputation Request

Date Received:

**Describe in detail the topic or issue you wish to speak about**

Please give sufficient details of the matter to enable consideration of your request for a deputation (this is not recorded in the minutes).

I wish to present my view on the proposed development of the treed windbreak of lot 101 Ashfield road that is located three minutes walk from my house.

Other Details

Have you contacted your local Councillor/s about this issue?

☐ Yes ☒ No

If yes, who did you speak to? _____

Have you contacted Council Administration about this issue?

☐ Yes ☒ No

If yes, please provide details below.

Do you wish to distribute handouts or include a presentation as part of your deputation?

☐ Yes ☒ No


If yes, a copy of your handouts or presentation must be provided on submission of this form for consideration by the Presiding Member.

Deputation Request

Date Received:

**Submission**

- ☒ I have read and understood the information provided at the beginning of this form and acknowledge that Council and Committee meetings are open to the public and there are no privileges protecting me in relation to defamation.
- ☒ I understand that by attending this meeting, my image may be broadcasted via the City of Playford YouTube channel.
- ☒ I will maintain appropriate behaviour at all times, and will be considerate of other people's points of view.
- ☐ I have attached a copy of handouts I wish to distribute / presentation to include as part of my deputation.

Name:	Kate Isterling
Signature:	
Date	17/11/2025

Please return completed deputation request to:

City of Playford 12 Bishopstone Road, DAVOREN PARK SA 5113	governance@playford.sa.gov.au
--	--

Deputation Request

Date Received:

4



REQUEST FOR DEPUTATION

Before you start

Please read the below information carefully before proceeding with this application.

A deputation may be made by a person or group who wish to appear personally before a Council meeting to speak on a particular matter. The subject matter of the deputation must be within the power of Council to consider, or is a matter that Council is willing to advocate for. Any matters of concern regarding Council Members and Council staff can be raised formally in writing with the Chief Executive Officer or as per the Complaints Handling Procedure.

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Deputation Request

Date Received:

1



REQUEST FOR DEPUTATION

To: The Chief Executive Officer

I/We hereby request to be heard at the Council meeting on:

(Please refer to the Council Meeting Calendar on our website for details of Council meetings)

<input checked="" type="checkbox"/> Next available meeting of Council	OR	
---	----	--

Applicant details

Applicant Name:	Dr William Isterling
Applicant Email:	william.isterling@gmail.com
Applicant Address:	8 Shipton Street, Elizabeth
Applicant Phone:	0403399700

What is your relationship to the City of Playford?

<input checked="" type="checkbox"/> Resident	<input checked="" type="checkbox"/> Ratepayer	<input type="checkbox"/> Other
--	---	--------------------------------

Speaker details

Who will you be speaking for?

<input checked="" type="checkbox"/> On my own behalf	<input type="checkbox"/> As the spokesperson of a group of persons Group Name:
--	---

Subject of Deputation:

The subject/topic, along with your name will be recorded in the minutes of the meeting.

Concerning the Proposed Development at Lot 101 Ashfield Road, Elizabeth

Deputation Request

Date Received:

**Describe in detail the topic or issue you wish to speak about**

Please give sufficient details of the matter to enable consideration of your request for a deputation (this is not recorded in the minutes).

I wish to present my view on the proposed development of the treed windbreak of lot 101 Ashfield road that is located three minutes walk from my house.

Other Details

Have you contacted your local Councillor/s about this issue?

☐ Yes ☒ No

If yes, who did you speak to? _____

Have you contacted Council Administration about this issue?

☐ Yes ☒ No

If yes, please provide details below.

Do you wish to distribute handouts or include a presentation as part of your deputation?

☐ Yes ☒ No

If yes, a copy of your handouts or presentation must be provided on submission of this form for consideration by the Presiding Member.

Deputation Request

Date Received:

**Submission**

- ☒ I have read and understood the information provided at the beginning of this form and acknowledge that Council and Committee meetings are open to the public and there are no privileges protecting me in relation to defamation.
- ☒ I understand that by attending this meeting, my image may be broadcasted via the City of Playford YouTube channel.
- ☒ I will maintain appropriate behaviour at all times, and will be considerate of other people's points of view.
- ☐ I have attached a copy of handouts I wish to distribute / presentation to include as part of my deputation.

Name:	William Isterling
Signature:	<i>William Isterling</i>
Date	17/11/2025

Please return completed deputation request to:

City of Playford 12 Bishopstone Road, DAVOREN PARK SA 5113	governance@playford.sa.gov.au
--	--

Deputation Request

Date Received:

4



November 27, 2025

Mahmoud Hasaneen
City of Playford
Via: *The PlanSA Portal*

Dear Mahmoud,

RE: DA 25030642 – RESPONSE TO REPRESENTATIONS

We have been instructed by the Applicant, Pelligra Group, to respond to the assertions made, and concerns raised, in the representations received in relation to the above application.

A total of eight representations were received during the notification period and three of the representors have indicated that they wish to be heard at the upcoming Council Assessment Panel (CAP) meeting.

Our responses to the relevant matters raised are outlined, in no particular order, below.

Land Use

Representors 1, 2, 3, 4, 5 and 6

Several representors questioned the need for the proposed facility and the extent to which it would deliver social and economic benefits to the community. Several representors also highlighted differences between the original and variation application, suggesting that the proposal has lost appeal and may be underutilised given the presence of other gym facilities in the area.

In response, we note the following:

- The ice arena itself represents a significant landmark, not only for Elizabeth but as a key component in the development of the northern CBD. Its inclusion provides a unique, national and domestic sporting asset for the region.
- Revo Gym has already expressed a vested interest in the development, indicating that they recognise the ongoing demand for such a facility despite other operators being present in the local market.
- Comments regarding ticket pricing for the ice arena are not planning considerations; however, it is reasonable to assume that the selected operator has undertaken appropriate commercial due diligence in determining pricing structures.
- Council retains oversight in relation to the land use and overall concept, but it is not providing capital financial contributions towards the development. The applicant is both purchasing the land and funding the construction, further demonstrating their confidence in the proposal.

In summary, if there were no demand for this type of facility in the area, the application would not have been pursued, nor would the applicant have made the significant financial commitments already invested in the project. Moreover, demonstrating demand for a particular land use is not a relevant consideration or test having regard to the provisions of the Planning and Design Code (Code).

Tree Removal and Loss of Open Space

Representors 2, 3, 4 and 6

A common concern raised by representors is the loss of the existing open space buffer and trees, noting that this was previously raised during the original application and remains a point of contention.

With respect to the open space buffer, we note that Council:

- passed a decision to proceed with the land revocation process for the purposes of promoting development of the land; and
- has obtained Ministerial Approval for the Revocation of Community Land Classification 2021-2022 that officially endorsed the revocation of the site.

The above actions were progressed to enable the land to be developed in a manner consistent with Council's strategic vision for the site.

With respect to tree removal, we note that reasonable development of the site is not achievable without the removal of a number of existing trees. The applicant has pursued the purchase of the site from Council through an expression of interest process, during which the proposed use and overall concept were designed to align with Council's strategic vision for the land.

Having regard to this process, the need for some tree removal was well established. Further, existing planning consent is already in place for the removal of some trees.

That said, we note that:

- new planning regulations has been introduced since the original approval, which requires a reassessment. Notwithstanding this, 8 regulated and 2 significant trees are proposed to be removed which is an improvement from the original approval which sought to remove 13 regulated and 3 significant trees. The reason for the reduction is because of the reduced floor area together with Trees 1, 2, and 3 no longer falling within the development site. Additionally, the six *Tamarix aphylla* trees are classified as Declared Weed Species under the *Natural Resources Management Act 2004* and are therefore exempt from protection requirements at this location;
- compensation is required for the removal of trees that are either 'regulated' or 'significant' status, as defined by the *Act*. The tree loss must be offset by the planting of a requisite number of new trees or payment in a designated tree fund. In this instance, the nine *Angophora hispida* trees proposed for planting within the car park will count towards the replacement trees, the remaining replacement requirements will be fulfilled through a payment into the relevant urban tree fund, to be made prior to the issue of development approval;
- six regulated trees and one 'exempt' tree (i.e., trees not protected by legislation) will also be retained adjacent the boundaries of the site; and
- the development will accord with the Australian Standard 4970-2009 Protection of Trees and will ensure all work in the vicinity of these trees are discussed and designed in consultation with the Project Arborist and a Tree Protection Plan.

Further, we note that Council, as part of its overarching strategy for the Land Revocation proposal, seeks to provide an enhanced entrance statement into the city, by uplifting Main North Road, revitalising the open greenspace and providing active landscaped areas and public pathways, meaning this specific

allotment does not result in completely wiping out the greenspaces that exist either side of Main North Road.

The Applicant, in consultation with Council are in the process of establishing a landscape proposal that improves the surrounding public realm and streetscape interface to improve and enhance amenity.

Air Pollution

Representors 2 and 4

Two representors have raised concerns about air pollution due to the site's proximity to Main North Road, suggesting it may not be an ideal location for people to exercise. It is understood that this concern relates to the existing context rather than the proposed development itself.

In response, we note that the proposed building will physically shield patrons from Main North Road, while also providing an additional buffer for the adjacent sensitive receivers to the east from traffic noise and potential emissions. The development comprises indoor recreational uses which, as the name suggests, will operate entirely indoors and be serviced by appropriate air-conditioning systems. Furthermore, all doors and windows are oriented westward, with no openings facing Main North Road or Phillip Highway.

It is also worth noting that development adjacent to Main North Road is common and reflects the commercial value of such locations. In this context, a commercial use that provides an effective buffer between Main North Road and nearby sensitive receivers is considered a positive and desirable outcome.

Traffic Congestion

Representors 2 and 4

We refer to the Traffic and Parking Report prepared by CIRQA, included in the lodgement package and made available during the public notification process. This report addresses traffic generation, distribution, and impacts, as well as car parking demand and availability.

In response to concerns regarding traffic congestion, we highlight the following:

- the proposed development is forecast to generate approximately 47 am weekday peak hour trips, 61 pm weekday peak hour trips, and 185 Saturday afternoon peak hour trips, with potential for higher volumes during occasional events. Importantly, these forecasts represent an approximate 15 percent reduction in traffic generation compared to the previously approved development; and
- the original traffic report submitted to and accepted by DIT concluded that the additional traffic volumes could be adequately accommodated within the surrounding road network. As the current proposal results in a lower traffic generation than previously assessed, the traffic impacts will be proportionally reduced. The revised and reduced traffic movements will therefore also be readily accommodated on the adjacent road network.

Accordingly, we submit that the proposed development will not result in an unsafe environment with respect to traffic volumes or movements and emphasise that the variation proposal further reduces overall vehicle trips.

Summary

We trust this adequately responds to the assertions made, and concerns raised, by the representors, and hereby reaffirm our view that the proposed development is deserving of planning consent.

We also wish to confirm our attendance at the scheduled CAP Meeting to respond to any verbal submissions.

If you have any queries or concerns regarding the proposed development, please do not hesitate to contact the writer.

Yours sincerely,



Laura Goulden
Consultant

**5.2 23022101- LOT 1259 ASHFIELD ROAD- MIXED USE DEVELOPMENT
COMPRISING INDOOR RECREATION FACILITY, RETAIL OUTLET, CAFE, TREE
DAMAGING ACTIVITY WITH ASSOCIATED PARKING AND LANDSCAPING**

Snapshot

Author:	David Storey- Ben Green & Associates
Assessing Officer:	Adam Squires
Proposal:	Mixed use retail and indoor recreation centre including ice arena, basketball courts, children's play centre, café, changerooms and storage areas, tree damaging activity involving the removal of 13 regulated and 3 significant trees with associated parking and landscaping
Development Number:	23022101
Date of Lodgement:	6 December 2023
Owner:	City of Playford
Applicant:	Pelligra Group C-/ City Collective
Location:	Lot 1259 Ashfield Road, Elizabeth
Zone:	Urban Activity Centre/ General Neighbourhood
Classification:	Code Assessed - Performance Assessed
Public Notification:	Yes
Representation Received:	Yes
Request for Additional Information Made?	Yes
Recommendation:	To Grant Development Plan Consent

Attachments:

- 1 [Planning Statement](#)
- 2 [Plans](#)
- 3 [Noise Assessment](#)
- 4 [Traffic Report](#)
- 5 [Waste Management plan](#)
- 6 [Arborist Report](#)
- 7 [Stormwater Management Plan](#)
- 8 [Public Notification representations](#)
- 9 [Response to representations](#)
- 10 [RFI Response](#)
- 11 [Event Traffic Control Plan](#)
- 12 [Updated Stormwater detail](#)
- 13 [RFI Response to DIT](#)
- 14 [Response to DIT stormwater query](#)
- 15 [Response to DIT Traffic query](#)
- 16 [DIT Response](#)

1. The Subject Land

The subject land known as Lot 1259 Ashfield Road, Elizabeth and comprises a single parcel that has its primary road frontage to Ashfield Road (western boundary) and secondary frontages to Main North Road to the east and Philip Highway to the north.

The land is currently described as Allotment 1259 within F258096 of the Certificate of Title Reference Volume 6275/ Folio 535.

The land is approximately 24,313m² in area with a frontage of 275 metres to Ashfield Road and 260 metres to Main North Road to the east and 71 metres to Philip Highway.

The land is currently vacant of structures with stands of established vegetation located throughout the site. There are no easements or encumbrances registered on the Title and there is no Land Management Agreement applicable to the land.

The northern portion of the land is zoned Urban Activity Centre and the southern portion is zoned General Neighbourhood.

The Subject Site

The proposed development is limited to the northern portion of the site as highlighted in red in the subject site map below.

A land division application has previously been approved to realign the boundary of the subject land to reflect the subject site of the proposed development.

Subject Site Map



Source: Nearmap

2. The Locality

The locality accommodates a range of retail and commercial land uses, established residential development and passive recreational areas.

To the northwest of the locality comprises a mix retail and commercial activities including bulky goods outlets, supermarket and other speciality shops of the Elizabeth Shopping Centre and the buildings of the City of Playford Civic Centre that form the Elizabeth City Centre.

Established residential areas primarily contain single storey detached dwellings on traditional residential allotments located to the southwest and to the east of the locality.

Fremont Park is located to the northeastern corner of the locality that provides passive recreational activities for the wider community.

The intersection of Philip Highway and Main North Road is a prominent feature to the northern portion of the locality.

2.1 Locality Plan



Source: SAPP

2.2 Zoning

The subject land is located within the Urban Activity Zone and the General Neighbourhood Zone as identified in the Planning and Design Code and is covered by the following Overlays:

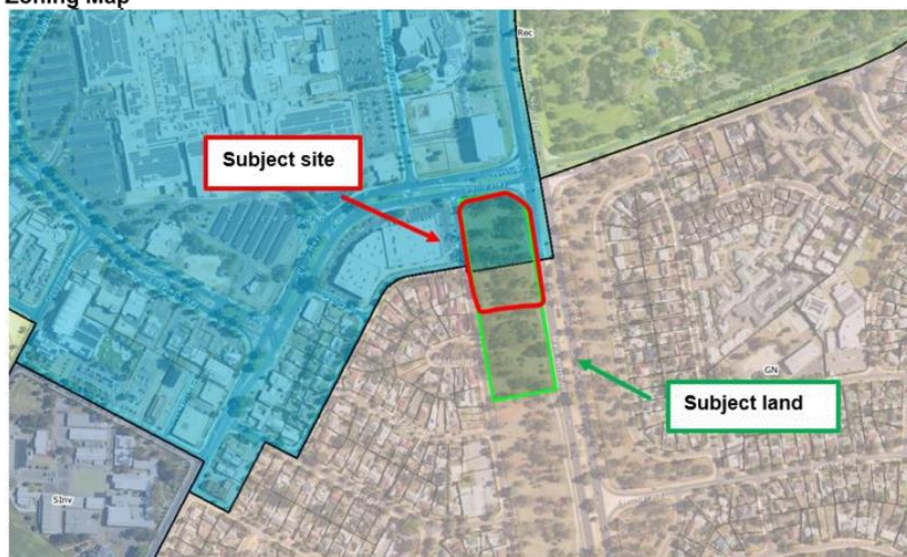
- Advertising Near Signalised Intersections
- Building Near Airfields
- Defence Aviation Area (All structures over 15 metres)
- Defence Aviation Area (All structures over 45 metres)
- Future Road Widening

- Hazards (Flooding - General)
- Major Urban Transport Routes
- Prescribed Wells Area
- Regulated and Significant Tree
- Stormwater Management
- Traffic Generating Development
- Urban Tree Canopy

For the purposes of section 102 of the Act, the relevant authority must assess the development against the applicable policies specified by Table 3 that are relevant to the particular development.

In accordance with Part 1 of the Planning and Design Code where there is more than one zone that applies to the whole of a site the relevant authority can determine policies that are not relevant do not need to be considered when making the assessment of a proposed development.

Zoning Map



Source: SAPPA

3. Background

The site of the proposed development is located to the southern corner of the Phillip Highway and Main North Road intersection with its primary frontage to Main North Road.

The site presents itself as a key landmark entrance to the Elizabeth City Centre which principally includes the Elizabeth Shopping Centre being a regional shopping centre serving the northern suburbs of Adelaide and the Playford Civic Centre which is home to the iconic Shedley Theatre, Playford Library, City of Playford Customer Care Centre and Council Chambers.

The site is located within an area of land colloquially known as the 'windbreaks', representing the northern extent of a portion of reserve which runs parallel to Main North Road in irregular pattern from the southern boundary of the Council area.

The subject land was subject to community land revocation in two stages, with the first portion revoked in June 2013, with a second in February 2018.

The subject land remains under the ownership of Council, whom endorsed an Expression of Interest process for this site and surrounding land in July 2021. This Expression of Interest period ran across August and September 2021. The surrounding land included that other Council and on the northern side of the Main North Road and Philip Highway intersection, inclusive of Lot 8, with an application for this land appearing before the Panel in June 2023. This Expression of Interest process has resulted in Council determining in August 2022 to enter into a Contract of Sale for the subject land with Pelligra.

Prior to the application progressing to submission, significant consideration has been given to the proposed land use, functionality and design of the facility by Council with the formation of an Evaluation Panel and Advisory Group. The Advisory Group has included internal Council stakeholders and Michael McKeown of Jensen Plus as the Urban Design Specialist.

On lodgement, the arborist report supplied with the application identified 10 regulated trees for removal within the site. As part of Council's consideration of the application, Council's arborist noted that 3 of the trees identified as not being regulated were now of a dimension which would be considered a regulated tree. The applicant provided an updated arborist report as part of the application which identified the 3 additional regulated trees. The description of the development was accordingly updated from the removal of ten (10) regulated trees to the removal of 13 (thirteen) regulated trees.

4. The Proposal

The Applicant seeks consent for a two-storey (16.2 metres high) mixed use development of 11,287 sqm that provides a range of recreation which includes ice skating, ice hockey, basketball, rock climbing, gymnasium, and a children's play space within a single purpose-built building.

The development includes the following (floor area):

- Indoor Recreation Facility comprising of:
 - Ice Rink/Ice Hockey Arena (2639 sqm)
 - Ice Arena Food and Beverage (158 sqm)
 - Gymnasium (377 sqm)
 - Basketball courts (1909 sqm)
 - Childrens Play Centre (Funtopia) (424 sqm)
 - Rock-climbing (739 sqm)
 - Locker areas and change-rooms (approx. 400 sqm)
 - Ancillary offices (52 sqm)
 - Storage (95 sqm)
- Shop (23 sqm)
- Café (397 sqm)
- Tree Removal (3 significant trees and 13 regulated trees)
- Car parking areas – 59 at ground level and 177 spaces within the basement level.
- Landscaping

Image of Proposal

Source: City Collective

This image and a copy of the architectural drawings depicting the above are contained within Attachment 1.

5. Procedural Matters**5.1 Classification**

The proposed development comprises of an indoor recreation facility (including gymnasium, children's play centre, indoor basketball centre), shop, café and tree removal.

It is noted that the proposed development comprising a gymnasium, children's play centre, indoor basketball centre are all considered to part of the indoor recreation facility definition, as detailed in Part 7 of the Planning and Design Code.

The above elements are not classified as an Accepted, Deemed-to-Satisfy or Restricted development within the relevant Tables of the Zone. The proposed development is therefore a Code Assessed - Performance Assessed development pursuant to Sections 105(b) and 107 of the Act, requiring an on-merit assessment against the relevant provisions of the Code.

5.2 Relevant Authority

Being the landowner of the subject land Council considered that there was a perception that Council may have a conflict of interest in assessing a development of this nature.

Council resolved that it write to the Minister for Planning to determine if it was appropriate for the Commission to or the Assessment Panel to be the relevant authority in determining a decision.

Pursuant to Section 94 of the *Planning, Development and Infrastructure Act 2016*, the Minister determined that Council's Assessment Panel could be the relevant authority in making a decision.

It is noted that as a form of development requiring public notification (referred below), the development is of a form which the Planning, Development and Infrastructure Act 2016

identifies the Council Assessment Panel as the relevant authority for the purpose of decision making.

5.3 Public Notification

Generally, all classes of performance assessed development require public notification unless, pursuant to Section 107(6) of the Act, the class of development is excluded from notification by the Code in Table 5 – Procedural Matters (PM) - Notification of the relevant Zones.

Public notification was required as the following elements were not listed in Table 5 of the General Neighbourhood Zone and did not meet the requirements of Table 5 of the Urban Activity Zone:

- Indoor Recreation Facility triggers notification, as the subject site is adjacent land used for residential purposes in a neighbourhood-type zone.

The notification process commenced on 8 January 2024 and closed on 29 January 2024 and received **15 representations (from 13 representors)**. A summary of the representations is provided below:

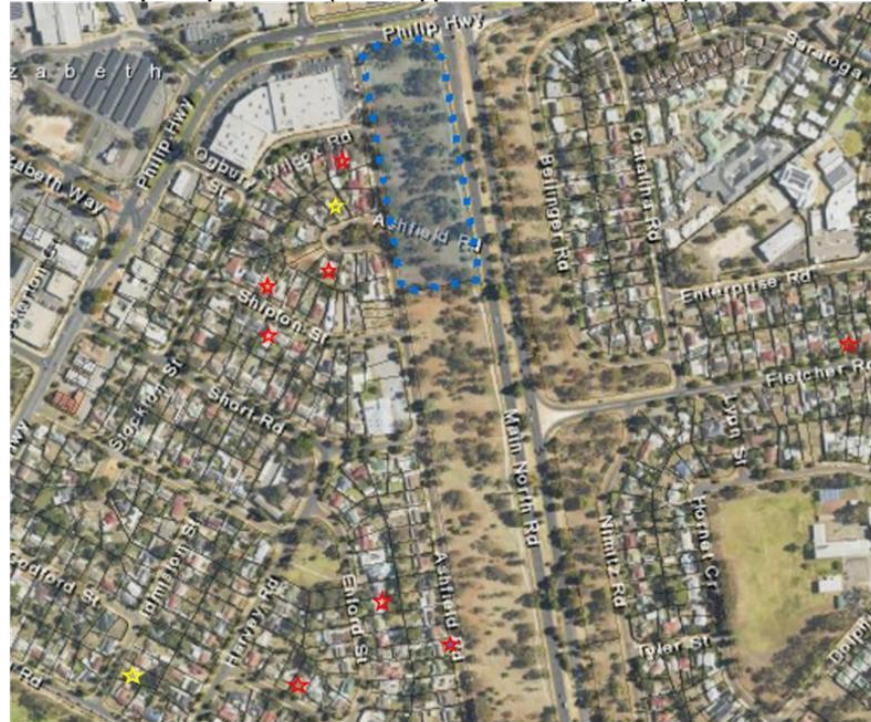
Representor	Summary of Issues Raised	Position	Wish to be heard
Janet Harbottle PO Box 227 PARA HILLS SA 5096	<ul style="list-style-type: none"> • Does not support another sporting complex that is likely to be under-utilised. • Funds would be better utilised supporting the residents of the area and providing better facilities throughout the city centre, not just in one small location. • Fails to understand the logic behind the proposed development and interested to see the data that shows the community support for this facility • The proposal will impact the residents adjacent to the site along with users of the adjacent Elizabeth Shopping Centre with unsavoury behaviour from young people. 	Oppose	No
Annette Ritchie 21 Idmiston Street ELIZABETH SA 5112	<ul style="list-style-type: none"> • Proposed development should be supported as it will provide jobs and provide activities for young people. • Provide opportunities in the participation of indoor recreation activities. 	Support	No
Jordan Ritchie 21 Idmiston Street ELIZABETH SA 5112	<ul style="list-style-type: none"> • The proposal will provide jobs for the Elizabeth area • Provide opportunities in the participation of indoor recreation activities • Increased tourism to the area when major ice hockey games are played 	Support	Yes
Steve Healey 15 Enford Street ELIZABETH SA 5112	<ul style="list-style-type: none"> • Notification process is limited with details of the proposal difficult to find on the website. • Abundant bird life within the site will be disturbed. • Increase in noise impacts to adjoining residents if tree buffer is removed. 	Oppose	No

	<ul style="list-style-type: none"> • Proposal will generate increase traffic movements along Ashfield Road and adjoining local streets. • The site is a green gateway to Elizabeth that is an attraction to the city. • Query the viability and need for a recreation centre on this site. 		
Phoebe Archer 7 Donhead Street ELIZABETH SA 5112	<ul style="list-style-type: none"> • No need for another commercial development on Ashfield Road. • Proposal will create additional rubbish. • Any development proposal on the site should serve the whole community. • Proposal will result in the loss of trees and open space areas. • Increase in noise and pollution. 	Oppose	Yes
William Isterling 8 Shipton Street ELIZABETH SA 5112	<ul style="list-style-type: none"> • A number of development features are not compliant with the zone policies including: <ul style="list-style-type: none"> - building height, - number and location of car parking spaces, - adverse impacts to local residents and does not support the local community. • Proposal is not in keeping with the residential area and will create adverse overshadowing. • Discrepancy in number of car parking spaces the Traffic Impact Assessment Report and the Planning Report. • The proposed development is not suitably connected for pedestrians with the Elizabeth Railway Station and the Elizabeth Shopping Centre. • Proposal will result in additional on street parking. • The proposed landscaping and types of vegetation is not suitable and will not screen the development from adjoining residents. • Concerned that Wilcox Road and Ogbury Road being local roads will not effectively accommodate additional traffic. • Most patrons visiting the site will not be from the local area, not tailored for the local community. • Concerned with how events will be managed and who will pay. • Proposal would be better suited in another location. • As a resident I wasn't asked what I wanted on the site. • Concerns with temperature control of the building, ice resurfacing and storage. • Seating capacity of ice arena is not stated. 	Oppose	No

	<ul style="list-style-type: none"> Unclear as to the purpose and operation of the restaurant. 		
Mohammed Dawood Rezaee 14 Ashfield Road ELIZABETH SA 5112	<ul style="list-style-type: none"> Support the provision of new facilities to accommodate the recreation needs of the community. Proposal will improve the aesthetics of the suburb and renew the area. 	Support	No
Christina Pires 22 Ashfield Road ELIZABETH SA 5112	<ul style="list-style-type: none"> The site is residential land and is vegetated that provides a buffer from Main North which is a source of air and noise pollution. Proposal will increase the level of noise. Impacts to the habitats of birds and animals. The site is a source of local flora and fauna. Concerned with the maintenance of the site by Council. Ashfield Road is a narrow local road that is not capable of accommodating additional traffic. Increase in traffic is of concern to local young residents. Additional vehicle parking along Ashfield Road will cause problems for traffic. Concerned with the location proposed entry and exit points for the development along Ashfield Road – being opposite to other retail activities and likely to cause conflicts with traffic. Query the need for the facility as there are other existing facilities in close proximity. Concerned that the proposal will encourage and lead to more development along Main North Road. Notification process has been lacking and there has been insufficient time to provide a response. 	Oppose	Yes
Beverly Rudnick 17 Fletcher Road ELIZABETH EAST SA 5112	<ul style="list-style-type: none"> Increase in traffic and pollution. Removal of a buffer zone that includes vegetation and significant trees. Proposal will have a negative impact on the aesthetics of the area. Poor maintenance of the site by Council. Proposal is not suited in this location and should be located at the Lionsgate site. The timing and extent of public notification has not been acceptable. 	Oppose	Yes
Margaret Isterling 3 Shipton Road, ELIZABETH SA 5112	<ul style="list-style-type: none"> Increase in traffic within the local area. Concerns that the proposal will result in vehicles using local roads as a thoroughfare. Proposal will result in additional car parking and vehicle movements that will make it unsafe for pedestrians. The proposal does not provide activities that support the local community and will 	Oppose	No

	attract people not from the local area.		
Conrad Isterling 60 Ashfield Road ELIZABETH SA 5112	<ul style="list-style-type: none"> • The proposed development is not an improvement of the site. • The proposed building is the size of a 'circus tent'. • Loss of trees • Vehicle access from Philip Highway is not suitable. • Ashfield Road is a narrow local road that is not suited to increased traffic movements. • Public notification process not appropriate during holiday period. • Inclusion of petition entitled "Stop Playford Council Continuing to Wreck Our Residential Area with Yet More Commercial Development". 	Oppose	Yes
Peter Ruwette 3 Wilcox Road ELIZABETH SA 5112	<ul style="list-style-type: none"> • Existing bulky goods outlets along Philip Highway cause serious safety concerns. • Concerns with activities including movement of vehicles, control of rubbish and crime issues associated with the bulky goods outlets. • The proposal will cause similar noise, air pollution and safety concerns that what currently occurs from the bulky goods outlets. • Proposal will remove well established trees and impact on birds and the local wildlife. • Proposal will cause health issues with the local residents. 	Oppose	Yes
Neave Colbert Address not supplied	<ul style="list-style-type: none"> • Increase to traffic, parking and blocking of driveways to adjoining residents. • Increase in noise and rubbish. • Increase in youth crime. • Increase in Council rates as ratepayers will have to pay for the new facility. 	Oppose	Yes

The following map identifies the location of representors (who nominated their residential address) who were in support or against the proposed development. Red star confirms the location of a representor who opposes the development, and a yellow star is a representor who supports the development.

Location Map – Representors (Red – Oppose / Yellow – Support)

Source: SAPPA

The Applicant's Planning Consultant responded to the representations addressing the following concerns:

- Interface and Amenity Impacts
- Loss of Open Space and Trees
- Building Height
- Traffic and Parking
- Noise and Air Pollution
- Waste
- Pedestrian Safety
- Crime and Unsavoury Behaviour
- Oversupply of Recreation Facilities
- Demand from local community
- Public Notification Process

Issues identified by the representors are also addressed further in the planning assessment section of this report.

It is noted that an assessment of the application is limited to that of the relevant policies and provisions within the Planning and Design Code and that a number of aspects raised as part of the public notification process do not relate to matters than be taken into account regarding a planning assessment of the application. For this reason, matters relating to oversupply of facilities, Council rates, property values, youth crime and Council decision making processes are not addressed as part of the report herein.

The representations received are contained within Attachment 8 while the Applicant's Planning Consultant's response is contained within Attachment 9.

5.4 Statutory Referrals

Pursuant to Schedule 9 of the Regulations, specifically Schedule 9 (3)(7) Development Affecting Transport Routes and Corridors, a statutory referral was issued to the Commissioner of Highways for this development application.

Responses provided from the Commissioner of Highways relating to stormwater flows across Main North Road and traffic movement into and around the site were raised in a request for information by the Commissioner. Response to these are provided within attachments 13, 14 and 15.

This information has been reviewed by staff from the Commissioner of Highways who have offered support of the proposed development with conditions (contained with the Recommendation section).

5.5 Internal Referrals

Internal referrals were undertaken to Council's engineers to review traffic management, car parking, access and stormwater and to the Council's Tree Service team with regards to the impacts to regulated and significant trees.

Additional information was provided in response to Council's review, with these being provided in attachment 10, 11 and 12.

Council's internal stakeholders have reviewed the information provided and have confirmed that the detail and information provided is sufficient to align with relevant Council policies and standards. Matters relating to traffic, stormwater and tree control are addressed in further detail within the body of the report.

6. Key Issues

The following matters are considered pertinent in reaching a recommendation for the proposal:

- Whether the proposal is an appropriate form of development in the Urban Activity Zone and the General Neighbourhood Zone.
- Whether the proposal will create any adverse visual or amenity impacts or conflicts between adjoining land uses within the locality.
- Whether the development will create adverse traffic impacts on the existing road network in the locality or generate any adverse noise or air pollution.

7. Planning Assessment - Zone

The subject land is located within the Urban Activity Zone and the General Neighbourhood Zone (Zones).

7.1 Urban Activity Zone - Desired Outcome and Land Use

The Urban Activity Zone encourages '*...a broad spectrum of regional level business, shopping, entertainment and recreational facilities that can cater for large crowds, smaller social gatherings and events over extended hour.*', as per Zone DO 1.

The proposal is considered to be an appropriate land use within the Zone as it provides a range of recreation facilities and shops that can accommodate large groups of people, located in proximity to nearby transport infrastructure and can operate over extended hours of operations. This is accordance with desired outcomes of the Zone.

The proposal incorporates a number of recreational and retail activities that complement the activity centre function of the Elizabeth City Centre and are listed as envisaged land uses in DPF 1.1 of the Urban Activity Zone. The Elizabeth City Centre is identified as a regional centre within Council's retail hierarchy and as such, the further expansion and of the centre is considered to be a logical and rationale expansion to existing facilities, providing opportunities for shared trip generation, shared services and able to leverage from existing services within the centre.

The proposal provides additional activation to the Elizabeth City Centre as hub for shopping, entertainment and recreational activities. The proposal reinforces the Elizabeth City Centre as a focus of social gatherings and promoting after hour activities. This supports PO 1.2 of the Zone.

It is considered that the proposal's range of recreational facilities is appropriate for its location that supports the key performance outcomes of the Zone.

7.2 Urban Activity Zone - Built Form and Character

The proposed development is a two-storey building that is located at the corner of Main North Road and Philip Highway.

The site presents itself as the southern gateway to the Elizabeth City Centre and the proposal activates this location by extending the built form that frames the Elizabeth City Centre along Main North Road and Philip Highway with the presentation of a visually interesting building.

Further the proposal seeks to integrate with the southern edge of the Elizabeth City Centre by providing recreational activities that support the function of the activity centre as place to people to meet to experience a range of goods and services.

The provision of suitably designed vehicular access points and parking areas ensure that the proposed facility can accommodate shared use of parking spaces between the other land uses within the city centre.

The elevations provide sufficient visual interest and finishes through the use of a mix of materials and articulation to the adjoining roads, specifically its frontage to Ashfield Road and Philip Highway.

The proposed materials consist of a combination of aluminium cladding in 'Southerly' colorbond finish and precast concrete wall panels in a white or "off-white" colour with a "scoring" pattern to the main facades. Other design and architectural features aluminium shading fins, tall narrow glazing panels and timber panel fencing.

Along with the articulated elements with varying building setbacks and frontages that present to Ashfield Road and Philip Highway, it is considered that the proposed building creates visual interest and positively contributes to the streetscapes of these roads. This supports PO 2.6 of the Zone.

Based on the above, it is considered that the development provides distinctive built elements of an appropriate scale for the land use and location. The built form comprises of a variety of building materials with visual interest and a scale that is appropriate within the Zone.

7.3 Urban Activity Zone – Building height

The indoor recreation facility building has a roof height of 16.2 metres which is consistent with existing height levels of buildings that are exhibited in the Elizabeth Shopping Centre and bulky goods outlets that are located to the west along Philip Highway.

The Urban Activity Zone does not provide a maximum or minimum building height but generally seeks medium rise development with the highest intensity of built form at the centre of the zone and lower scale at the peripheral zone interface.

The built element of the proposed building that faces Ashfield Road and to adjoining residential properties that are located in the General Neighbourhood Zone has been designed with articulated design elements and varying setbacks to provide an orderly transition to the built form in the neighbouring zone and complements the streetscape character.

7.4 Urban Activity Zone – Advertisements

Proposed signage is not included within the proposal and will be dealt with in a separate application.

It is noted that the drawings submitted with the proposed development incorporates indicative signage on the building (wall mounted) located to the northern and western frontages and the applicant has confirmed that these would be addressed by a separate application.

7.5 Urban Activity Zone – Vehicle Parking

The proposed development incorporates vehicle parking areas that are sited to street level and basement parking.

Being at grade (ground level) and basement level along with proposed landscaping ensures parking areas are sited and designed to minimise visual dominance when viewed from the street and from adjoining residential properties. This satisfies PO 6.1 of the Urban Activity Zone.

7.6 Concept Plan 81 - Edinburgh Defence Airfield Lighting Constraints

The proposal is located within the 'Controlled Light Installation Area', of Concept Plan 81 – Edinburgh Defence Airfield Lighting Constraints and is located within the Civil Aviation Safety Authority's 6km radius from a (defence) airfield.

It is considered that the proposed development will not cause any adverse upward light that would adversely impact on the operation of the airfield.

7.7 General Neighbourhood Zone – Desired Outcome and Land Use

The General Neighbourhood Zone primarily seeks low and medium density housing that supports a range of needs of lifestyles but does anticipate employment and community service uses that contribute to making the neighbourhood a convenient place to live without comprising residential amenity.

Whilst the proposal for indoor recreation centre is not listed as an envisaged form of development of the General Neighbourhood Zone, PO 1.1 of the Zone considers and allows for complementary non-residential uses that support an active, convenient and walkable neighbourhood.

It is considered that the proposed development provides opportunities for community and recreational based activities whilst being designed and sited to complement the

residential character amenity of the local area. The incorporation of articulated design features, low reflective materials and generous setbacks presents a scale and built form that complements the residential character.

It is noted that the main portion of the proposed development is located within the northern part of the site (Urban Activity Centre Zone), whilst a portion of the recreation facility including children's play space (Funtopia), gymnasium, basketball courts and the basement car parking are located within the General Neighbourhood Zone.

It is considered that these elements are representative of elements of development which would not be uncommon or unexpected within the General Neighbourhood Zone. While the size and scale of the built form along the zone boundary may be greater than generally experienced in the zone, with a portion of development encroaching within the General Neighbourhood Zone, weighting to Zone policy is applied against the adjoining policies of the Urban Activity Centre.

The indoor recreation facility component located within the General Neighbourhood Zone is considered to be an appropriate form of development that contributes to and supports an active and convenient neighbourhood as sought by PO 1.1 of the General Neighbourhood Zone.

It is considered that the overall design of the proposed building will appropriately limit potential negative impacts on the residential amenity of the General Neighbourhood Zone and provides an acceptable separation to the residences opposite (on Ashfield Road). On a balanced assessment against the relevant provisions of the Zone, the portion of the development within the General Neighbourhood Zone is not considered to be at variance with the General Neighbourhood Zone.

7.8 General Neighbourhood Zone – Building height and Setbacks

The proposal for a two-storey building presents a low-rise development that complements the varying built form that is displayed to the southern edge of the Elizabeth City Centre and surrounding residential areas which are generally single storey to the west of the site. Two storey development is evident in the wider locality due to infill type development which reflects the changing character surrounding the Activity centre due to densification.

The proposed building provides an appropriate transition of built form and scale of buildings from the large bulky commercial buildings located within Elizabeth City Centre and the adjoining bulky goods outlets the north and west of the subject land to the more residential built form located to the southwest. Given the presence of Main North Road as an arterial road to the eastern boundary of the site, the two-storey element of the development provides suitable context against this road, with the carpark area providing separation to the dwellings to the west.

The siting of the building being setback from Ashfield Road (minimum setback of 16.6 metres), generous spacing around the site and attractive built form limits the visual impact of the development and complements the suburban residential streetscape. This supports PO 4.1 and PO 5.1 of the General Neighbourhood Zone.

7.9 General Neighbourhood Zone - Advertisements

The proposal only includes provisional signage that show indicative placements of signs to the northern and western facades of the building, with this aspect of the development located within the adjoining zone.

The final signage plan will be part of a future and separate application pending the details of the future operator. It is reasonably expected that any signage would not be

located within the GNZ, instead being more likely to locate towards the intersection of Main North Road and Philip Highway.

8. Planning Assessment – Overlay

8.1 Advertising Near Signalised Intersections

The architectural drawings submitted by the applicant show indicative signage locations on the northern and western elevations of the building, however these are provided as a guide only to provide an indicative representation of the size and scale of the development and what future form may represent.

The indicative signage does not form part of the application and will be subject to a future and separate application.

As the proposed development does not comprise any illuminated and moving or changing display it is considered that the development does not cause unreasonable distraction to road users.

8.2 Building Near Airfields

The Building Near Airfields Overlay seeks that the operational and safety requirements of airfields are maintained through the management of lighting, turbulence and other activities.

The proposed development does not incorporate any outdoor lighting that would pose a hazard to commercial or military aircraft operations.

It is considered that the proposal does not result in any adverse impacts to the operations identified in this Overlay.

8.3 Defence Aviation Area

The application proposes a building which exceed 15 metres in height (16.2 metres) that may potentially impact on the operational and safety requirements of nearby Defence Aviation Areas.

In accordance with the Defence Aviation Area Overlay it is a requirement that the applicant obtain the necessary approval/clearances from the relevant government agency. This consent is obtain outside of the statutory planning and referral framework and as such, if the Panel are minded to support the application, a reserved matter relating to this consent has been included for consideration.

8.4 Future Road Widening

The Future Road Widening Overlay applies to the subject land which seeks to ensure that any development is located and designed to minimise any impact on future road widening requirements.

The design and setback of the building has taken into account relevant road widening setbacks to ensure any future upgrade or works to Main North Road or the intersection would not impact the proposed built form of the application. DIT has assessed the traffic impacts of the proposed development and determined that the proposal will not impact on future road widening requirements of Main North Road and Philip Highway.

8.5 Hazards (Flooding – General)

The applicant has engaged FMG to prepare a Stormwater Management Report describes the localised flood inundation, and how the proposed development will mitigate any impacts as a result of these localised floods.

As guided by FMG, the finished floor level of the proposed built form will be raised 300mm above flood levels, whilst flood waters will be unrestricted as they flow through the adjacent reserve to the north.

It is considered the development being designed with a finished floor level at least 300mm above kerb level will prevent the entry of floodwaters into the site. This satisfies DPF 2.1 of the Hazards (Flooding – General) Overlay.

The FMG report also considered that a new 150mm high barrier kerb along Main North Rd will be installed to safely divert existing overland floodwaters within the road reserve and a 215m³ detention tank is proposed to be installed on site to ensure post development peak discharge during the major/minor storm events do not exceed the pre-development peak flow rate during equivalent events. These works have been considered as part of the referral to the Commissioner of Highways, with These design elements will assist to minimise general flood risk of the proposal. This supports DO1 and PO 1.1 of the Hazards (Flooding – General) Overlay.

8.6 Major Urban Transport Routes

The proposal does not propose any direct vehicle access to/from a State Maintained Road.

The applicant has sought advice from CIRQA traffic engineers who have assessed the proposal and determined that it can provide safe and efficient access from a local road (Ashfield Road) and on to Main North Road which is listed as a Major Urban Transport Route. The information relating to this, along with associated SIDRA analysis from the applicants consultants has been reviewed by staff at DIT who have confirmed the anticipated vehicle movements as being appropriate for the location and existing road network. As part of the response contained within Attachment 16, DIT have recommended a number of conditions which are provided within the recommendation portion of the report.

8.7 Regulated and Significant Trees

The applicant engaged Arborman Tree Solutions to undertake an assessment of existing trees at the proposed site of the development. The purpose of the assessment is to identify potential impacts the proposed development will have on the trees and provide mitigation strategies to minimise the impact where appropriate.

Following liaison with the Council Tree Arborist team the assessment was updated to consider twenty-six trees which are identified as a mix of indigenous, native and exotic species.

Of the 26 trees assessed, four (4) are identified as Significant trees, and twenty (20) are identified as Regulated trees. One tree was determined to be exempt from regulation and another tree as unregulated.

The applicant sought to work with Council where possible to mitigate removal and retain trees where possible within the development footprint.

The proposal includes the removal of three (3) significant trees and thirteen (13) regulated trees as a direct result of physical clashes with the proposed built form.

As part of the development, the applicant was able to demonstrate that regulated trees along the Ashfield Road frontage (4, 5, 6 & 7) and Main North Road (10 & 11) as shown below, were able to be incorporated as part of the development.



Council's Tree Arborist has assessed the report and associated works, noting impacts to the TPZ's of retained trees. They have considered that the impacts to the retained trees are acceptable and that the long term health and viability of these trees can be appropriately managed, subject to a number of conditions including permeable materials be used to surfaces in the car parking areas.

With regard to the significant and regulated trees which are proposed to be removed, an assessment of the amenity value of these trees has been undertaken, along with consideration as to how the reasonable development of the land in a manner envisaged by the zoning of the land could be facilitated.

It is acknowledged that the trees within the site make a positive contribution to the amenity of the area, as part of the 'Wind Breaks' which run for the length of Main North Road. This amenity value is considered to derive largely from the cumulative volume of the trees rather than any singular distinct tree contributing more or less than other trees along this portion of the road.

The subject land can be reasonably anticipated to experience a higher order of use and site coverage due to the Urban Activity Centre Zoning and the anticipated suite of land uses within the zone. As identified in the above sections, the proposed land use is considered to be appropriate for the zone, and weighting of this is taken when considering the development of the land.

Due to the disbursement of trees across the site, it is not considered that all trees could be practically retained while providing for the reasonable development of the site. It is noted that where possible, trees along the site boundaries are to be retained, which provides for the highest level of visibility to the public along site boundaries.

Given the extend and volume of trees in the wider locality, on balance, the removal trees within the subject site is not considered to result in a level of harm to the immediate and wider amenity which would outweigh development of the land as reasonably anticipated by the Zone. In this respect, the development is considered to appropriately address DO1, PO1.1, PO1.2 and PO1.4 of the Regulated and Significant Tree Overlay.

8.8 Stormwater Management

The proposal includes a stormwater management plan prepared by FMG. The stormwater management plan demonstrates the suitability of the development and the effective management of stormwater.

The FMG report confirms that preliminary building finished floor level (FFL) will be raised 300mm above flood levels and flood can travel along the adjacent kerb and verge to the north and west if necessary.

Further the report confirms the establishment of the finished floor levels at 35.00m AHD FFL will mitigate any potential flood impacts, where this provides a 300mm freeboard in flood events except for the standalone retail food area nominated in the northeastern corner, with a FFL of 33.50m AHD has been nominated.

The report also notes that all the walls and windows in eastern and northern side of the site must be protected against water. All verges around the building will be adjusted to ensure the land does not grade towards the building.

Stormwater detail has been reviewed by Council's engineer who has confirmed that the built form and onsite detention provided are appropriate to mitigate existing overland flows which travel across Main North Road from the east. This detail has also been reviewed by DIT who have provided similar advice.

Stormwater flows generated from the site would be accommodated within the existing stormwater system along Ashfield Road. Noting the age and capacity of this system, Council identified pre-development flow levels which the development was not to exceed. The stormwater detail provided by FMG contained within Attachment 7 and 12 has been reviewed by Council's engineer who has confirmed that the discharge rates of 50l/s and 219 l/s during minor and major storm events are acceptable for drainage within the existing system

8.9 Traffic Generating Development

The proposal does not propose vehicle access to/from a State Maintained Road, nor would it have an impact on the performance of State Maintained Roads in the vicinity given the existing road network in which Ashfield Road is under the care and control of the City of Playford, in which vehicles will access the site from.

As identified above, staff at DIT have reviewed the proposed traffic rates and detail, with support being provided, subject to condition as detailed in their response within Attachment 16.

8.10 Urban Tree Canopy

This Overlay relates to the provision of trees for residential development. As no residential development is proposed this Overlay does not apply.

9. General Development Policies

9.1 Advertisements

As noted in the Overlay section, signage is shown in the plan documentation is indicative only and does not form part of the proposed development.

Any proposed signage will be subject to a future and separate application.

9.2 Design in Urban Areas

The development incorporates a mix of materials including concrete, fibre cement cladding, glazing, and aluminium shading fins that present an architecturally attractive building which are considered to be durable and sustainable.

The two-storey building reinforces its corners through an articulated ground level facades with curved walling and differing materials. The building provides an attractive front façade to Ashfield Road with the inclusion of a canopy over the entrance which provides protection through to a recessed entry to the foyer.

The building is situated with a reduced setback to the north and west, which reinforces its street address from the Main North Road and Philip Highway perspective.

Surveillance is provided through active land uses at ground level with the use of café and retail area and its primary entrance that fronts Ashfield Road. Additional surveillance is afforded through the glazed restaurant tenancy at the northern part of the development.

Waste storage is appropriately contained on site, with a dedicated collection area which is separate to the main pedestrian environment of the development. The applicant has engaged with Rawtec to identifying guidelines for waste management.

The Waste Management Plan includes a high-level proposal for a waste management system, with preliminary designs to confirm how waste can be managed at the site. This detail has been reviewed by Council staff who have confirmed that it generally aligns with collection practice as would be expected for a commercial development of this type. Service and waste collection is provided to existing business's to the west along Philip Highway and an expansion to this service could reasonably expect to be accommodated.

9.3 Interface Between Land Uses

The Interface between Land Uses General Development section of the Planning and Design Code contains a suite of provisions that seek to mitigate adverse impacts on or from neighbouring land uses. These policies are considered applicable to the proposed development.

The subject site adjoins a General Neighbourhood Zone where are a number of sensitive receivers are located in the form of residential properties to the west of the subject site. There is also land on the opposite side of Main North Road and to the immediate south of the subject site (which is currently vacant) that is located within the General Neighbourhood Zone.

An Environmental Noise Assessment (Acoustic Report) has been undertaken by Sonus for the proposed development to assess the level of noise impacts from car parking activities, rubbish collection, operating hours, and mechanical plants operations.

The report concluded that the proposal, subject to a number of noise attenuation measures, can be supported to ensure that it will not cause unreasonable interference or detrimentally affect the amenity of the locality and ensures consistency with the intent of the Planning and Design Code. As noted in the report, a high level of vehicle noise is currently experienced from Main North Road (a noise generation source in its own right), with proposed level of activity and noise likely to be less than currently experienced from the arterial road. Subject to the conditions and recommendations contained within the report, it is considered that the development would not result in an unreasonable level of noise generation.

The Noise Assessment Report is included in the Attachments as Attachment 3.

The key issues identified for consideration relate to hours of operation, noise, overshadowing and light spill. Each of these matters are considered under the following headings:

Hours of Operation

The facility is proposed to be operational between 6 am and 10 pm for the ice arena and associated café and basketball courts, 9am to 9pm for the rock climbing and children play centre and externally operated café. The gymnasium will operate 24 hours per day 7 days a week.

The proposed hours of operation is generally consistent with other similar indoor recreation facilities which typically accommodate busy periods during the morning and evening on weekdays and busy periods during the day on weekends. Comparable hours of operation are evident within the wider Elizabeth Centre, with a 24-7 gym currently operation in the group of shops located within the building to the west of the site.

Subject to the proposed development incorporating a number of acoustic treatments, identified by Sonus the proposed development including the operation of a 24-hour access gymnasium can be supported.

Noise

It is anticipated that the proposed development will generate level of noise from vehicle movements, patrons entering and exiting the facility and music from the gymnasium.

The proposed building of the facility has been sited and designed to ensure the recreational activities are located to the western and northern portion of the building. The proposed development is setback 16.6 metres from its western boundary which assists to limit any adverse noise impacts to the adjoining residents located along Ashfield Road.

Although the activity of the proposed development will generate some noise levels predicted to exceed limits for sensitive receivers located along Ashfield Road, Sonus consider the development can be supported subject to a number of noise attenuation measures being incorporated into the design including (as outlined in Sonus report):

- *mechanical plant screen be constructed;*
- *acoustic insulation be incorporated around plant and equipment*
- *a boundary fence be constructed between the site and future residences and*
- *24-hour gym be constructed from materials with a minimum construction thickness/specification.*

A Condition of Approval consistent with the recommendations contained within the Sonus Environmental Noise Assessment has been included in the recommendation should the Panel determine to grant Planning Consent for the proposal.

Final detail relating to mechanical plant, as recommended in the Sonus report is to provided for consideration by way of a reserved matter, as detailed in the recommendation.

Lighting

All proposed lighting and illumination within the site will be designed to conform with Australian Standards AS 4282 – Control of the obtrusive effects of outdoor lighting.

This will ensure that all lighting will be directed downwards into the site and not into adjoining residential allotments. This supports PO 6.1 of the Design in Urban Areas Module.

A Condition of Approval that reinforces this requirement has been included within the recommendation should the Panel determine to grant Planning Consent for the proposal.

9.4 Traffic Impact, Access and Car Parking

The applicant engaged with CIRQA traffic engineers to provide design and traffic assessment advice for the proposed development and specifically to prepare a traffic impact assessment report.

The proposal includes vehicular parking areas that include 59 spaces at ground level (at grade) and 177 spaces within the basement resulting in a total of 236 parking spaces. Vehicle parking areas will also include a separate pick-up/drop-off bay along with a designated waste collection/loading bay.

Vehicle Parking

The proposed development provides 236 off street parking spaces within the site.

To assess minimum car parking rates consideration is to be taken from *Table 1 – General Off Street Parking Requirements* and *Table 2 – Off Street Car Parking Requirements in Designated Areas*.

Parking rates for non-residential development located within an Urban Activity Centre Zone (Designated Area) is to be assessed against the provisions of Table 2 and land within the General Neighbourhood Zone is to be assessed against Table 1.

In accordance with *Table 1 – General Off Street Car Parking Requirements* of the Code, the following car parking rates apply to the proposed land uses:

- *Indoor recreation facility* – 6.5 spaces per 100 m² for a fitness centre and 4.5 space per 100sqm for all other types of indoor recreation facilities; and
- *Shop (including café/restaurant)* - 0.4 spaces per seat.

On the basis of the Table 1, there would be a requirement for approximately 400 parking spaces associated with the proposal if the assessment was just based on Table 1 and therefore the proposal would be deficient in the number of parking spaces.

However given its location within the Urban Activity Zone it is considered that an assessment of minimum parking spaces primarily in accordance with Table 2 is appropriate.

CIRQA notes that this assessment should consider the mixed-use nature of the site and associated efficiencies that would be achieved and that the Planning and Design Code does contemplate lower parking provision based on development and land use consideration.

Based on a number of considerations CIRQA has considered that it is reasonable to adopt reduced parking provision rates for the assessment of the proposal. CIRQA notes that the Code identifies a minimum provision of 3 spaces per 100 sqm gross leadable floor are for non-residential development within 'Designated Areas' and that this rate is considered appropriate for this development.

CIRQA has thus adopted a flat rate of 3 spaces per 100 sqm to all tenancies, and therefore would be a requirement for 250 parking spaces associated with the proposal. The proposed provision of 236 parking spaces would have a minor shortfall against such a requirement.

It is considered that the assessment of the minimum number of parking spaces by CIRQA in accordance with Table 2 is appropriate (3 spaces per 100 sqm of gross leasable floor area) and satisfies DPF 5.1 of the Transport, Access and Parking General Development Policy Module.

In addition, it is worthy to note the location of the subject site in proximity to the Elizabeth City Centre which provides extensive parking spaces that provides shared parking between the city centre and the subject land. This satisfies PO 6.3 of the Transport, Access and Parking Module.

Further the City Centre is a high frequency public transit area that supports opportunities for patrons of the proposal facility to use public transport rather than private vehicles thus supporting less reliance on car usage. This supports the intent of the desired outcomes of the Urban Activity Centre Zone.

As with all commercial developments which may be subject to periods of high parking demands, Council will monitor parking use and provision to ensure that inappropriate parking practices do not become prevalent within the local street network. As part of this ongoing monitoring, Council would be able to consider event parking restrictions, resident permits or other controls as dictated by future use. Parking provision has been reviewed by Council's traffic engineer who has confirmed that the development provides a level of parking consistent with Zone requirements and as expected for a mixed use commercial development which provides opportunity for shared trip generation.

In summary, it is considered that the proposed development supports the integrated transport system by providing sufficient parking spaces in accordance with Table 2 – Designated Areas and connecting with the high public transit opportunities of the Elizabeth City Centre. This satisfies the desire outcome of the Transport, Access and Parking Module.

Bicycle parking is not provided within the car parking areas or around the site however end of trip facilities is proposed within the basement. Similar to parking provisions detailed above, if cycle parking were identified as a future demand, Council would be able to consider this as part of their cycling and walking strategy. This supports PO 9.3 of the Transport, Access and Parking Module.

Access, Movement and Layout

Pedestrian access to the site will be via the existing signalised pedestrian crossing at the intersection of Main North Road and Philip Highway. As part of Council's broader project scope for the CBD, Council have undertaken master planning work for the upgrade of public realm along Ashfield Road and Philip Highway, with further consideration towards pedestrian crossings and refuge along Philip Highway. These works are currently in concept design, with a view toward progressing this project in coming years. Notwithstanding this, it is considered that the existing signalised crossing point is appropriate to facilitate pedestrian movements across Philip Highway.

The CIRQA report confirms that the vehicle access is proposed via Ashfield Road by two (two-way) access points to accommodate vehicle movements associated with the site.

It is considered that the access points are well spaced and located to limit any adverse impacts on the neighbouring properties and not to interfere with street trees and infrastructure to maintain the appearance of the streetscape.

With regards to the movement of vehicles, the CIRQA report notes that the proposal is forecast to:

generate in the order of 55 am and 72 pm peak hour trips on weekdays and 219 peak hour trips on Saturdays. Higher volumes may be generated during events (ice hockey

league games) at the site, however, this would infrequently and would occur well outside of the commuter traffic periods.

While there will be an increase in movements distributed via the adjacent roads and intersections, it is considered the additional movements will be readily accommodated at the proposed site access and on the adjacent road network.

The increase to traffic movements is considered to be reasonable and will minimise any adverse impact on the functional performance of the transport system. This supports PO 1.1 of the Transport, Access and Parking Module.

With regards to larger vehicles accessing the site the CIRQA report also advises that the access points and internal circulation roads have been designed to accommodate the turning movements of the largest vehicles anticipated within the site including the loading and unloading of vehicles and that all movements can enter and exit in a forward direction. It is considered that layout design of the parking areas and proposed movements will cause any adverse queuing on public roads. This satisfies PO 1.4 of the Transport, Access and Parking Module.

It is noted that a number of representations raised concerns with the potential for the local street network to be impacted by vehicle movements and parking overspill. As detailed above, the parking provision has been reviewed and considered appropriate for the site.

Regarding vehicle movement patterns, Council's Traffic engineer has reviewed the anticipated trip generation and movement breakdowns prepared by CIRQA. While some level of local movement may be generated by the development, Council's engineer supported the position of CIRQA which suggested a trip disbursement of less than 5 southbound movements along Ashfield Road for the weekday AM and 10 for the Weekday PM.



Figure 2 - Forecast distribution of movements during the weekday am, (weekday pm) and [Saturday] peak hours

This level of trip disbursement would be within the scope and function of the existing road network and would not result in volumes of traffic to change the function or require substantive upgrade to the existing network.

In conclusion, it is considered that the proposal can provide adequate on-site car parking and will not create adverse impacts upon the adjoining road network.

Council's Traffic engineer did not raise any concerns with the proposed development and confirmed that the traffic detail and internal circulation for the site were appropriate for the proposed development. Additional information was provided regarding event management plans for certain events and Council's engineer has confirmed these as being appropriate. A condition relating to this is provided for in the recommendation portion of the report.

9.5 Landscaping

The Applicant has prepared a Landscaping Plan that proposes plant species and specified heights of the trees and plants. Given that the built form would be the predominant element of the site, landscaping areas are limited to those within the parking area of the site and along the site boundaries.

The landscape plan demonstrates appropriate levels of predominantly within the Ashfield Road frontage and within the car parking areas to soften the impact of the proposed built form and extensive hardstand areas.

These landscape elements have been incorporated with existing retained trees and located along Ashfield Road and provide new trees and landscaping with the parking areas and to the primary frontage of the building to soften the visual impact of the building.

Within an Urban Activity Zone, there is a reasonable expectation of built form occupying larger areas of the site, with zone policy geared towards active frontages (PO1.6 & PO 2.6) and attractive built form (PO 1.1 & 2.3). The proposed landscaping is considered to compliment the proposed built form, achieving a design outcome which is appropriate to both the current and future setting of the site.

It is considered that there is an appropriate level of landscaping to enhance the visual appearance of development and overall amenity of the site and locality, satisfying Zone Performance Outcome 5.1 and 5.2 of the Design in Urban Areas Module.

9.6 Stormwater

In Council's Request for Further Information, it was requested the applicant provide additional detail in relation to the stormwater calculations and further details relating to DRAINS modelling.

The applicant provided updated stormwater and civil designs with details on the DRAINS modelling. This detail has been reviewed by Council's engineer who has confirmed that the stormwater detail provided by FMG has addressed previous comments. Stormwater has been addressed in greater detail within section 8.8 of this report and is considered appropriate for the proposed development.

9.7 Event Management

The applicant has prepared an Event Traffic Management Plan, which details how the road network will be managed to mitigate implications on the local road network and limiting any adverse impacts to adjacent residences which specifies overspill parking areas, speed restrictions as well as designating pedestrian movements.

Whilst not strictly a planning issue, it is acknowledged that the site may generate higher levels of activity in certain events. It is considered that the details of an event management plan be adopted by way of a condition to ensure that such a plan is kept current to reflect future events and is available for inspection as may be requested by Council. Such events are considered to be those referred to on page 12 of the CIRQA report as 'event mode' activities which would generate traffic volumes above the forecast weekday and weekend peaks during normal operation.

10. Conclusion

The development aligns with the overarching principles of the Urban Activity Zone and the General Neighbourhood Zone to provide a use that supports the local and wider community.

On a balanced assessment, and as detailed in the preceding pages, the development is not considered to be seriously at variance with relevant provisions of the Planning and Design Code.

The proposed development delivers a land use that is considered to be acceptable for the Zones. The design, bulk and scale of the proposed development is compatible with the Urban Activity Zone Desired and Performance Outcomes and has been designed to minimise adverse impacts to the residential properties located within the General Neighbourhood Zone.

The built form is considered to be appropriately high quality as would be expected for a site with such prominence to the public realm and achieves built form outcomes consistent with the evolution of the Elizabeth Regional Centre.

The proposal is not anticipated to result in any unreasonable noise impacts upon the living amenity of nearby residents as outlined in the Sonus Acoustic Report subject to the adoption of acoustic treatments.

The proposed removal of the 3 significant and 13 regulated trees has been considered against the existing amenity provision of the locality and the applicable policy and on balance is considered to be supportable. It is noted that the retention of existing trees along Ashfield Road and the Main North Road frontage of the site are considered to be positive outcomes in integrating the built form with existing character of the site.

The Commissioner of Highways was identified as prescribed body to provide advice and assess the traffic impacts of the proposed development. Staff from the Department of Infrastructure and Transport undertook an assessment of the application and following some the provision of additional information from CIRQA traffic engineers are satisfied with the proposed parking and traffic management of the site.

The concerns of the representors have been addressed the response document provided by the applicant with these concerns being considered as part of the assessment herein. The primary concerns in relation to traffic and noise have been adequately addressed and supported with relevant reports and documentation prepared by suitably qualified professionals.

The development exhibits suitable merit when assessed on balance against the relevant Desired Outcomes, Performance Outcomes and applicable Designated Performance Features to such a degree that it warrants Planning Consent subject to the conditions listed below in the Recommendation.

STAFF RECOMMENDATION

That pursuant to the authority delegated to the Council Assessment Panel by the Council, it is recommended that the Council Assessment Panel:

- a) DETERMINES that the proposed development is not seriously at variance with the policies in the Planning and Design Code; and
- b) GRANTS Planning Consent to the application by Pelligra Pty Ltd C-/ City Collective for a mixed use retail and indoor recreation centre including ice arena, basketball courts, children's play centre, café, changerooms and storage areas, tree damaging activity involving the removal of 13 regulated and 3 significant trees with associated parking and landscaping at Lot 1259 Ashfield Road, Elizabeth as detailed in Development Application ID 23022101 subject to conditions:

Reserved Matter

The following matter(s) have been reserved pursuant to section 102(3) of the *Planning, Development and Infrastructure Act 2016*, and sub-delegated to the Assessment Manager for a determination. Prior to the issue of Development Approval, the Applicant shall provide:

- The applicant is to submit a mechanical services plan (or similar) that confirms the type and size of mechanical equipment system to be installed within the facility prior to Council issuing Development Approval.
- The applicant to gain the appropriate clearance/approval from Department of Defence or relevant agency for development of a structure over 15 metres in height prior to Council issuing Development Approval.

ConditionsCouncil Conditions:

1. The development must be undertaken, completed and maintained in accordance with the plan(s) and information detailed in this Application except where varied by any condition(s) listed below.

Reason: To ensure that the development is constructed and operated in accordance with the plans and details provided.

2. All recommendations and noise attenuation measures contained within the Noise Assessment Report by Sonus Document Reference S7578C3 dated July 2023, shall be implemented prior to occupation of the site and complied with at all times.

3. Operating hours of the facility (Monday to Sunday) herein approved as follows:

- 6am to 10pm for the ice arena and associated café;
- 6am to 10pm for the basketball courts;
- 9am to 9pm for the rock climbing and children play centre
- 9am to 9pm for the externally operated cafe; and
- 24 hours for the gymnasium

4. Deliveries and service hours herein approved are as follows:

Rubbish Collection and deliveries:

- 9am -7pm on Sundays or Public Holidays
 - 7am-7pm on any other day
5. In lieu of planting 35 replacement trees, \$5705 (@ \$163 per tree) must be paid into Council's Urban Tree Fund within one month of the tree(s) being removed.
- Reason: To ensure compliance with the legislative requirement for the planting of replacement trees, pursuant to Section 127(4) of the Planning, Development and Infrastructure Act 2016.
6. All loading and unloading of goods and merchandise shall be carried out upon the subject land and no loading of any goods or merchandise shall be permitted to be carried out in the street in conjunction with the consent herein granted.
7. Landscaping shown on the plans herein approved shall be established to the reasonable satisfaction Council prior to the operation of the development and shall be maintained and nurtured at all times with any diseased or dying plants being replaced within the first planting season following removal.
8. The operator of the facility is to maintain a Traffic Event Management Plan that will be provided to Council (5 business days) prior to any major event (ice hockey or of a similar scale) where traffic and pedestrian movements are required to be managed.
9. All waste and rubbish shall be stored in covered containers prior to removal and shall be screened from public view.
10. Any lights on the subject land must be directed and screened so that overspill of light into the nearby properties is avoided and motorists are not distracted.
11. All vehicle car parks, driveways and vehicle entry and manoeuvring areas shall be designed and constructed in accordance with the relevant Australian Standards and be constructed, drained and paved with bitumen, concrete or paving bricks in accordance with sound engineering practice and appropriately line marked to the reasonable satisfaction of Council prior to the occupation or use of the development.
12. Car parking areas, driveways and vehicle manoeuvring areas shall be maintained at all times to the reasonable satisfaction of the relevant authority.
13. Low impact permeable surface and base shall be installed in the car parking area adjacent to trees 4,5,6,7 in accordance with methods outlined in Appendix E - Tree Protection Guidelines of the Arborman Arborist Report dated 1 February 2024.
14. A project arborist shall attend the site during any periods of construction within the Tree Protection Zone's of trees 4, 5, 6 & 7 to ensure that any works mitigate damage to the root zones in a manner consistent with the recommendations contained in the Arborman Arborist Report dated 1 February 2024.
15. Methods of tree protection identified in the Tree Protection Guidelines of the Arborman Arborist Report dated 1 February 2024 shall be adopted for the duration of the construction period.
16. All storm-water drainage shall discharge so that it does not flow or discharge onto land of adjoining owners or, in the opinion of the relevant authority, detrimentally affect structures on this site, any adjoining land or public road.

Department of Infrastructure and Transport Conditions:

17. Access to the site shall be gained via Ashfield Road in accordance with the site plan produced by City Collective, dated July 2023.
18. All stormwater run-off shall be collected on-site and discharged without impacting the safety and integrity of the adjacent road network (including the adjacent embankments). Any existing (and proposed) drainage on the abutting roads shall be accommodated by the development and any alterations to road drainage infrastructure as a result of this development are to be at the expense of the applicant.

Notes

1. The Metropolitan Adelaide Road Widening Plan shows a possible requirement for a strip of land up to 4.5 metres in width from the Main North Road and Philip Highway frontage of this site for future upgrading of Main North Road and Philip Highway intersection. The consent of the Commissioner of Highways under the Metropolitan Adelaide Road Widening Plan Act 1972 is required to all building works on or within 6.0 metres of the possible requirement. As the development encroaches within the above area, the attached consent form and a copy of the approved plan/s must be provided to DIT (via dit.landusecoordination@sa.gov.au) for consent purposes.



MINUTES

of

COUNCIL ASSESSMENT PANEL MEETING

Pursuant to the provisions of Section 82 of the Planning, Development and Infrastructure Act 2016

HELD IN

**COUNCIL CHAMBERS
PLAYFORD CIVIC CENTRE
10 PLAYFORD BOULEVARD, ELIZABETH**

ON

**THURSDAY, 18 APRIL 2024
AT 6:00PM**

The meeting commenced at 6:04 pm.

1 ATTENDANCE RECORD

1.1 Present

MR GEOFF PARSONS – PRESIDING MEMBER

Ms Cherie Gill (Deputy) Mr Paul Mickan

Ms Misty Norris

Also in attendance for the meeting:

Assessment Manager
Senior Manager Development Services
Senior Development Officer - Planning
Development Officer - Planning
Development Officer - Planning
Planning Consultant
Manager Governance
Governance Support
ICT Support Officer
Minute Taker

Mr Adam Squires
Mr Matt Dineen
Mr Mahmoud Hasaneen
Mr Jamie Hanlon
Ms Natalie Luk
Mr Andrew Humby
Ms Zoey Squires
Ms Kiraly Gosnell
Mr Abdoulaye Barry
Mrs Skye Nitschke

Cr Norris left the meeting room at 7:33 pm in response to her declared conflict of interest.

**5.2 23022101- LOT 1259 ASHFIELD ROAD- MIXED USE DEVELOPMENT
COMPRISING INDOOR RECREATION FACILITY, RETAIL OUTLET, CAFE,
TREE DAMAGING ACTIVITY WITH ASSOCIATED PARKING AND
LANDSCAPING**

Representors: Jordan Ritchie | Not in attendance
Phoebe Archer | Not in attendance
Christina Pires | Not in attendance
Beverly Rudnick | written representation displayed on
screen
Conrad Isterling | Addressed the Panel
Peter Ruwette | Not in attendance
Neave Colbert | Addressed the Panel

Applicant: Pelligra Group C-/ City Collective, Renae Grida, Blake
Soroka & Ben Wilson – CIRQA via Zoom | Addressed the
Panel

Edi Bergamin, City of Playford | responded to Panel
queries

PANEL RESOLUTION**CAP552**

That pursuant to the authority delegated to the Council Assessment Panel by the Council, it is recommended that the Council Assessment Panel:

- a) **DETERMINES** that the proposed development is not seriously at variance with the policies in the Planning and Design Code; and
- b) **GRANTS** Planning Consent to the application by Pelligra Pty Ltd C-/ City Collective for a mixed use retail and indoor recreation centre including ice arena, basketball courts, children's play centre, café, changerooms and storage areas, tree damaging activity involving the removal of 13 regulated and 3 significant trees with associated parking and landscaping at Lot 1259 Ashfield Road, Elizabeth as detailed in Development Application ID 23022101 subject to conditions:

Reserved Matter

The following matter(s) have been reserved pursuant to section 102(3) of the *Planning, Development and Infrastructure Act 2016*, and sub-delegated to the Assessment Manager for a determination. Prior to the issue of Development Approval, the Applicant shall provide:

- The applicant is to submit a mechanical services plan (or similar) that confirms the type and size of mechanical equipment system to be installed within the facility prior to Council issuing Development Approval.
- The applicant to gain the appropriate clearance/approval from Department of Defence or relevant agency for development of a structure over 15 metres in height prior to Council issuing Development Approval.
- A final landscaping plan shall be supplied for the subject development providing details of plantings species, location and maintenance/pruning schedule is to be provided for assessment and approval by the Assessment Manager.
- An updated parking plan providing details of bicycle parking shall be supplied for the subject development providing patron and staff cycle parking for assessment and approval by the Assessment Manager.

Conditions**Council Conditions:**

1. The development must be undertaken, completed and maintained in accordance with the plan(s) and information detailed in this Application except where varied by any condition(s) listed below.

Reason: To ensure that the development is constructed and operated in accordance with the plans and details provided.

2. All recommendations and noise attenuation measures contained within the Noise Assessment Report by Sonus Document Reference S7578C3 dated July 2023, shall be implemented prior to occupation of the site and complied with at all times.
3. Operating hours of the facility (Monday to Sunday) herein approved as follows:
 - 6am to 10pm for the ice arena and associated café;
 - 6am to 10pm for the basketball courts;
 - 9am to 9pm for the rock climbing and children play centre;
 - 9am to 9pm for the externally operated cafe; and
 - 24 hours for the gymnasium

4. Deliveries and service hours herein approved are as follows:

Rubbish Collection and deliveries:

- 9am -7pm on Sundays or Public Holidays
 - 7am-7pm on any other day
5. In lieu of planting 35 replacement trees, \$5705 (@ \$163 per tree) must be paid into Council's Urban Tree Fund within one month of the tree(s) being removed.

Reason: To ensure compliance with the legislative requirement for the planting of replacement trees, pursuant to Section 127(4) of the *Planning, Development and Infrastructure Act 2016*.
 6. All loading and unloading of goods and merchandise shall be carried out upon the subject land and no loading of any goods or merchandise shall be permitted to be carried out in the street in conjunction with the consent herein granted.
 7. Landscaping shown on the plans herein approved shall be established to the reasonable satisfaction Council prior to the operation of the development and shall be maintained and nurtured at all times with any diseased or dying plants being replaced within the first planting season following removal.
 8. The operator of the facility is to maintain a Traffic Event Management Plan that will be provided to Council (5 business days) prior to any major event (ice hockey or of a similar scale) where traffic and pedestrian movements are required to be managed.
 9. All waste and rubbish shall be stored in covered containers prior to removal and shall be screened from public view.
 10. Any lights on the subject land must be directed and screened so that overspill of light into the nearby properties is avoided and motorists are not distracted.
 11. All vehicle car parks, driveways and vehicle entry and manoeuvring areas shall be designed and constructed in accordance with the relevant Australian Standards and be constructed, drained and paved with bitumen, concrete or paving bricks in accordance with sound engineering practice and appropriately line marked to the reasonable satisfaction of Council prior to the occupation or use of the development.
 12. Car parking areas, driveways and vehicle manoeuvring areas shall be maintained at all times to the reasonable satisfaction of the relevant authority.
 13. Low impact permeable surface and base shall be installed in the car parking area adjacent to trees 4, 5, 6, & 7 in accordance with methods outlined in Appendix E - Tree Protection Guidelines of the Arborman Arborist Report dated 1 February 2024.
 14. A project arborist shall attend the site during any periods of construction within the Tree Protection Zones of trees 4, 5, 6 & 7 to ensure that any works mitigate damage to the root zones in a manner consistent with the recommendations contained in the Arborman Arborist Report dated 1 February 2024.
 15. Methods of tree protection identified in the Tree Protection Guidelines of the Arborman Arborist Report dated 1 February 2024 shall be adopted for the duration of the construction period.
 16. All storm-water drainage shall discharge so that it does not flow or discharge onto land of adjoining owners or, in the opinion of the relevant authority, detrimentally affect structures on this site, any adjoining land or public road.
- Department of Infrastructure and Transport Conditions:**
17. Access to the site shall be gained via Ashfield Road in accordance with the site plan produced by City Collective, dated July 2023.
 18. All stormwater run-off shall be collected on-site and discharged without impacting the safety and integrity of the adjacent road network (including the adjacent embankments). Any existing (and proposed) drainage on the abutting roads shall

be accommodated by the development and any alterations to road drainage infrastructure as a result of this development are to be at the expense of the applicant.

Notes

1. The Metropolitan Adelaide Road Widening Plan shows a possible requirement for a strip of land up to 4.5 metres in width from the Main North Road and Philip Highway frontage of this site for future upgrading of Main North Road and Philip Highway intersection. The consent of the Commissioner of Highways under the *Metropolitan Adelaide Road Widening Plan Act 1972* is required to all building works on or within 6.0 metres of the possible requirement. As the development encroaches within the above area, the attached consent form and a copy of the approved plan/s must be provided to DIT (via dit.landusecoordination@sa.gov.au) for consent purposes.

CARRIED UNANIMOUSLY

Cr Norris returned to the meeting room at 8:35 pm.

6 APPLICATIONS FOR CONSIDERATION – NO PERSONS TO BE HEARD

Nil

7 APPLICATIONS FOR CONSIDERATION - CATEGORY 1

Nil

8 OUTSTANDING MATTERS – APPEALS AND DEFERRED ITEMS

Nil

9 OTHER BUSINESS

9.1 STAFF REPORTS

Nil

10 CONFIDENTIAL MATTERS

Nil

11 POLICY DISCUSSION FORUM

Nil

12 CLOSURE

The meeting closed at 8:37 pm.